

7.1.2 TPA/55011 - 1575 DANDENONG ROAD AND 9-15 PARK ROAD, OAKLEIGH - CONSTRUCTION OF BUILDINGS AND WORKS AND ALTERATION OF ACCESS TO A TRANSPORT ZONE

Responsible Manager:	Catherine Sherwin, Manager City Planning
Responsible Director:	Peter Panagakos, Director City Development

EXECUTIVE SUMMARY

This application proposes buildings and works to develop the site for the purpose of a Customer Fulfilment Centre associated with Woolworths supermarkets. This use requires a warehouse facility to fulfil online orders of supermarket products, involving bulk deliveries of goods to the land, storage and some manufacture (a bakery), and the delivery of goods to customers throughout Victoria.

The application was exempt from public notification under the Planning Scheme requirements.

Key issues to be considered relate to building setback to the street (Park Road), car parking and traffic generation, landscaping opportunities and tree retention.

This report assesses the proposal against the provisions of the Monash Planning Scheme including the relevant state, regional and local planning policy framework.

The reason for presenting this report to Council is the proposed development cost of \$82 million.

The proposal is considered appropriate having regard to the relevant provisions of the Monash Planning Scheme. It is recommended that Council issue a Planning Permit subject to conditions.

RESPONSIBLE DIRECTOR:	Peter Panagakos, Director City Development
RESPONSIBLE MANAGER:	Catherine Sherwin, Manager City Planning
RESPONSIBLE PLANNER:	Anne Maree Roberts
WARD:	Oakleigh
PROPERTY ADDRESS:	1575 Dandenong Road and 9-15 Park Road, Oakleigh
NUMBER OF OBJECTIONS:	N/A
ZONING:	Industrial Zone (Schedule 1)
OVERLAY:	Design and Development Overlay (Schedule 1)
EXISTING LAND USE:	Motor Vehicle Sales
RELEVANT POLICY:	<u>Planning Policy Framework</u> Clause 11.01-1R - Settlement – Metropolitan Melbourne Clause 11.02-1S - Supply of Urban Land Clause 13.07-1S - Land Use Compatibility

	<p>Clause 15.01-1S&R - Urban Design Clause 15.01-2S - Building Design Clause 15.01-5S - Neighbourhood Character Clause 15.03-2S - Aboriginal Cultural Heritage Clause 17.01-1S&R - Diversified Economy Clause 17.02-1S - Business Clause 18.01-1S - Land Use and Transport Planning Clause 19.03-3S - Integrated Water Management</p> <p><u>Local Planning Policy Framework</u></p> <p>Clause 21 - Municipal Strategic Statement (Introduction) Clause 21.05 - Economic Development Clause 21.08 - Transport and Traffic Clause 21.13 - Sustainability and Environment Clause 22.03 - Industry and business development and character policy Clause 22.04 - Stormwater Management Policy Clause 22.05 - Tree Conservation Policy Clause 22.13- Environmentally Sustainable Development Policy</p> <p><u>Particular Provisions</u></p> <p>Clause 52.06 - Car Parking Clause 52.29 - Land Adjacent to the Principal Road Network Clause 53.18 - Stormwater Management in Urban Development Clause 65 - Decision Guidelines</p>
STATUTORY (60 DAY) PROCESSING DATE:	27 January 2024
DEVELOPMENT COST:	\$82,150,000

LOCALITY PLAN



& NEIGHBOURHOOD PLAN



RECOMMENDATION

That Council resolves to Grant a Planning Permit (TPA/55011) for building and works and alterations to access to a road in a Transport Zone 2 at 1575 Dandenong Road & 9-15 Park Road OAKLEIGH VIC 3166 subject to the following conditions:

Amended Plans Required

- 1. Before the development starts, amended plans to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. The plans must be drawn to scale and dimensioned. When the plans are endorsed they will then form part of the Permit. The plans must be generally in accordance with the decision plans prepared by Watson Young Architects, Revision F dated 3 November 2023 but modified to show:**
 - a) The location of any required fire services, electricity supply, gas and water meter boxes must be discreetly located and/or screened to complement the development service enclosures.**
 - b) The boundaries of the land to be dimensioned.**
 - c) Trees to be retained to be numbered in accordance with the Arborist Report prepared by Chatfield Arborists and Consultants dated November 2023.**
 - d) No excavation within 5 metres of the outer edge of the buttress flare of the 3 Fig trees located in Park Road (trees 9, 19 and 11).**
 - e) No excavation within 3.6m of the tree base of the northernmost Fig Tree (tree 12).**
 - f) No new excavation within 3 metres of the tree base of the Willow Myrtle (tree 20) in Park Road.**
 - g) Any disused vehicle crossovers to be marked to be reinstated with kerb and channel.**
 - h) All Traffic control measures specified in the Traffic Impact Assessment Report prepared by Ratio Consultants dated 10 November 2023 detailing line marking and signage to be provided on the site plan.**
 - i) A corner splay or area at least 50 per cent clear of visual obstructions extending at least 2 metres along the frontage road from the edge of an exit lane and 2.5 metres along the exit lane from the frontage, to provide a clear view of pedestrians on the footpath of the frontage road. The area clear of visual obstructions may include an adjacent entry or exit lane where more than one lane is provided, or adjacent landscaped areas, provided the landscaping in those areas is less than 900mm in height.**
 - j) A Landscape Plan in accordance with Condition 3 of this Permit.**
 - k) A Waste Management Plan in accordance with Condition 4 of this Permit.**
 - l) A Sustainable Management Plan in accordance with Condition 5 of this Permit.**
 - m) A Tree Management Plan in accordance with Condition 6 of this Permit.**

All to the satisfaction of the Responsible Authority.

Layout not to be Altered

- 2. The development as shown on the endorsed plans must not be altered without the written consent of the Responsible Authority.**

Compliance with documents approved under this permit

- 3. At all times what the permit allows must be carried out in accordance with the requirements of any document approved under this permit to the satisfaction of the responsible authority.**

Landscape Plan

- 4. Concurrent with the endorsement of any plans pursuant to Condition 1, a landscape plan prepared by a Landscape Architect or a suitably qualified or experienced landscape designer, drawn to scale and dimensioned must be submitted to and approved by the Responsible Authority. When endorsed, the plan will form part of the Permit. The Landscape Plan must be generally in accordance with the Landscape Concept Plan prepared by Urbis Pty Ltd, Revision C dated 17 November 2023, except that the plan must show:
 - a) Retained trees including Council street trees to be numbered in accordance with the Arborist Report prepared by Chatfield Arborist dated 16 November 2023.**
 - b) Details and dimensions of required TPZ during construction for all trees on site to be retained.**
 - c) All changes in accordance with Condition 1 of this Permit.****

Before the occupation of the buildings allowed by this permit, landscaping works as shown on the endorsed plans must be completed to the satisfaction of the Responsible Authority and then maintained to the satisfaction of the Responsible Authority.

Waste Management Plan

- 5. The provisions, recommendations and requirements of the endorsed Waste Management Plan prepared by Ratio and dated May 2023 must be implemented and complied with to the satisfaction of the Responsible Authority.**

Sustainable Design Assessment

- 6. The Sustainable Management Plan prepared by Sustainable Design Consultants dated May 2023 will be endorsed as part of the planning permit and the development must incorporate the sustainable design initiatives outlined in the Sustainable Management Plan to the satisfaction of the Responsible Authority.**

Tree Management Plan

- 7. Concurrent with the submission of amended plans required by Condition 1 and prior to any demolition or site works, a Tree Management Plan (TMP) must be submitted to and approved by the Responsible Authority. The TMP must be prepared by a suitably qualified and experienced Arborist and must set out recommendations and requirements in relation to the management and maintenance of all Council street trees in Park Road and all trees on site known as Tree Nos. 6, 7, 15, 18 and 19 (as identified in the Arborist Report**

submitted with the application, prepared by Chatfield) Arborists and Consultants dated 16.11.2024.

The TMP must be approved by the Responsible Authority prior to the commencement of any works, including demolition and/or levelling of the site. The TMP must make specific recommendations in accordance with the Australian Standard AS4970: 2009 - Protection of Trees on Development Sites and detail the following to the satisfaction of the Responsible Authority ensuring the trees to be retained remain healthy and viable during construction:

- a) A Tree Protection Plan drawn to scale that shows:
 - I. Tree protection zones and structural root zones of all trees to be retained,
 - II. All tree protection fenced off areas and areas where ground protection systems will be used;
 - III. The type of footings within any tree protection zones;
 - IV. Any services to be located within the tree protection zone and a notation stating all services will either be located outside of the tree protection zone, bored under the tree protection zone, or installed using hydro excavation under the supervision of the Project Arborist; and
 - V. A notation to refer to the Tree Management Plan for specific detail on what actions are required within the tree protection zones.
- b) Details of how the root system of any tree to be retained will be managed. This must detail any initial non-destructive trenching and pruning of any roots required to be undertaken by the Project Arborist.
- c) Supervision timetable and certification of tree management activities required by the Project Arborist to the satisfaction of the responsible authority; and
- d) Any remedial pruning works required to be performed on tree canopies located within subject site. The pruning comments must reference Australian Standards 4373:2007, Pruning of Amenity Trees and a detailed photographic diagram specifying what pruning will occur.

The recommendations contained in the approved tree management plan must be implemented to the satisfaction of the Responsible Authority.

Construction Management Plan

8. Prior to the commencement of any site works (including any demolition and excavation), a Construction Management Plan (CMP) must be submitted and approved by the Responsible Authority. No works are permitted to occur until the Plan has been endorsed by the Responsible Authority. Once endorsed, the CMP will form part of the permit and must be implemented to the satisfaction of the Responsible Authority. The CMP must address the following issues:
 - a) Appropriate measures to control noise, dust and water and sediment laden runoff;
 - b) Appropriate measures for the prevention of silt or other pollutants from entering into the Council's underground drainage system or road network;
 - c) Appropriate measures relating to removal of hazardous or dangerous material from the site, where applicable;

- d) **A plan showing the location and design of a vehicle wash-down bay for construction vehicles on the site so as to prevent material leaving the site and being deposited on Council's road network;**
- e) **A program for the cleaning and maintaining surrounding road surfaces;**
- f) **A site plan showing the location of any site sheds, on-site amenities, building waste storage and the like, noting that Council does not support the siting of site sheds within Council road reserves;**
- g) **Measures to provide for public Safety and site security;**
- h) **A plan showing the location of parking areas for construction and sub-contractors' vehicles on and surrounding the site, to ensure that vehicles associated with construction activity cause minimum disruption to surrounding premises. Any basement car park on the land must be made available for use by sub-constructors/tradespersons upon completion of such areas, without delay;**
- i) **A Traffic Management Plan showing truck routes to and from the site;**
- j) **A swept path analysis demonstrating the ability for trucks to enter and exit the site in a safe manner for the largest anticipated truck associated with the construction;**
- k) **Appropriate measures to ensure that sub-contractors/tradespersons operating on the site are aware of and adhere to the requirements of the CMP;**
- l) **The provision of contact details of key construction site staff; and**
- m) **Include a requirement that except with the prior written consent of the Responsible Authority, a requirement that demolition, excavation or construction works must only be carried out during the following hours:**
 - **Monday to Friday (inclusive) – 7.00am to 6.00pm;**
 - **Saturday – 9.00am to 1.00pm;**
 - **Saturday – 1.00pm to 5.00pm (Only activities associated with the erection of buildings that does not exceed the EPA guidelines)**
 - **No works are permitted on Sundays or Public Holidays.**

The provisions, recommendations and requirements of the endorsed Construction Management Plan must be implemented and complied with by all contractors to the satisfaction of the Responsible Authority.

Landscaping and Tree Protection

9. **All trees specified in the endorsed Tree Management Plan are to be protected and maintained in accordance with the recommendations set out in the report, to the satisfaction of the Responsible Authority.**
10. **Before occupation of any of the buildings, landscaping works as shown of the endorsed plans must be completed to the satisfaction of the Responsible Authority and then maintained to the satisfaction of the Responsible Authority.**
11. **An in-ground, automatic watering system linked to rainwater tanks on the land must be installed and maintained to the common garden areas to the satisfaction of the Responsible Authority.**

Street Tree Removal

12. The existing street trees #21 a *Fraxinus augstifolia* (Desert Ash) #22 a *Quercus robur* (English Oak) in Dalgety Street must only be removed and replaced by Council at the cost of the developer, prior to the commencement of the development.

Landfill Gas Assessment

13. Prior to the commencement of the development authorised under this permit (excluding works reasonably required to conduct the landfill gas assessment), the permit holder must to the satisfaction of the Responsible Authority:

- a) Engage a professional environmental consultant with demonstrated experience in the assessment of landfill gas in the subsurface environment, to conduct an assessment of any methane within the land, subsurface services and buildings and structures on the land and prepare and submit to the responsible authority the scope of the proposed risk assessment adopting the methane gas action levels prescribed at items 6 and 7 of schedule 3 of the Environment Protection Regulations 2021 (Vic) as set out below.

<i>Item</i>	<i>Location for assessing methane gas concentration action levels</i>	<i>Methane gas concentration action level</i>
6	Subsurface services on, and adjacent to, the waste	10,000 parts per million
7	Buildings and structures on, and adjacent to, the waste	5000 parts per million

- b) Upon approval of the scope of the risk assessment by the responsible authority, have the consultant conduct the risk assessment and prepare a report to be submitted to the responsible authority which contains the consultant's opinion as to any potential risk associated with landfill gas beneath the land and any recommendations for the management or monitoring of the gas. The consultant must provide an opinion on whether further investigation is required, in particular, whether such further investigation should consist of an environmental audit under Part 8.3 of the *Environment Protection Act 2017*.
- c) Implement any recommendations of the risk assessment report.
- d) If the risk assessment report, or the responsible authority recommends an audit under Part 8.3 of the *Environment Protection Act 2017*:
- i. engage an environmental auditor appointed (or taken to be appointed) under the *Environment Protection Act 2017* to prepare and submit to the

satisfaction of the responsible authority a scope of the proposed audit which includes consideration of both landfill gas and odour risk;

- ii. have the environmental auditor conduct an audit under Part 8.3 of the *Environment Protection Act 2017* in accordance with the agreed scope;
- iii. implement any recommendations of the audit report.

14. Prior to commencement of use or occupation of the development, the permit holder must:
- a) implement all recommendations in an environmental audit statement to the satisfaction of the responsible authority;
 - b) where the recommendations require verification of works or compliance, provide written confirmation of compliance from a suitably qualified environmental professional or other suitable person acceptable to the responsible authority. Compliance sign-off must be in accordance with any requirements in the conditions or recommendations regarding verification of works.
 - c) if the risk assessment report or audit report requires ongoing management or monitoring, the owner must enter into an agreement under section 173 of the *Planning and Environment Act 1987* with the responsible authority requiring the implementation of any ongoing requirements. The owner/operator under this permit must pay the reasonable costs of the preparation, execution and registration of the section 173 agreement.

Landfill Gas Risk Assessment Peer Review

15. Prior to the commencement of the development authorised under this permit, the permit holder must:
- a) provide to Council a copy of the LGRA undertaken in accordance with condition 13 within 14 days of receiving the LGRA;
 - b) pay Council's costs and expenses associated with a Council-arranged peer review of the LGRA. The peer review will be undertaken by an independent and suitably qualified environmental consultant nominated by Council; and
 - c) obtain a copy of the peer review obtained by Council.

The recommendations of the LGRA including any requirements arising from the peer review are to be implemented by the permit holder.

Maintenance

16. All buildings and works must be maintained in good order and appearance to the satisfaction of the responsible authority.

Car Parking and Accessways

17. Details of any car park control equipment must be submitted to and approved in writing by the Responsible Authority. These details must include a car park control device which

can be accessed by visitors to the development including clear instructions on how to operate any security system.

18. All loading and unloading of vehicles must be carried out within the boundaries of the land/ designated loading bay and must be conducted in a manner that does not cause any interference with the circulation and parking of vehicles on the land, to the satisfaction of the Responsible Authority.
19. Before the development of each building permitted is completed, areas set aside for parked vehicles and access lanes for each building as shown on the endorsed plans must be:
 - a) constructed to the satisfaction of the Responsible Authority;
 - b) properly formed to such levels that they can be used in accordance with the plans;
 - c) surfaced with an all-weather sealcoat to the satisfaction of the Responsible Authority;
 - d) drained, maintained and not used for any other purpose to the satisfaction of the Responsible Authority;
 - e) line-marked to indicate each car space and all access lanes to the satisfaction of the Responsible Authority.

Parking areas and access lanes must be kept available for these purposes at all times.

20. The accessible parking spaces should be designed in accordance with the Australian Standard for *Off-Street Parking for people with disabilities, AS/NZS 2890.6*. The vehicle path to and from each accessible space shall have a minimum headroom of 2200mm. The headroom above each dedicated space and adjacent shared area shall be a minimum of 2500mm.

Service Enclosures

21. Any required fire services, electricity supply, gas and water meter boxes must be discreetly located and/or screened to compliment the development to the satisfaction of the Responsible Authority. Any required services must be clearly detailed on endorsed plans forming part of this permit.

Bicycle parking spaces

22. Bicycle parking facilities shall generally follow the design and signage requirements set out in Clause 52.34 of the Monash Planning Scheme. A minimum of 20% ground level (horizontal) parking spaces as per AS2890.3:2015 is required.

Stormwater & Drainage

23. The site must be drained to the satisfaction of the Responsible Authority.
24. The internal drainage easement is to be removed at the time of consolidation and the 225 mm Council drain is to be removed to the satisfaction of the Responsible Authority.

Department of Transport (PPR 43528/23)

25. Prior to commencement of use, the bus stop must be removed, and the concreted area replaced with grass to the satisfaction of the Responsible Authority, at no cost to the Head, Transport for Victoria.

Satisfactory Continuation and Completion

26. Once the development has started it must be continued and completed to the satisfaction of the Responsible Authority.

Time for Starting and Completion

27. In accordance with section 68 of the Planning and Environment Act 1987, this permit will expire if one of the following circumstances applies:

- a) The development is not started before 3 years from the date of issue.
- b) The development is not completed before 6 years from the date of issue.

In accordance with section 69 of the Planning and Environment Act 1987, the responsible authority may extend the periods referred to if a request is made in writing before the permit expires, or.

- (i) within six (6) months afterwards if the development has not commenced; or
- (ii) within twelve (12) months afterwards if the development has not been completed.

Council and the Victorian Civil and Administrative Tribunal are unable to approve requests outside of the relevant time frame.

NOTES

- A. This is not a Building Permit. Building approval must be obtained prior to the commencement of the above approved works.
- B. Council's Horticulture Department must be contacted regarding the removal of the street trees proposed. The trees will be removed by Council at a cost to the Permit holder.
- C. Unless no permit is required under the planning scheme, no sign must be constructed or displayed without a further permit.
- D. All new vehicle crossings and access roads are to be no closer than 1.0 metre, measured at the kerb, to the edge of any power pole, drainage or service pit, or other services. Approval from affected service authorities is required as part of the vehicle crossing application process.
- E. Engineering permits must be obtained for new or altered vehicle crossings and new connections to Council pits and these works are to be inspected by Council's Engineering Department. A refundable security deposit of \$1000 is to be paid prior to the drainage works commencing.

- F. The redundant crossings are to be removed and reinstated with kerb and channel to the satisfaction of Council. The footpath and nature strip are to be reinstated to the satisfaction of Council.
- G. All stormwater collected on the site from all hard surface areas must not be allowed to flow uncontrolled into adjoining properties or the road reserve.
- H. One copy of the plans for the drainage and civil works must be submitted to and approved by the Engineering Department prior to the commencement of works. The plans are to show sufficient information to determine that the drainage works will meet all drainage conditions of the permit.

COUNCIL PLAN STRATEGIC OBJECTIVES

Sustainable City

Ensure an economically, socially, and environmentally sustainable municipality.

Enhanced Places

Pursue a planning framework that meets Monash needs.

Good Governance

Maintain the highest standards of good governance.

BACKGROUND

History

The land at 1575 Dandenong Road has historically been used for motor vehicle sales since the early 1970's. To the site's rear, at its north-eastern edge, land known as 9-15 Park Road was later developed with the present day building in the late 1980's. The most recent permit issued for the land at 9-15 Park Road was to use this property for motor vehicle parts and servicing (Permit No.27753, 2001). Several Permits were also issued for 1575 Dandenong Road including creation of an internal car wash facility and works to the existing showroom issued in 2016 and 2019 respectively.

Site and Surrounds

The subject site is located on the north-west corner of Dandenong and Park Roads in Oakleigh. The land has an abuttal to Dalgety Street in the north-west corner.

The site, which comprises of multiple land Titles, has a frontage to Dandenong Road of approximately 130 metres and to Park Road of 218 metres. The site area is 23,472 square metres.

The land is generally rectangular in shape with a slope of approximately 5 metres from the south-east corner towards the north-east. There are several easements encumbering the site for drainage, sewerage, and electricity supply purposes. A telecommunications tower and associated equipment is located within one of the easements along the northern boundary.

There are no restrictive covenants or agreements registered on the Title.

The land is developed with numerous buildings, including a car showroom and hardstand display area fronting Dandenong Road, workshop, warehouses, and offices.

There are eleven trees (11) trees on the site, none have been identified as significant. A total of seven (7) street trees abut the site along the Park Road frontage, and two (2) trees abutting the site on Dalgety Street.

Surrounding land is developed with low rise buildings used for a variety of industrial uses, with the Oakleigh Recreation Centre located directly opposite on Park Road. The immediate surrounding area can be described as follows:

North

No. 17 Park Road is developed with a single-storey industrial building, at-grade car parking and informal car storage. Access to the site is provided via a double cross-over located at the north end of the site. Additional access is provided through the rear of the site via Dalgety Street.

Further north of the site is the Oakleigh Golf Course and Scotchman's Creek Trail.

East

On the eastern side of Park Road there are a number of industrial uses including a restricted retail premises (equipment hire), motor vehicle repairs, and kitchen manufacturing. The buildings are a mix of single and double storey scale.

A small single level Place of Worship is at number 4 Park Road.

The Monash Oakleigh Recreation Centre is located at No. 2 Park Road, with car parking provided to the centre and rear of the site and a three storey built form at the corner of Park Road and Bolch Place.

South

Across the service lane and 7 lanes of Dandenong Road is residential land (zoned GRZ2) developed with primarily single dwellings and medium density housing, and the Lamplighter Motel.

West

No. 1557-1567 Dandenong Road is developed with a single-storey building and hardstand display used for motor vehicle sales. Behind this site fronting Atkinson Street is its service centre (motor vehicle repairs) and the Quince's Bus Depot.

An aerial photograph of the subject site and surrounding land can be found attached to this report (Attachment 2).

PROPOSAL

The proposal seeks to develop a Customer Fulfilment Centre (CFC) associated with Woolworths supermarkets. A CFC is a warehouse facility which fulfils online orders of supermarket products. This will involve bulk deliveries of goods to the land, storing and selection of goods, some manufacture of goods on the land (bakery items), and the delivery of goods to customers throughout Victoria.

The delivery of products to customers will be undertaken by trucks or smaller vehicles. Woolworths propose to include a small pick-up facility which could be utilised by small contractors

(in cars or vans). The CFC also proposes an ancillary office space used for the management of the warehouse.

The building will comprise the following:

- Ground level warehouse and ancillary bakery, office lobby, amenities and loading docks with a net floor area (NFA) of 8,808 square metres.
- Basement with 152 car parking spaces, 125 van parking spaces, 42 bicycle parking spaces and associated end of trip facilities.
- Mezzanine level 1 of 4,592 square metres NFA with warehouse and ancillary office space.
- Mezzanine level 2 of 4,415 square metres NFA with warehouse and ancillary office space (canteen and outdoor area).
- A front setback of 20 metres to Dandenong Road and a side setback of 7.6 metres to Park Road.
- New vehicular access will be created in Dandenong and Park Roads requiring the existing and redundant bus stop near the south-west corner of the site to be removed. The existing vehicle crossing at the rear of the site is to be widened to cater for semi trailer trucks.
- Contemporary architecture with a range of external materials including Colorbond wall cladding, precast concrete panels, extensive glazing on the south-east corner and metal feature canopy to Park Road elevation.
- In addition to fulfilling online deliveries, the site will also operate a Contractor Pick Up service that will operate independently from the warehouse and located in a dedicated area in the south-east corner of the building with access from Park Road. This is the only part of the site that will have access for pick up drivers.
- It is envisaged the facility will operate 24 hours a day, seven days a week. The warehouse will operate with overlapping shifts and the office will operate during normal business hours, Monday to Friday.
- The number of employees per shift (excluding delivery drivers) will vary from 20-60 staff and the office will have 15 staff. The number of employees present on site at any one time will be approximately 130, typically between 9.00am and 12.00pm on weekdays due to overlapping shifts.

The proposal can be summarised in detail as follows:

Maximum Overall Height	16.8 metres
Number of Storeys	4
Leasable Floor Area	18,200 square metres
Site Coverage	53.41%
Hours of Operation	24 hours per day, 7 days per week
Vehicle Access	2 Crossovers in Dandenong Road, 3 crossovers in Park Road, 1 crossover to Dalgety Street
Car Parking	277 (152 car parking spaces, 125 van parking spaces and 12 pick-up spaces)
Bicycle Spaces	96 spaces
Signage	Not part of this application

Materials	Colorbond wall cladding, precast concrete panels, extensive glazing
Tree Removal / Landscaping	There are eleven (11) trees on site and six (6) are proposed to be removed. Two (2) Council street trees are also proposed to be removed and are located in Dalgety Street.

The minimum building setbacks are detailed as follows:

	North (Rear)	East (side -Park Road)	South (front - Dandenong Road)	West (side)
Basement	6 metres	4 metres	20 metres	0 metres
Ground Floor	16.2 metres	12.8 metres	20 metres	6.9 metres
Mezzanine Level 1	16.2 metres	7.6 metres	20 metres	6.9 metres
Mezzanine Level 2	16.2 metres	7.6 metres	20 metres	6.9 metres

Attachment 1 (Development Plans) and 1a (Landscape Plan) details plans forming part of the application.

PERMIT TRIGGERS

Zoning

The subject site is located within the Industrial Zone, Schedule 1 (IZ1). Pursuant to Clause 33.01-4 of the Monash Planning Scheme, a Planning Permit is required to construct a building or construct or carry out works.

The use as a warehouse and industry (food manufacture component) does not require a permit as the proposal meets the conditions of Section 1.

Overlay

The subject site is affected by the Design and Development Overlay, Schedule 1 (DDO1). Pursuant to Clause 43.02-2 of the Monash Planning Scheme, a Permit is required to construct a building or construct or carry out works.

Particular and General Provisions

Clause 52.06: Car Parking

Before a new use commences the number of car parking spaces required under Clause 52.06-5 must be provided to the satisfaction of the responsible authority. Pursuant to the provisions of the scheme, a permit is required to reduce the number of car parking spaces required under Clause 52.06-5.

The proposal generates a requirement for 275 spaces and a total of 277 are proposed to be provided on the site. Therefore, there is no permit triggered under this provision.

Clause 52.29: Land Adjacent to The Principal Road Network

A permit is required to create or alter access to a road in a Transport Zone 2 pursuant to Clause 52.29-2. The proposal seeks to alter vehicle access along Dandenong Road, which is in the Transport Zone 2.

Closed Landfill Buffer

The subject site is identified as a closed landfill site (previous quarry) and is also located within the buffer area of three (3) other closed landfill sites. A Landfill Gas Investigation has been carried out and has identified that the site is suitable for the proposed commercial development, with landfill gas mitigation controls to be incorporated into the future buildings and underground services constructed on the site. This will require completion of further landfill gas investigations across the site and design of gas mitigation measures for the future commercial redevelopment. The preparation of a Landfill Gas Risk Assessment Report will be required by permit condition. This report is to be prepared by a suitably qualified environmental consultant and undertaken in accordance with the *Environment Protection Act 2017* and associated regulations and all relevant EPA guidelines including EPA Publication 1642.

Cultural Heritage Management Plan (CHMP)

The site is located within an area identified as having cultural heritage sensitivity. A preliminary cultural heritage assessment was carried out in February 2021 by Tardis Archeology. This assessment and a further statement provided in November 2023 concluded that the previous activities on the site have resulted in significant ground disturbance and that a mandatory Cultural Heritage Management Plan (CHMP) will not be required for the proposed works.

Attachment 3 details the zoning and overlays applicable to the subject site and surrounding land.

CONSULTATION

Further information was requested of the Permit Applicant on 3 July 2023. In this letter, officers raised the following preliminary concerns:

- The 3 metre setback to Park Road is insufficient to satisfy the landscaping objectives of the DDO.
- The 3 metre setback of the supporting beams and/or roof to the canopy structure along Park Road is insufficient to accommodate canopy planting above the height of the structure without potential impacts to tree canopies.
- Review of the basement design is required to increase setbacks to Park Road to facilitate greater areas for planting.
- Excessive external loading areas along Park Road.
- Preliminary Traffic concerns including:
 - vehicle access and the potential for additional traffic onto Park Road which provides for one way traffic north of the site into residential areas.
 - vehicle access in proximity to an established pedestrian crossing.
- Removal of the three street trees; #8 (Park Road), #21 and #22 (Dalgety Street) and all works in close proximity to Council street trees.

Officers advised the Applicant in writing that should these concerns not be addressed, that this application was unlikely to be supported by Council.

The Permit Applicant responded to this letter on 28 November 2023 by providing the requested information. In relation to the preliminary concerns the following amendments were made:

- The setback of the canopy along Park Road was increased from 3 metres to 4 metres.
- The metal canopy was setback 4 metres from Park Road with gaps for canopy tree growth.
- The basement design was modified to accommodate the increased 4 metre setback.
- New measures were incorporated to restrict the movements of vans and decrease the potential for additional traffic on Park Road, and improvements for pedestrian safety.
- Council street Tree #8 on Park Road will be retained.

The Applicant has been advised that this application was coming to the March Council meeting, and a letter was sent to the Applicant with the details of the Council meeting. The Applicant has also been advised that this application is recommended for approval subject to conditions, and an outline of the conditions and the ramifications of the conditions on the proposal has been explained.

Public Notice

The application was not advertised as pursuant to the provisions of both the zone and overlay the application is exempt from the notice requirements of section 52(1)(a), (b) and (d).

Referrals

External Referral

Department of Transport and Planning (DTP) (PPR 43528/23)

DTP does not object to the issue of a permit subject to a condition requiring the bus stop to be removed and the concreted area be replaced with grass.

Internal Referral

Traffic Engineer

The Traffic Engineers have advised that plans under assessment have addressed initial concerns and have recommended standard conditions. The development has sought to reduce the volume of vehicles exiting to the north onto Park Road by restricting van traffic to a left turn only at the northern access and encouraging vans to exit via the central Park Road access. This arrangement is considered satisfactory.

Strategic Planning

Council's Strategic Planning department have no concerns with the proposal noting:

- The subject site is not affected by the Boulevard Urban Design Framework.
- The height of the proposed development is modest at 16.8 metres, equivalent to 4-5 storeys, and less than the preferred heights in the adjacent DDO10 (Area B, C1 Zone fronting the north side of Dandenong Road) which is up to 21 metres (6 residential storeys).
- The proposed setbacks are consistent with the DDO1 and a substantial improvement from the existing situation.
- There is no concern regarding the Park Road setback with buildings above ground set back at minimum 7.6m from the boundary. The architectural elements along the Park Road interface within the 7.6m setback are lightweight and proposed to be vegetated. Combined with canopy trees, this is a suitable interface with Park Road and the Oakleigh Recreation Centre and obscure the loading areas.

Horticulture

The removal of the 15 metre tall Ash and Oak street trees in Dalgety Street (trees 21 and 22) is of concern to Council's Horticulture division, as they provide a significant amenity contribution in this mostly industrial pocket.

Advice has been provided that there should be no excavation within 5 metres of the outer edge of the buttress flare of the three (3) of the Fig trees in Park Road (trees 9, 19 and 11). The northernmost Fig (tree 12) requires an excavation clearance of 3.6 metres of the tree base.

No new excavation should be done within 3 metres of the tree base of the Willow Myrtle (tree 20) in Park Road.

Drainage Engineer

There are no concerns subject to standard conditions being included in the permit, in addition to a specific condition requiring the 225 mm Council drain and internal drainage easement to be removed to the satisfaction of the Responsible Authority.

Waste Services

Council's Waste Services advised that the submitted Waste Management Plan has met Council's requirements.

Water Sensitive Urban Design

The proposed WSUD measures detailed in the Sustainable Management Plan and Stormwater Treatment have been assessed and are satisfactory.

RELEVANT PLANNING POLICY

Planning Policy Framework (PPF)

Clause 17.03-1S (Industry) and Clause 17.01-1R (Economic Development) encourages the maintenance of land for industry in appropriate locations, protecting these areas from non-industrial land uses that will prejudice the availability of land for these uses.

Manufacturing and storage industries that generate significant volumes of freight are to be located close to air, rail and road freight terminals. To this respect, the site is located on a main arterial road with good access to the Monash Freeway and several other main arterial roads.

Clause 13.07-1S (Amenity, Human Health and Safety) seeks to ensure that the use or development of land is compatible with adjoining and nearby land uses and that off-site impacts are minimized.

Sustainable industry is encouraged by the policy framework which seeks to ensure buffers are provided between industrial uses and sensitive areas. Warehouse is a use that does not require a Planning Permit provided the activity meets safety buffer distances to any nearby residential land.

Clause 18.02-4S (Roads) seeks to ensure an adequate supply of car parking is appropriately designed and located. Land is to be set aside for car parking subject to the existing and potential modes of access including public transport, the demand for off-street car parking, road capacity and the potential demand for car parking.

Local Planning Policy Framework (LPPF)

Industry, business and activity centres in Monash need to continue to positively attract and retain local business to provide employment opportunities and investment. This may be achieved by proactively supporting the industry and business communities, retaining and enhancing the competitive advantages of Monash, and by creating viable, attractive and convenient industry, business and activity centres.

Monash is experiencing a loss of traditional industrial uses which are being replaced with commercial office-type premises. This is eroding the industrial employment base within the municipality and is likely to have socio-economic implications within the community such as diminished access to blue-collar employment and inflated overheads for existing businesses in these areas.

The industry and business character identified in Clause 21.07 (Business Parks and Industry) recognises that Monash has a long history of creating industrial development in a garden industrial environment. This character is important to the business community of Monash and to Council.

Clause 21.07-2 specifically identifies that substantial building setbacks planted with trees and other vegetation are regarded as very important in maintaining and enhancing the established Garden City Character and economic image of Monash.

Clause 22.03-1 (Industry and Business Development and Character Policy) identifies the site as being within the 'Industrial 1' character area. This policy works in conjunction with Clause 21.07-2, seeking to maintain landscaped front setbacks, noting specifically that poorly sited and designed business or industrial developments have the ability to progressively erode the basic fabric of the Garden City Character.

This policy identifies the precinct is strongly set by the businesses and industries which front the Princes Highway. These include car yards, service stations and other businesses. The buildings are simple in form and are set within hard paved areas. Some have an associated array of colourful signs, flags and banners. Behind the businesses fronting the Princes Highway are a number of other larger industries and businesses. These have varied architecture, setbacks and parking arrangements and are visually unrelated to each other.

The desired future character of this area seeks to:

- *Retain the commercial and industrial character;*
- *Buildings should address the Princes Highway. Variations in building height will be allowed, but large height differences should be gradated.*
- *Redevelopment should allow the progressive improvement of the aesthetic quality of the main road corridors, eventually allowing the Princes Highway to present a positive image of the municipality to road users.*
- *Standardisation of setbacks as development occurs should enable the development of a substantial landscape strip along the Princess Highway.*
- *Businesses should still have a high exposure to the road, but tree planting within the setback with high canopies and clear trunks should soften the hard qualities of the Character Type and help enclose the Princes Highway and unify the diverse range of land uses.*
- *Visual clutter should be progressively minimised by controlling the size and positioning of advertising signs and overhead services. A visual relationship between the colour*

schemes used within each property will be encouraged whilst recognising the importance of corporate colours.

- *The provision of tree planting within properties, including the rear of properties, should soften the hard urban qualities of the area and provide shade and a backdrop to the utilitarian buildings. The canopies of the trees will help unify the diverse collection of elements and improve pedestrian amenity.*

Industrial 1 Zone

The Industrial 1 Zone seeks to provide for manufacturing industry, the storage and distribution of foods and associated uses in a manner which does not affect the safety and amenity of local communities.

Design and Development Overlay, Schedule 1

Schedule 1 of the Design and Development Overlay includes design objectives and requirements for 'buildings and works' as follows:

- *To ensure that development, including front setbacks, is in keeping with and contributes to the Garden City Character as set out in the Municipal Planning Strategy.*
- *To ensure that the building scale and form in terms of height and bulk complements and does not visually overwhelm surrounding buildings.*
- *To ensure that streetscape engineering details of new developments integrate with the existing streetscape.*
- *To ensure that fences or planting along property boundaries do not adversely affect urban character or adjacent open space.*
- *To ensure that the landscape treatment within the front setback contributes to the positive aspects of the applicable industry or business Character Type identified in Clause 22.03.*
- *To retain existing on-site vegetation if possible.*
- *To ensure that car parking, vehicle access and service areas do not visually impinge on front setbacks or affect streetscape elements such as trees and nature strips.*
- *To minimise visual clutter.*

Under the schedule to the DDO, specific requirements relate to buildings and works including building, car park setbacks, fences, engineering design and services.

ASSESSMENT

Built Form

Height and Scale

The proposed building has been purposely designed to respond to the site's main road corner location and provides a strong presentation to the Dandenong Road and Park Road frontages. The building presents with the main focal point to the south eastern portion of the site with an angled roof line and setback to account for the triangle shaped of the site at the corner. The building will have a double storey commercial appearance at the front of the site to provide for office space and main feature of the building with the main warehouse component located to the rear.

The maximum height of the development is 16.8 metres including to the top of the roof feature which rises up towards the south east corner of the site. The roof scales down slightly with the warehouse component also with a maximum height of 16.8 metres. This height is typical of modern warehouse development and is the equivalent of 4- 5 storeys. The building height will sit comfortably on what is a substantial site with a frontage of over 130 metres to Dandenong Road and 220 metres to Park Road. The scale of the building will suitably complement existing development within the wider surrounding area, including the 3 storey height of the Oakleigh Recreation Centre. It is considered the building will add visual interest amongst the generous landscape frontages of both Dandenong Road and Park Road as shown in the below images:



Image above- perspective of building from Dandenong Road and Park Road



(Image above- perspective of building from Dandenong Road)

Setbacks

The proposal satisfies the objectives and setback requirements to the Design and Development Overlay Schedule 1(DDO1) as:

- The proposal has provided a 20 metre setback to Dandenong Road and a minimum 7.6 metre building setback from Park Road.
- Landscaping is incorporated within the setbacks to Dandenong Road and Park Road, including the provision of 40 canopy trees within the Dandenong Road setback and 5 canopy trees within the Park Road setback.

- Although the loading bay is setback 4 metres from the Park Road boundary, this is considered to comply with DDO1 as it is not a building or car park area. Importantly the 4 metre setback allows for large canopy tree planting and supporting landscaping along the frontage which satisfies the intent of the setback.
- No fences are proposed within the front setback to Dandenong Road. Fencing is provided to one section along the northern end of the Park Road frontage toward the rear of the site and comprises a 1.8 metre high black painted metal pickets. This is adjacent to the boom gate to the northern vehicle crossover and where the canopy structure ends. The transparent fence will allow for views into the site and provides pedestrian safety from the loading docks. New chain fencing will be provided to the western (side) and northern (rear) boundaries which is satisfactory as this does not present to a street interface.
- The proposal provides substantial landscaping opportunities from existing conditions which are dominated by hard surfaces.
- Services, including substation, switch room and water tanks are to be located in the basement, however a permit condition will be required to clarify the location of any other additional services to ensure that any at ground enclosures sit amongst the landscaping.
- Two new vehicular crossovers along Dandenong Road and three new crossovers are proposed along Park Road. They have been located to minimise impact on street trees with no trees to be removed along either road and appropriate setbacks to the street trees. The proposal will remove the five existing crossovers on these two roads which will no longer be required and reinstated as footpath and nature strip.
- A dedicated loading area is provided to the rear of the site for semi trailer trucks that will have minimal visibility from the site when viewed from Park Road and is not expected to be seen from Dandenong Road. All vehicle movements associated with semi trailers will be from a widened vehicle access in Dalgety Street which presents as the rear of the property with a substantially less public interface than Dandenong or Park Roads. This is a suitable response for a large scale development in an industrial area needing to provide extensive hard standing areas. The widening of the vehicular access to Dalgety Street will require the removal of two street trees. These are discussed in more detail later in this report.
- The Park Road frontage is provided with variation in building setbacks ranging from a minimum of 7.6 metres to 18.5 metres and is to be provided with a feature canopy with a setback of 4 metres. The canopy is of light weight construction, unroofed and supported with series of beams and pylons. The pylons range in height from 8.2 metres to 10 metres and are of different size and shape for visual interest. The beams and pylons have been spaced to allow for existing street trees and new canopy planting in between selected supports. Welded steel mesh is to be applied in between other supports to allow for vertical planting along the Park Road setback. The purpose of the structure is to reduce the visual impact of hard standing areas by creating a semi enclosed loading dock and a landscaped wall along the street. This is considered a suitable response to improve the presentation of a substantial warehouse building as shown in the below images:



(Images above – feature canopy to be provided to Park Road)

Materials / Detailed Design

The proposed architectural style is contemporary in design with a high-quality appearance with a range of robust materials comprising of Colorbond wall cladding and pre-cast concrete finishes. Proposed detailing will provide an articulated façade treatment and use of extensive glazing to the office component to the south east corner of the building. The office has been designed as an architectural element of the building, creating interest and a clear entrance point when viewed from the site’s frontage. The warehouse has been designed to break up the visual impact of blank walls with the use of different tones and materials and variation in the roof form. This approach ensures that the development does not present as a large warehouse building and incorporates the use of various materials and finishes to provide a highly articulated and attractive façade to the road reserve. The building provides an active and engaging interface to both streets, particularly to the prominent location on Dandenong Road.

Landscaping

Tree Removal

The application proposes the removal of six trees on the site and the removal of two Council street trees from Dalgety Street. The submitted Arborist report has identified the trees on site as of low to medium significance with no trees rated as high significance. Based on the comprehensive proposed landscaping and the scale of the development the removal of these 6 trees is appropriate.

The two Council trees that are proposed to be removed are at the rear of the site at Dalgety Street and are impacted by the proposed widening of the existing vehicle crossover to cater for semi trailer truck movements. The trees are known as Tree 21 a *Fraxinus angustifolia* (Desert Ash) with a height of 15 metres and Tree 22 a *Quercus robur* (English Oak) with a height of 12 metres and assessed by the applicant's arborist of medium retention value and useful life expectancy of 20 to 30 years. Both trees have been assessed by Council's Arborist as in good condition with a significant amenity contribution in an industrial area as shown in the below photograph.



(Photo above of Trees #21 and #22 from Applicant's Arborist)



(Photo above of Tree #21 from Applicant's Arborist)



(Photo above of Tree #22 from Applicant's Arborist)

In this case it is not possible to change the alignment of the crossover due to the narrow frontage of 18.5 metres to Dalgety Street or reduce the width of the crossover from proposed 8 metres to provide the recommended clearances for retention. It is noted that this part of the site does not have the same degree of pedestrian exposure as the road effectively terminates at the site

boundary. Vehicle access is encouraged via Dalgety Street to allow for a fluid movement of vehicles through the site, reducing congestion along Park Road. On balance with the traffic movement considerations and narrow street frontage, it is considered an acceptable outcome to remove the trees subject to the applicant paying the amenity value.

Proposed Landscaping

The proposal has been designed to incorporate generous landscaping and canopy tree coverage on the site, particularly within the front and eastern setbacks which will enhance the Garden City Character of the area.



(Image above – proposed landscaping plan)

It is proposed to provide 56 canopy trees on the site comprising of four different species ranging in height from 6 - 30 metres. This is to include 2 *Corymbia maculata* (Spotted Gum) trees that will achieve a height of 15-30 metres and width of 12-15 metres at maturity and 18 *Eucalyptus melliodora* (Yellow Box) trees with a height of 10-15 metres and width of 8-10 metres at maturity.

The Dandenong Road frontage is to be landscaped with 40 canopy trees supplemented with shrubs and groundcovers within the setback with a depth of 20 metres. Within the Park Road setback, the proposal will incorporate 5 canopy trees with additional shrub planting and providing climbing plants which are designed to extend over the feature canopy to shield the loading bay. Landscape mounding is provided along the setback to increase the height of the vegetation within the space as shown in the below image:



(Image above – cross section of mound landscaping to Park Road.)

Landscaping is limited along the northern boundary due to the large hard standing area at the rear of the building. There is however some landscape at the front section with an interface to Park Road and this provides a softening to the presentation of the driveway. It is not proposed to landscape along the western boundary due to the driveway adjoining the boundary. The interface at the western boundary is with a non sensitive use supporting a car yard and therefore softening of the built form is not needed here. It is also not uncommon in this industrial area for the sides and rear of sites to be void of significant vegetation.

It is noted that Council’s Traffic Engineers have recommended a standard condition requiring that all vehicle crossings are provided with a corner splay to provide a clear view of pedestrians on the footpath of the frontage road. This detail has not been provided on the development or landscaping plans and may result in modifications to the proposed landscaping, the spacing of canopy trees on the site but will need to be resolved at design stage of detailed plans.

Overall, the proposed landscaping will significantly increase visual amenity and contribute to the Garden City Character of the area.

Environmental Sustainability

Clause 22.13 (Environmentally Sustainable Development Policy) requires the Submission of a Sustainability Management Plan (SMP). A report was prepared by SDC Sustainable Development Consultants which includes a BESS assessment. The assessment has been undertaken to meet sustainability policy provisions contained within the Planning Policy Framework and Local Planning Policy and achieves a BESS score of 57% which exceeds the 50% requirement. The SMP also provides for a Green Travel Plan with initiatives to be implemented on site to encourage staff to walk, cycle or use public transport.

Car Parking, Traffic and Access

Parking Allocation

The requisite car parking spaces required under Clause 52.06 would be provided as shown in the following table:

Use	Net Floor Area	Clause 52.06 Requirement	Car spaces required	Car spaces provided
Warehouse	18,200 m ²	2 to each premises plus 1.5 to each 100m ² of net floor area	275	277

The proposal exceeds the Scheme requirement with the provision of 152 car parking spaces and 125 van parking spaces. SWEPT path diagrams and assessment confirms the layout has been designed in accordance with Clause 52.06. Vehicle turning movements are satisfactory and all vehicles can exit in a forward direction.

It is also noted that the changes to vehicle access and the removal of existing redundant crossings in Park Road will allow for up to five additional on-street car spaces on Park Road.

Vehicle Access

The development proposes new vehicle crossings, removal of existing and changes to one existing access on the site summarised as follows:

- Dandenong Road – Removal of the one existing and construction of two new crossings. This will also require the existing and redundant bus stop near the south-west corner of the site to be removed.
- Park Road – Removal of five existing crossovers and the construction of three new crossings.
- Dalgety Road – widening of the existing crossing.

The development provides for separate entry and egress access points for employees, delivery vans and larger trucks:

Entrance points:

- Vans only entrance via Dandenong Road Service Road (south-west)
- Contractors (smaller vehicles) entrance via Dandenong Road Service Road (south-east)
- Cars and vans entrance via Park Road (north-east)
- Trucks entrance via Dalgety Street (north-west)

Egress points:

- Contractors (smaller vehicles) exit via Park Road (south-east)
- Cars exit via Park Road (north-east)
- Vans exit via Park Road (east- adjacent to loading docks)
- Truck and van exit via Dalgety Street (north-west)

Council's Traffic Engineers raised concern with the location of the proposed northern crossing on Park Road and the proximity to the existing pedestrian crossing associated with the Oakleigh Recreation Centre. Concern was also raised with the potential for vans exiting onto Park Road and travelling north through residential areas to avoid travelling along Dandenong Road. In response the applicant's Traffic Engineers proposed additional measures to restrict the movements of vans and decrease the potential for additional traffic on Park Road. These measures are outlined as follows:

- The internal accessway adjacent to the outbound loading docks has been designed to restrict vans to turn left only and not allow egress to the site via the northern access point to Park Road. It is proposed to provide a boom gate that will only permit access via the northern access point to staff cars.
- The boom gate to the northern crossing will help reduce vehicle speed in and out of the site via this access point, as there is a requirement for vehicles to stop upon arriving at the boom gate.
- Additional signage is also proposed internally, which will warn staff departing the site to 'Beware of pedestrian crossing on Park Road'. Staff will also be informed that the usage of this access is restricted to employee staff vehicles only (no trucks permitted)
- Swept path assessments undertaken demonstrate that efficient and safe vehicle circulation is achieved within the site. The inbound loading docks entering from the Dalgety entrance, will require two compactor spaces to be vacant in order for the inbound dock to be accessible.

The above measures satisfied the concerns that had been raised by Council's Traffic team.

Traffic Generation

The data provided in the applicant's Traffic Impact Report concludes that the road network surrounding the site in the Dalgety Street/ Atkinsons Street, Dandenong Road / Park Road and Dandenong Road service road are well within traffic capacity limits. Traffic movements generated by the proposal are therefore expected to continue to be accommodated within the existing road network, with negligible implications.

Loading and Waste Collection

Waste collection has been resolved with waste storage areas provided at the rear of the ground floor of the development and access provided via the crossover in Dalgety Street. Waste collection vehicles of up to 12.5m in length (heavy rigid vehicles) can enter and exit the site in a forward direction.

Loading is to be managed with 23 docks along Park Road for smaller rigid vehicles for delivery of goods with all loading bays able to operate independently. All inbound loading will be to the rear of the site with 8 inbound docks to cater up to 19-metre-long articulated vehicles that can reverse into loading bays and leave in forward direction.

FINANCIAL IMPLICATIONS

There are no financial implications to this report.

POLICY IMPLICATIONS

There are no policy implications to this report.

CONSULTATION

Community consultation was not required.

SOCIAL IMPLICATIONS

There are no social implications to this report.

HUMAN RIGHTS CONSIDERATIONS

There are no human rights implications to this report.

GENDER IMPACT ASSESSMENT

A GIA was not completed because this agenda item is not a 'policy', 'program' or 'service'.

CONFLICT OF INTEREST

No officers involved in the preparation of this report have any direct or indirect conflict of interest in this matter.

CONCLUSION

The proposed development is consistent with the Monash Planning Scheme including relevant state, regional and local policies. The proposal results in significant investment in the site and provides for employment opportunities in the industrial sector of Monash. The design response is of high architectural quality with generous landscaping opportunities and is consistent with the Garden City Character and Industry and Business Development and Character Policies. The proposed development appropriately complements the surrounding built form and has purposely been designed to allow for the expansion of Woolworths operations.

Overall, the proposal is considered appropriate and it is recommended that a Planning Permit be issued subject to conditions.

ATTACHMENT LIST

1. Attachment 1 - Proposed Development Plans - 1575 Dandenong Rd & 9-15 Park Rd [7.1.2.1 - 13 pages]
2. Attachment 1 A - Landscape Plans - 1575 Dandenong Rd & 9-15 Park Rd [7.1.2.2 - 7 pages]
3. Attachment 2 - Aerial Photograph - 1575 Dandenong Rd & 9-15 Park Rd [7.1.2.3 - 1 page]
4. Attachment 3 - Zoning and Overlays Map - 1575 Dandenong Rd & 9-15 Park Rd [7.1.2.4 - 1 page]

SHEET LIST

TP00	COVER SHEET
TP01	SITE LOCALITY PLAN
TP02	EXISTING CONDITIONS
TP03	SITE & GROUND FLOOR
TP04	BASEMENT
TP05	MEZZANINE L1
TP06	MEZZANINE L2
TP07	ROOF PLAN
TP08	ELEVATIONS
TP09	SECTIONS
TP10	SHADOW DIAGRAMS
TP11	PERSPECTIVE IMAGES
TP12	EAST ELEVATION DETAIL



ARTISTS IMPRESSION ONLY
SCALE : NTS

CHADSTONE CFC
1575 DANDENONG RD, OAKLEIGH, VIC 3166

watson
young

Attachment 7.1.2.1 Attachment 1 - Proposed Development Plans - 1575 Dandenong Rd & 9-15 Park Rd



No.	DATE	REVISION	BY	CHK
A	18.04.2023	TOWN PLANNING ISSUE	JG	JF
B	20.06.2023	TOWN PLANNING ISSUE	JG	JF
C	20.05.2023	TOWN PLANNING ISSUE	JG	JF
D	18.12.2023	FOR APPROVAL	JG	JF
E	18.11.2023	FOR APPROVAL	JG	JF

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PROJECT:
CHADSTONE CFC
 1575 DANDENONG RD, OAKLEIGH, VIC 3166

TITLE:
 SITE LOCALITY PLAN



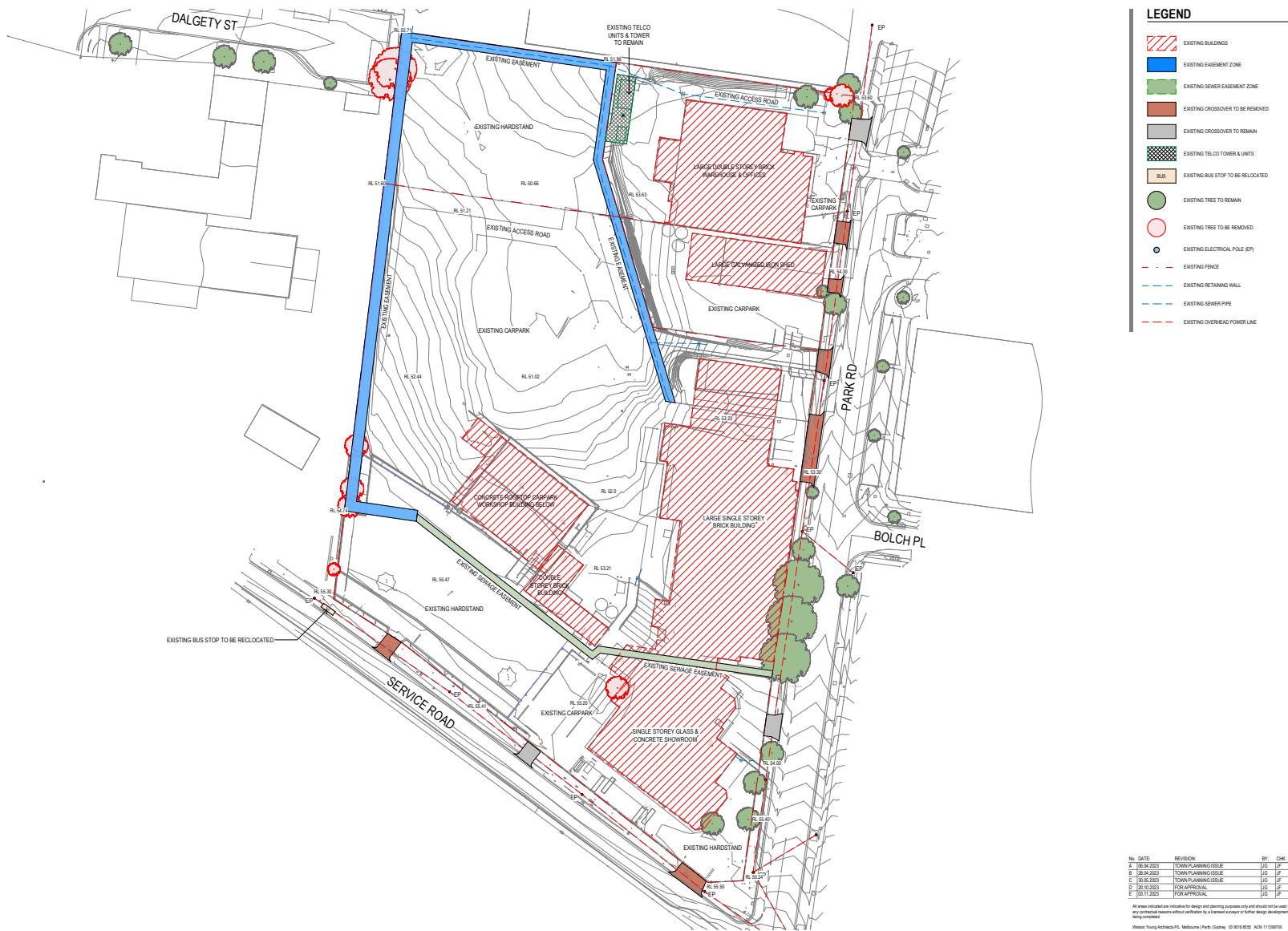
CLIENT:
 Woolworths Group

DATE: NOVEMBER, 2021
 DRAWN BY: JG
 SCALE: 1 : 1,000 @ A1
 SCALE: 1 : 2,000 @ A3

21154
TP01
E



Attachment 7.1.2.1 Attachment 1 - Proposed Development Plans - 1575 Dandenong Rd & 9-15 Park Rd



LEGEND

- EXISTING BUILDINGS
- EXISTING EASEMENT ZONE
- EXISTING SEWER EASEMENT ZONE
- EXISTING CROSSOVER TO BE REMOVED
- EXISTING CROSSOVER TO REMAIN
- EXISTING TELCO TOWER & UNITS
- EXISTING BUS STOP TO BE RELOCATED
- EXISTING TREE TO REMAIN
- EXISTING TREE TO BE REMOVED
- EXISTING ELECTRICAL POLE (EP)
- EXISTING FENCE
- EXISTING RETAINING WALL
- EXISTING SEWER PIPE
- EXISTING OVERHEAD POWER LINE

No.	DATE	REVISION	BY	CHK
A	18.04.2023	TOWN PLANNING ISSUE	JG	JF
B	25.04.2023	TOWN PLANNING ISSUE	JG	JF
C	30.05.2023	TOWN PLANNING ISSUE	JG	JF
D	18.02.2023	FOR APPROVAL	JG	JF
E	18.11.2023	FOR APPROVAL	JG	JF

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PROJECT:
CHADSTONE CFC
1575 DANDENONG RD, OAKLEIGH, VIC 3166

TITLE:
EXISTING CONDITIONS



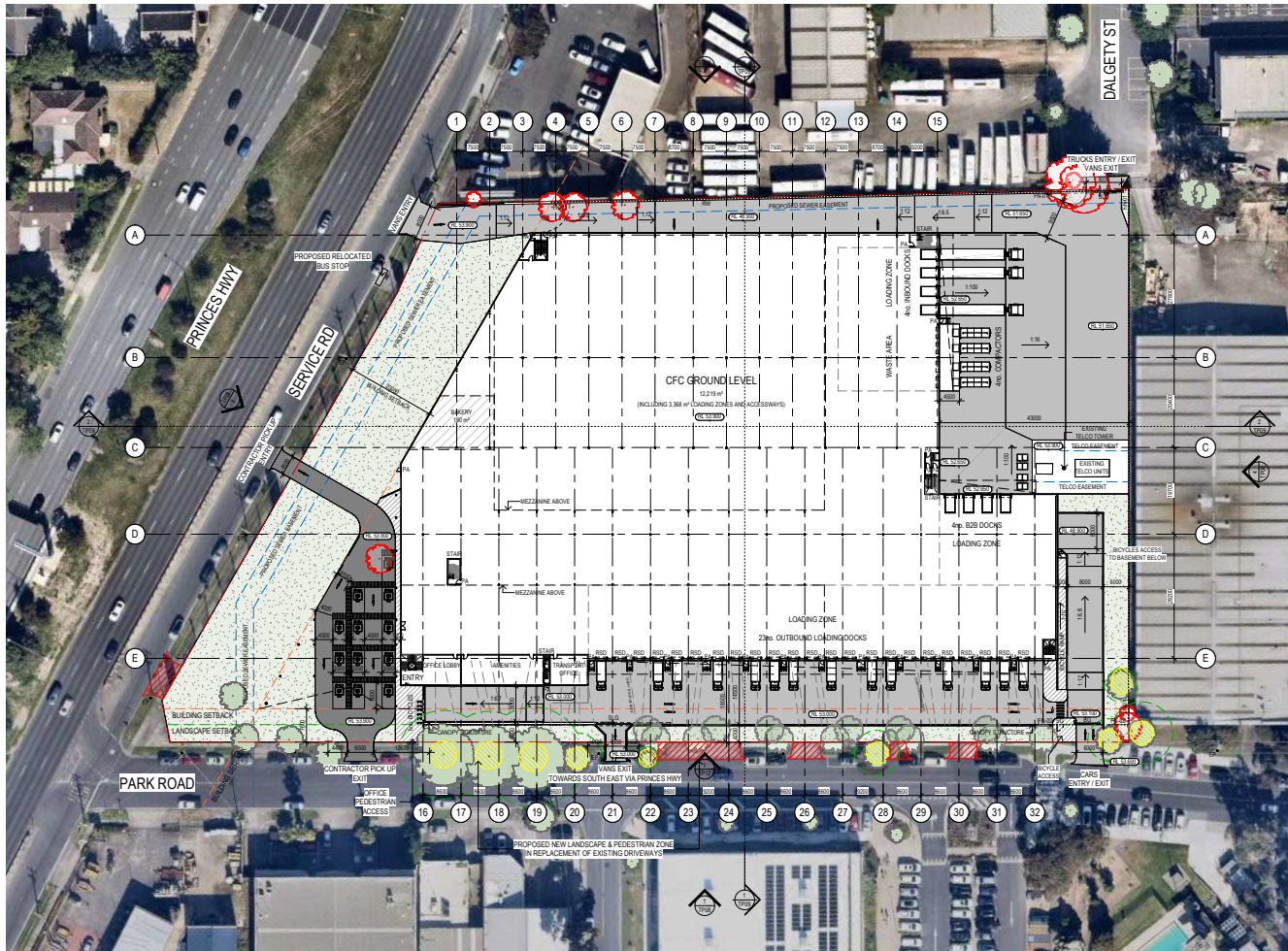
CLIENT:
 Woolworths Group

DATE: NOVEMBER, 2021
DRAWN BY: JG
SCALE: 1:500 @ A1
SCALE: 1:1,000 @ A3

PROJECT NO.	21154
DRAWING NO.	TP02
REVISION	E



Attachment 7.1.2.1 Attachment 1 - Proposed Development Plans - 1575 Dandenong Rd & 9-15 Park Rd



LEGEND

- [Symbol] INDICATES EXTENT OF HEAVY DUTY PAVEMENT TO CIVIL ENGINEERS DETAILS
- [Symbol] INDICATES EXTENT OF LIGHT DUTY PAVEMENT TO CIVIL ENGINEERS DETAILS
- [Symbol] CONCRETE PAVING WITH EXPOSED AGGREGATE FINISH OR SIMILAR
- [Symbol] AREA OF GRASS (LANDSCAPING REFER TO LANDSCAPE ARCHITECT'S DRAWINGS FOR LAYOUT AND DETAILS)
- FN-01 180mm HIGH BLACK VINYL COATED CYCLOPE WIRE FENCE, 3 BARBS WIRES TO 2100mm HIGH
- FN-02 180mm HIGH BLACK PAINTED METAL PICKET DRUMMAY / PALMER STYLED FENCING
- SG SWING GATE
- SLG SLIDING GATE
- PA PERSONAL ACCESS DOOR
- RSD ROLLER SHUTTER DOOR
- BG BOOM GATE
- SC SKIP CONTAINER
- EP ELECTRICAL POLE
- RW RETAINING WALL
- RWT RAIN WATER TANK
- [Green Circle] EXISTING TREE TO REMAIN
- [Red Circle] TREE TO BE REMOVED
- [Green Circle] PROPOSED NEW TREE
- [Green Circle] TREE PROTECTION ZONE
- [Yellow Circle] STRUCTURAL ROOT ZONE
- [Red Dashed Line] SITE BOUNDARY
- [Blue Dashed Line] PROPOSED EASEMENTS
- [Green Dashed Line] BUILDING SETBACK
- [Green Dashed Line] LANDSCAPE SETBACK
- [Red Hatched Box] EXISTING CROSSOVERS TO BE REPLACED WITH NEW LANDSCAPE & PEDESTRIAN ZONES

NOTES

ALL NEW CROSSOVERS IN ACCORDANCE WITH LOCAL COUNCIL REQUIREMENTS
 ALL PARKING SPACES IN ACCORDANCE WITH VICTORIAN PLANNING SCHEME
 ALL DISABLED PARKING SPACES IN ACCORDANCE WITH AUSTRALIAN STANDARD AS2890.1 (4.4 x 2.4m)
 SITE STORMWATER DRAINAGE IN ACCORDANCE WITH LOCAL AUTHORITY & COUNCIL REQUIREMENTS
 ALL RELATIVE LEVELS ARE SHOWN TO A.H.D. (Australian Height Datum)
 NET FLOOR AREA (NFA) IS DEFINED AS THE TOTAL FLOOR AREA OF ALL FLOORS OF ALL BUILDINGS ON A SITE. IT INCLUDES HALF THE WIDTH OF ANY PARTY WALLS AND THE FULL WIDTH OF ALL OTHER WALLS. IT DOES NOT INCLUDE THE AREA OF STAIRS, LOADING BAYS, ACCESSWAYS OR CARPARKING AREAS, OR ANY AREAS COVERED BY MACHINERY REQUIRED FOR AIR CONDITIONING, HEATING, POWER SUPPLY OR LIFTS ACCORDING TO VICTORIAN PLANNING SCHEME.
 GROSS FLOOR AREA (GFA) IS THE TOTAL FLOOR AREA OF A BUILDING, MEASURED FROM THE OUTSIDE OF EXTERNAL WALLS OR THE CENTRE OF PARTY WALLS, AND INCLUDES ALL ROOFED AREAS ACCORDING TO VICTORIAN PLANNING SCHEME.
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 ALL RELATIVE LEVELS ARE INDICATIVE ONLY AND SUBJECT TO FURTHER CIVIL DETAIL DESIGN AND MIGHT VARY ± 0.5m

DEVELOPMENT ANALYSIS

BUILDING AREA	GFA	NFA
OFFICE LOBBY	193 m ²	93 m ²
BASEMENT CARPARK	193 m ²	93 m ²
WAREHOUSE	12,049 m ²	8,808 m ²
OFFICE	293 m ²	292 m ²
GROUND FLOOR	12,342 m ²	9,100 m ²
WAREHOUSE	5,765 m ²	3,736 m ²
OFFICE	874 m ²	855 m ²
MEZZANINE L1	6,639 m ²	4,592 m ²
WAREHOUSE	7,438 m ²	3,891 m ²
OFFICE	543 m ²	524 m ²
MEZZANINE L2	7,981 m ²	4,415 m ²
TOTAL AREA	27,154 m ²	18,200 m ²

PARKING	AREA
CARS	152
VANS	125
MOTORBIKES	15
BICYCLE - BASEMENT PARKING	42
BICYCLE - GF VISITORS	6

SITE COVERAGE	AREA
SITE AREA	23,430 m ²
BUILDING FOOTPRINT	12,364 m ²
SITE COVERAGE	53.41%

No.	DATE	REVISION	BY	CHK.
B	18.04.2023	TOWN PLANNING SCHEME	JG	JF
C	19.05.2023	TOWN PLANNING SCHEME	JG	JF
D	19.05.2023	FOR APPROVAL	JG	JF
E	19.05.2023	FOR APPROVAL	JG	JF
F	19.11.2023	FOR APPROVAL	JG	JF

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PROJECT:
CHADSTONE CFC
 1575 DANDENONG RD, OAKLEIGH, VIC 3166

TITLE:
PROPOSED FLOOR PLAN
 SITE & GROUND FLOOR



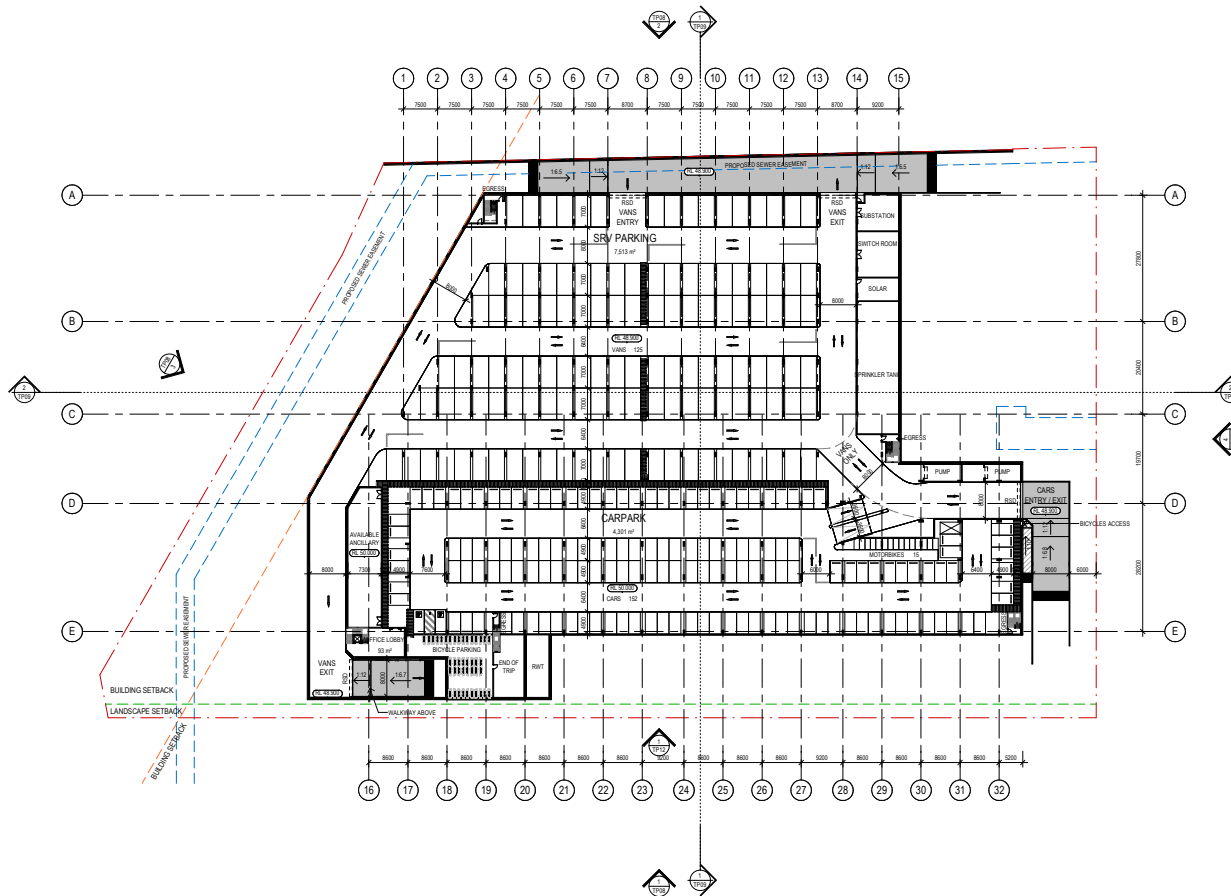
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DATE: NOVEMBER, 2021
 DRAWN BY: JG
 SCALE: 1:500 @ A1
 SCALE: 1:1,000 @ A3

21154
TP03
F



Attachment 7.1.2.1 Attachment 1 - Proposed Development Plans - 1575 Dandenong Rd & 9-15 Park Rd



NOTES

- ALL NEW CROSSOVERS IN ACCORDANCE WITH LOCAL COUNCIL REQUIREMENTS
- ALL PARKING SPACES IN ACCORDANCE WITH VICTORIAN PLANNING SCHEME
- ALL DISABLED PARKING SPACES IN ACCORDANCE WITH AUSTRALIAN STANDARD AS2880 (3.4m x 2.4m)
- SITE STORMWATER DRAINAGE IN ACCORDANCE WITH LOCAL AUTHORITY & COUNCIL REQUIREMENTS
- ALL RELATIVE LEVELS ARE SHOWN TO A.H.D. (Australian Height Datum)
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LEGEND

- INDICATES EXTENT OF HEAVY DUTY HARDSTAND TO CIVIL ENGINEERS DETAILS
- INDICATES EXTENT OF LIGHT DUTY PAVEMENT TO CIVIL ENGINEERS DETAILS
- CONCRETE PAVING WITH EXPOSED AGGREGATE FINISH OR SIMILAR
- AREA OF GRASS (LANDSCAPE REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPE LAYOUT AND DETAILS)
- FN 01 1800mm HIGH BLACK VINYL COATED CYCLOPE WIRE FENCE, 1.8M WIRE TO 1700mm HIGH
- FN 02 1800mm HIGH BLACK PAINTED METAL PICKET (DIPLOMAT / PALISADE STYLE) FENCING
- SG SWING GATE
- SLG SLIDING GATE
- PA PERSONAL ACCESS DOOR
- RSD ROLLER SHUTTER DOOR
- BG BOOM GATE
- SC SKOP CONTAINER
- EP ELECTRICAL POLE
- RW RETAINING WALL
- RWT RAIN WATER TANK
- EXISTING TREE TO REMAIN
- TREE TO BE REMOVED
- PROPOSED NEW TREE
- TREE PROTECTION ZONE
- STRUCTURAL ROOT ZONE
- SITE BOUNDARY
- PROPOSED EASEMENTS
- BUILDING SETBACK
- LANDSCAPE SETBACK
- EXISTING CROSSOVERS TO BE REPLACED WITH NEW LANDSCAPE & PEDESTRIAN ZONES

No.	DATE	REVISION	BY	CHK.
A	18.04.2023	TOWN PLANNING ISSUE	JG	JF
B	18.04.2023	TOWN PLANNING ISSUE	JG	JF
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PROJECT:
CHADSTONE CFC
 1575 DANDENONG RD, OAKLEIGH, VIC 3166

TITLE:
PROPOSED FLOOR PLAN
 BASEMENT



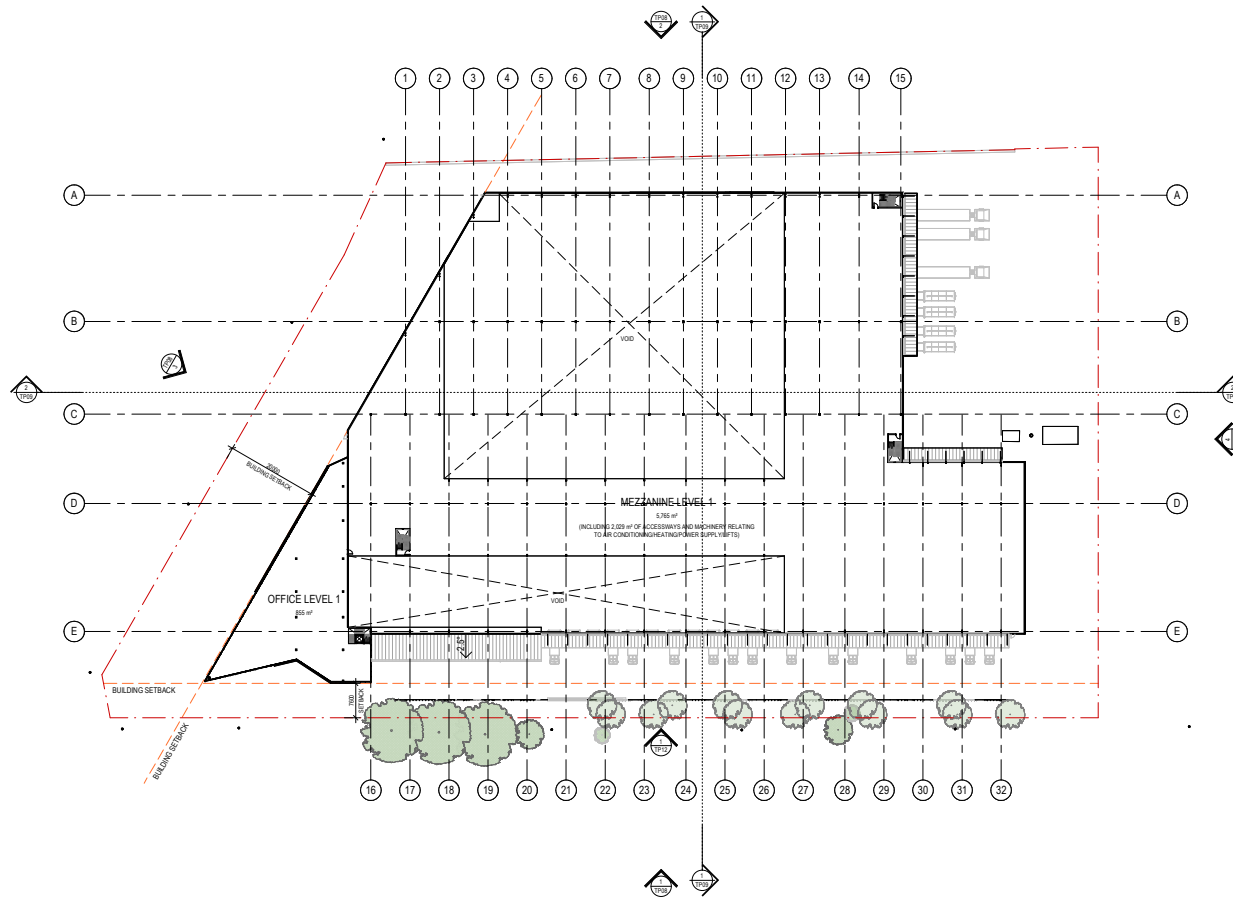
CLIENT:
Woolworths Group

DATE: NOVEMBER, 2021
 DRAWN BY: JG
 SCALE: 1: 500 @ A1
 SCALE: 1: 1,000 @ A3

21154
TP04
E



Attachment 7.1.2.1 Attachment 1 - Proposed Development Plans - 1575 Dandenong Rd & 9-15 Park Rd



NOTES

- ALL NEW CROSSOVERS IN ACCORDANCE WITH LOCAL COUNCIL REQUIREMENTS
- ALL PARKING SPACES IN ACCORDANCE WITH VICTORIAN PLANNING SCHEME
- ALL DISABLED PARKING SPACES IN ACCORDANCE WITH AUSTRALIAN STANDARD AS2890 (3.4m x 2.4m)
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LEGEND

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- INDICATES EXTENT OF LIGHT DUTY PAVEMENT TO CIVIL ENGINEERS DETAILS
- CONCRETE PAVING WITH EXPOSED AGGREGATE FINISH OR SIMILAR
- AREA OF GRASS (LANDSCAPING REFERS TO LANDSCAPE ARCHITECTS DRAWINGS FOR LANDSCAPE LAYOUT AND DETAILS)
- FN 01** 1800mm HIGH BLACK VINYL COATED CYCLOPE WIRE FENCE, 3 BARS WIRE TO 1200mm HIGH
- FN 02** 1800mm HIGH BLACK PAINTED METAL PICKET (DIPLOMAT / PALISADE STYLE) FENCING
- SG** SWING GATE
- SLG** SLIDING GATE
- PA** PERSONAL ACCESS DOOR
- RSD** ROLLER SHUTTER DOOR
- BG** BOOM GATE
- SC** SKOP CONTAINER
- EP** ELECTRICAL POLE
- RW** RETAINING WALL
- RWT** RAIN WATER TANK
- EXISTING TREE TO REMAIN
- TREE TO BE REMOVED
- PROPOSED NEW TREE
- TREE PROTECTION ZONE
- STRUCTURAL ROOT ZONE
- SITE BOUNDARY
- PROPOSED EASEMENTS
- BUILDING SETBACK
- LANDSCAPE SETBACK
- EXISTING CROSSOVERS TO BE REPLACED WITH NEW LANDSCAPE & PEDESTRIAN ZONES

NO.	DATE	REVISION	BY	CHK.
A	18.04.2023	TOWN PLANNING ISSUE	JG	JF
B	18.04.2023	TOWN PLANNING ISSUE	JG	JF
C	18.05.2023	TOWN PLANNING ISSUE	JG	JF
D	18.05.2023	FOR APPROVAL	JG	JF
E	18.11.2023	FOR APPROVAL	JG	JF

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PROJECT:
CHADSTONE CFC
 1575 DANDENONG RD, OAKLEIGH, VIC 3166

TITLE:
PROPOSED FLOOR PLAN
 MEZZANINE L1



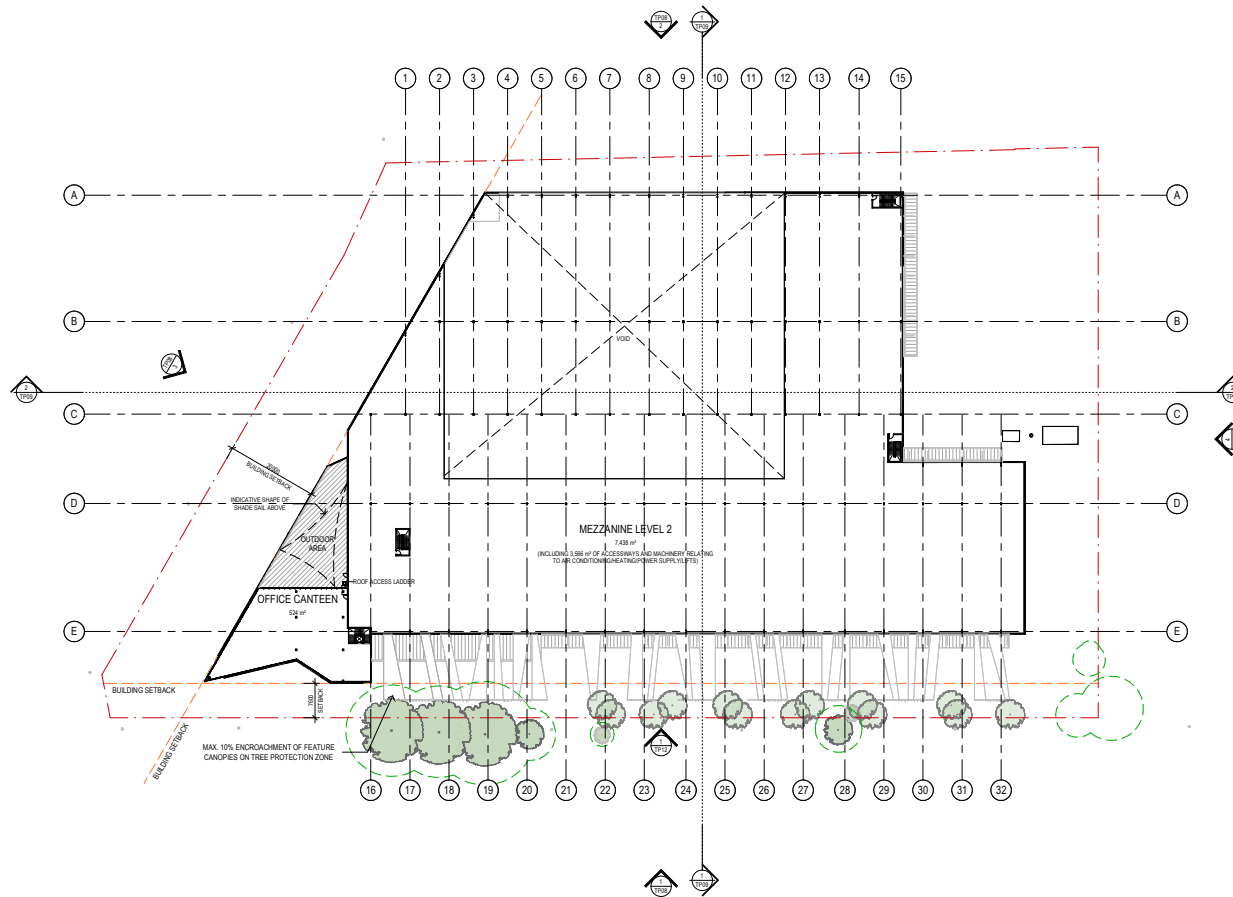
CLIENT:
 Woolworths Group

DATE: NOVEMBER, 2021
 DRAWN BY: JG
 SCALE: 1: 500 @ A1
 SCALE: 1: 1,000 @ A3

21154
TP05
E



Attachment 7.1.2.1 Attachment 1 - Proposed Development Plans - 1575 Dandenong Rd & 9-15 Park Rd



NOTES

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- BUILDING SETBACK
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NO.	DATE	REVISION	BY	CHK.
A	18.04.2023	TOWN PLANNING ISSUE	JG	JF
B	19.04.2023	TOWN PLANNING ISSUE	JG	JF
C	19.05.2023	TOWN PLANNING ISSUE	JG	JF
D	19.05.2023	FOR APPROVAL	JG	JF
E	19.11.2023	FOR APPROVAL	JG	JF

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PROJECT:
CHADSTONE CFC
 1575 DANDENONG RD, OAKLEIGH, VIC 3166

TITLE:
PROPOSED FLOOR PLAN
 MEZZANINE L2

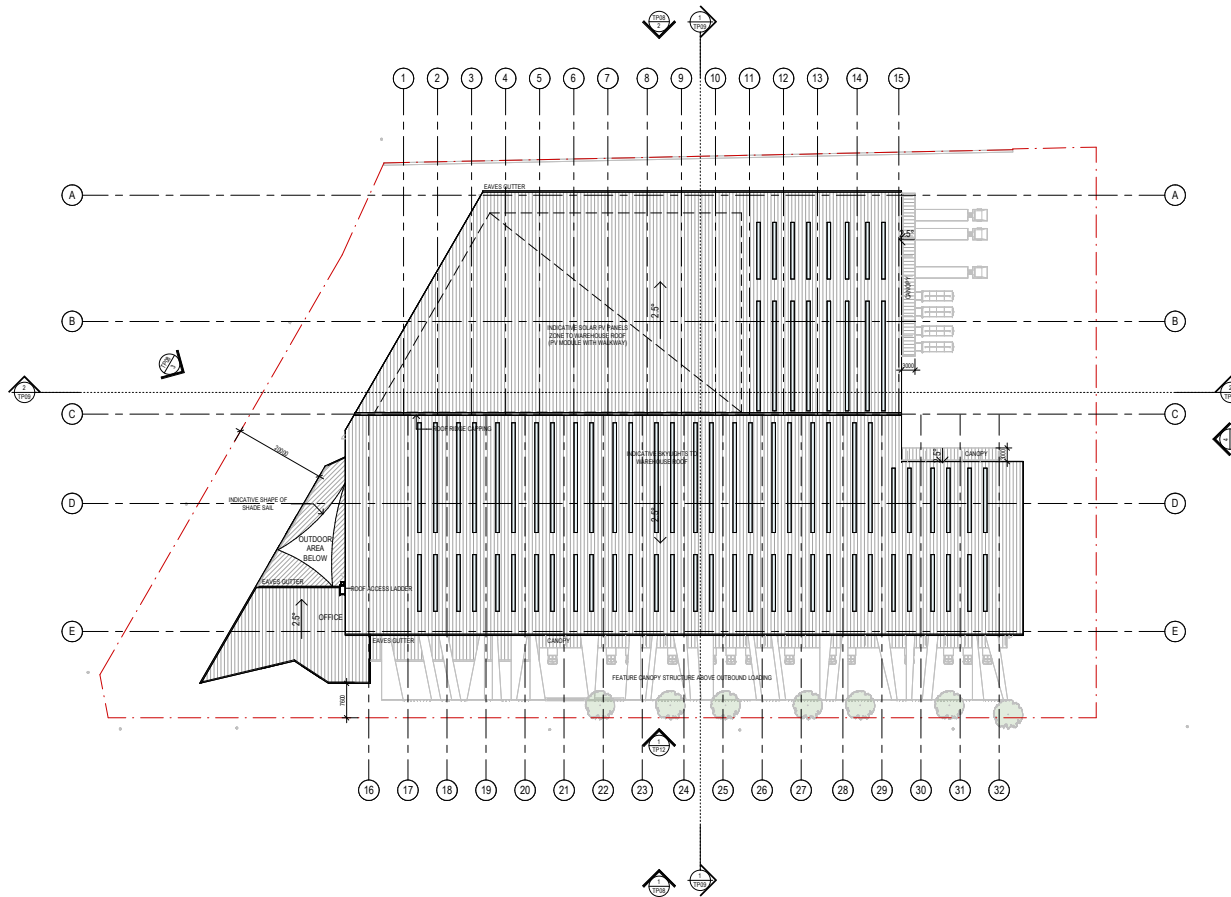


DATE: NOVEMBER, 2021
 DRAWN BY: JG
 SCALE: 1: 500 @ A1
 SCALE: 1: 1,000 @ A3

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TP06
E



Attachment 7.1.2.1 Attachment 1 - Proposed Development Plans - 1575 Dandenong Rd & 9-15 Park Rd



No.	DATE	REVISION	BY	CHK.
A	18.04.2023	TOWN PLANNING ISSUE	JG	JF
B	20.04.2023	TOWN PLANNING ISSUE	JG	JF
C	20.05.2023	TOWN PLANNING ISSUE	JG	JF
D	18.02.2023	FOR APPROVAL	JG	JF
E	18.11.2023	FOR APPROVAL	JG	JF

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PROJECT:
CHADSTONE CFC
 1575 DANDENONG RD, OAKLEIGH, VIC 3166

TITLE:
PROPOSED FLOOR PLAN
ROOF PLAN



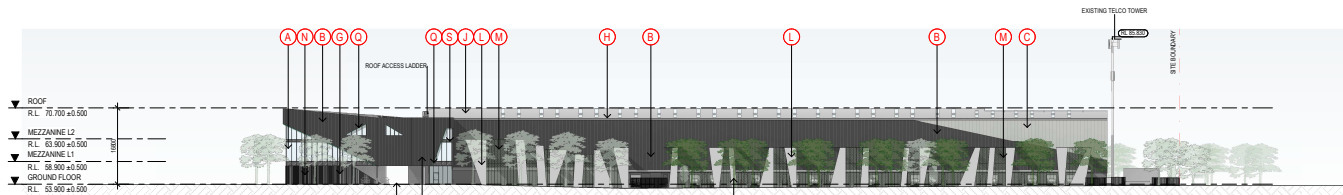
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DATE: NOVEMBER, 2021
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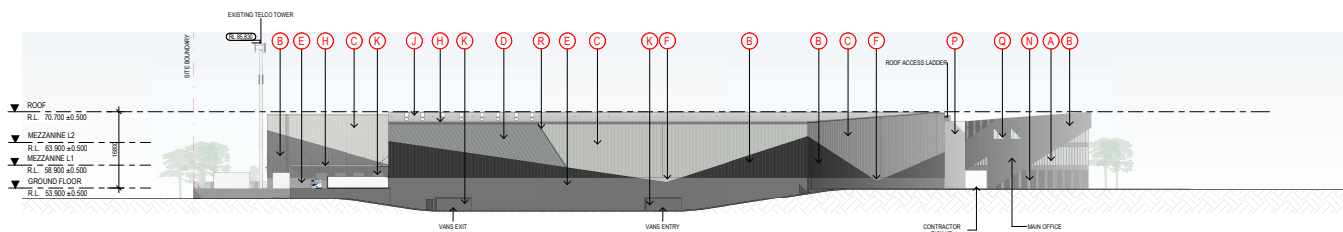
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DRAWING NO. TP07	REVISION E



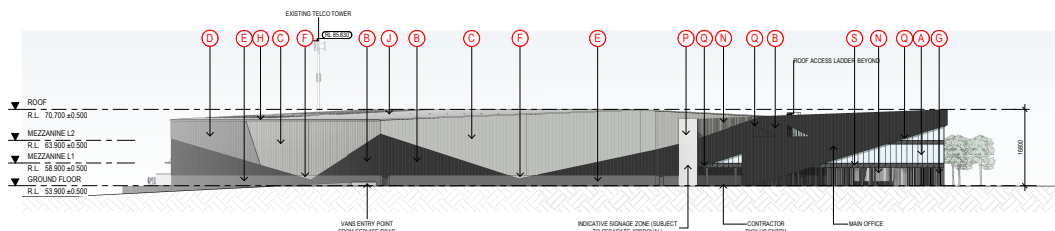
Attachment 7.1.2.1 Attachment 1 - Proposed Development Plans - 1575 Dandenong Rd & 9-15 Park Rd



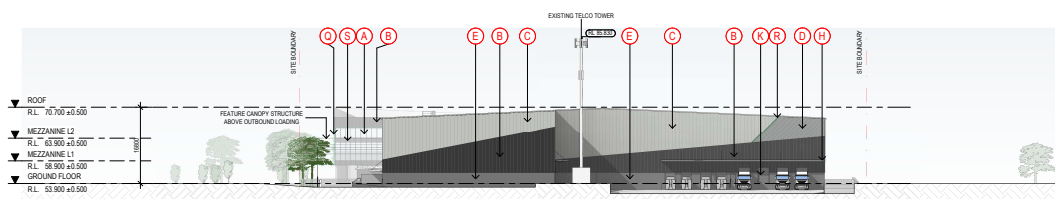
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SCALE: 1 : 500



WEST ELEVATION
SCALE: 1 : 500



SOUTH ELEVATION
SCALE: 1 : 500



NORTH ELEVATION
SCALE: 1 : 500

EXTERNAL FINISHES

A	ALUMINIUM FRAMED GLAZING	CLEAR GLASS
B	COLORBOND WALL CLADDING	COLORBOND MONUMENT
C	COLORBOND WALL CLADDING	COLORBOND SHALE GREY
D	COLORBOND WALL CLADDING	COLORBOND WINDSPRAY
E	PRECAST CONCRETE PANEL, TEXTURE PAINT	COLORBOND BASALT
F	PRECAST CONCRETE PANEL, TEXTURE PAINT	COLORBOND SHALE GREY
G	CONCRETE SLAB & COLUMNS	EXPOSED CONCRETE
H	GUTTER, FASCIA & DOWNPIPES	TO MATCH WALL CLADDING BEHIND
J	ROOF CLADDING	ZINCALUME
K	ROLLER & PA DOORS	COLORBOND BASALT
L	FEATURE LIGHT WEIGHT CANOPY ABOVE DOCKS	TENSION HEAVY DUTY FABRIC LIGHT GREY
M	CLIMBERS SUPPORT STRUCTURE	WELDED WIRE STEEL MESH
N	METAL SCREEN BATTENS	TIMBER COLOR
P	SIGNAGE	INDICATIVE SIGNAGE ZONE
Q	OFFICE CLADDING EDGES (AS INDICATED)	GREEN LED STRIP
R	WAREHOUSE CLADDING EDGES	COLORBOND COTTAGE GREEN
S	ALUMINIUM FRAMED GLAZING TO OFFICE SLAB EDGE	COLOURED-BACK GLAZING COLORBOND BASALT

PROJECT:
CHADSTONE CFC
1575 DANDENONG RD, OAKLEIGH, VIC 3166

TITLE:
ELEVATIONS



DATE: NOVEMBER, 2021
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SCALE: 1 : 500 @ A1
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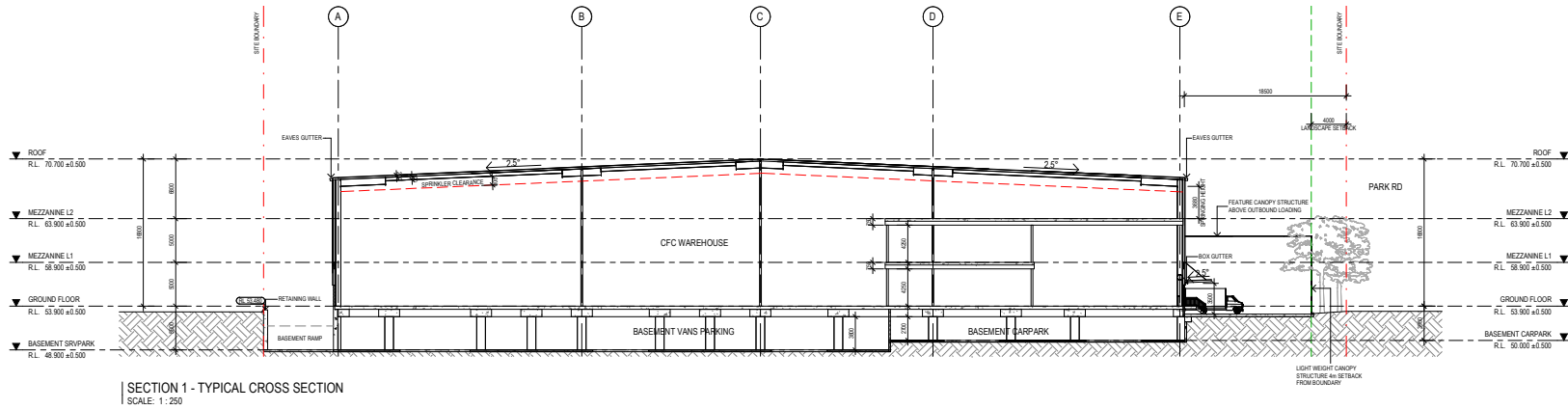
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No.	DATE	REVISION	BY	CHK.
A	18.04.2021	TOWN PLANNING ISSUE	JG	JF
B	18.04.2021	TOWN PLANNING ISSUE	JG	JF
C	18.05.2021	TOWN PLANNING ISSUE	JG	JF
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E	18.11.2021	FOR APPROVAL	JG	JF

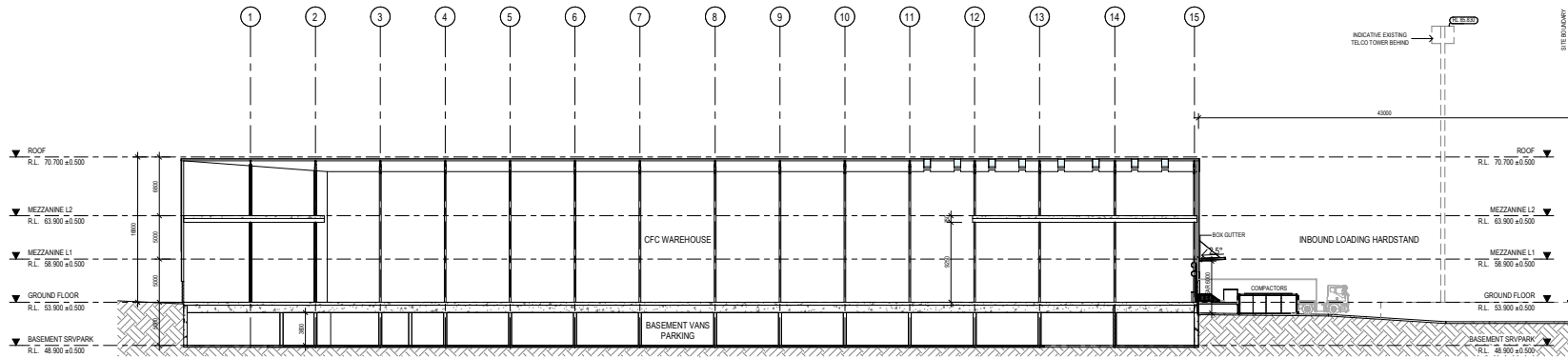
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Attachment 7.1.2.1 Attachment 1 - Proposed Development Plans - 1575 Dandenong Rd & 9-15 Park Rd



SECTION 1 - TYPICAL CROSS SECTION
SCALE: 1 : 250



SECTION 2 - TYPICAL LONG SECTION
SCALE: 1 : 250

NO.	DATE	REVISION	BY	CHK.
B	18.04.2023	TOWN PLANNING ISSUE	JG	JF
C	20.05.2023	TOWN PLANNING ISSUE	JG	JF
D	19.09.2023	FOR APPROVAL	JG	JF
E	18.12.2023	FOR APPROVAL	JG	JF
F	18.11.2023	FOR APPROVAL	JG	JF

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PROJECT:
CHADSTONE CFC
1575 DANDENONG RD, OAKLEIGH, VIC 3166

TITLE:
SECTIONS



DATE: NOVEMBER 2021
DRAWN BY: JG
SCALE: 1: 250 @ A1
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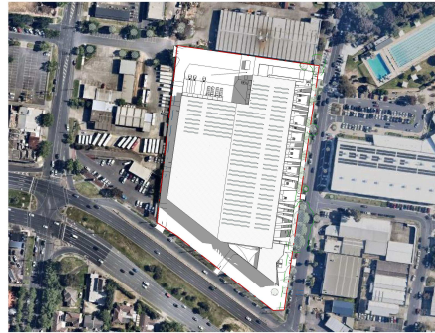
21154
TP09
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Attachment 7.1.2.1 Attachment 1 - Proposed Development Plans - 1575 Dandenong Rd & 9-15 Park Rd



SPRING EQUINOX 9AM
SCALE: 1:2000



SUMMER SOLSTICE 9AM
SCALE: 1:2000



WINTER SOLSTICE 9AM
SCALE: 1:2000



SPRING EQUINOX 12PM
SCALE: 1:2000



SUMMER SOLSTICE 12PM
SCALE: 1:2000



WINTER SOLSTICE 12PM
SCALE: 1:2000



SPRING EQUINOX 3PM
SCALE: 1:2000



SUMMER SOLSTICE 3PM
SCALE: 1:2000



WINTER SOLSTICE 3PM
SCALE: 1:2000

No.	DATE	REVISION	BY	CHK.
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PROJECT:
CHADSTONE CFC
1575 DANDENONG RD, OAKLEIGH, VIC 3166

TITLE:
SHADOW DIAGRAMS



DATE: NOVEMBER, 2021
DRAWN BY: JG
SCALE: 1:2,000 @ A1
SCALE: 1:4,000 @ A3

JOB NO	21154
DRAWING	TP10
REVISION	E



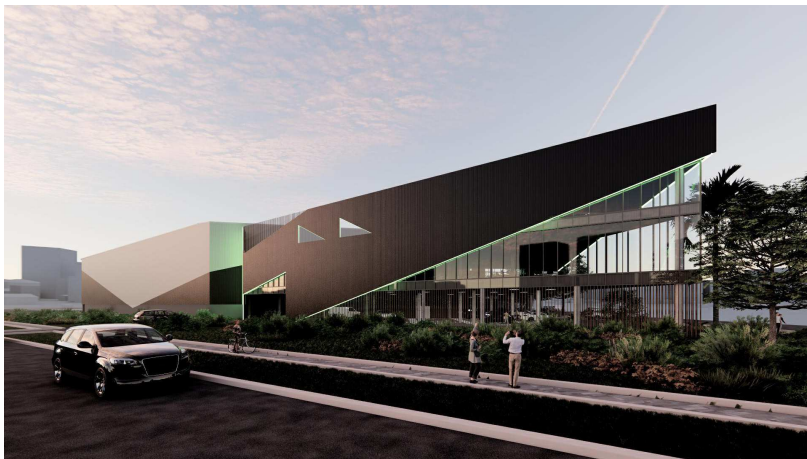
Attachment 7.1.2.1 Attachment 1 - Proposed Development Plans - 1575 Dandenong Rd & 9-15 Park Rd



PARK RD OFFICE PERSPECTIVE
ARTISTS IMPRESSION ONLY



PARK RD OUTBOUND LOADING DOCKS
ARTISTS IMPRESSION ONLY



SERVICE ROAD OFFICE PERSPECTIVE
ARTISTS IMPRESSION ONLY



INBOUND & B2B LOADING DOCKS
ARTISTS IMPRESSION ONLY

No.	DATE	REVISION	BY	CHK.
A	18.04.2023	TOWN PLANNING ISSUE	JG	JF
B	20.04.2023	TOWN PLANNING ISSUE	JG	JF
C	20.05.2023	TOWN PLANNING ISSUE	JG	JF
D	20.02.2023	FOR APPROVAL	JG	JF
E	10.11.2023	FOR APPROVAL	JG	JF

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PROJECT:
CHADSTONE CFC
1575 DANDENONG RD, OAKLEIGH, VIC 3166

TITLE:
PERSPECTIVE IMAGES

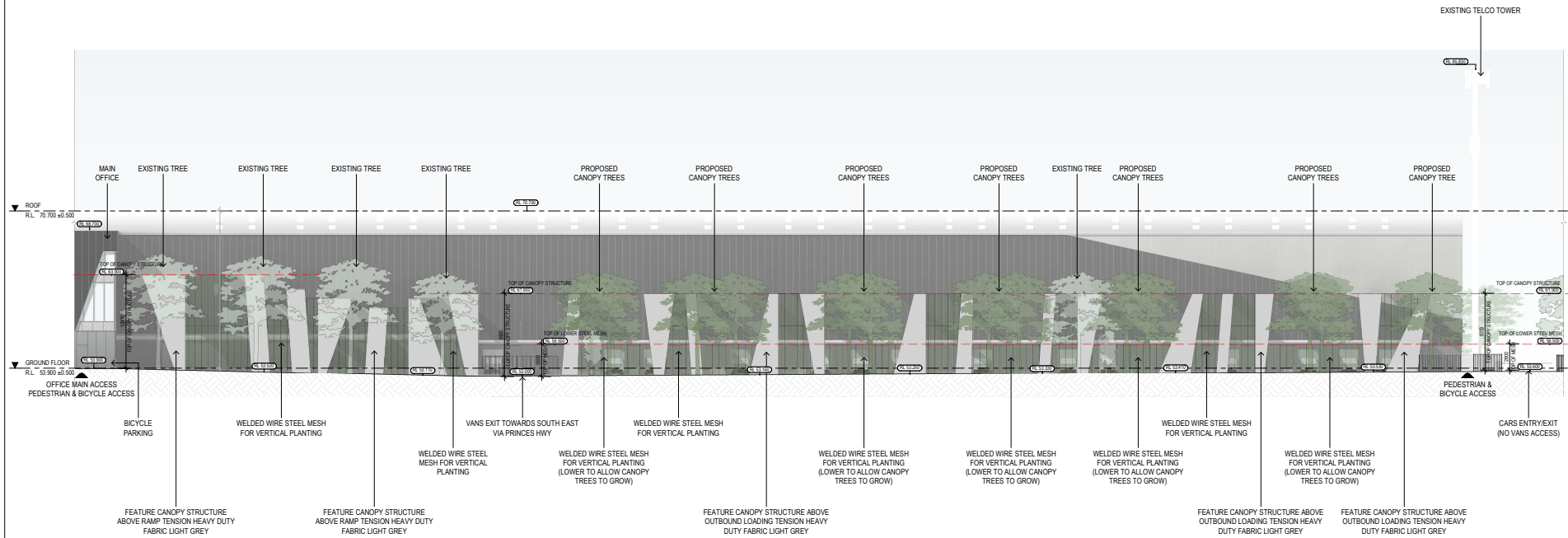
CLIENT:
Woolworths Group

DATE: NOVEMBER, 2021
DRAWN BY: JG
SCALE: NTS @ A1
SCALE: NTS @ A3

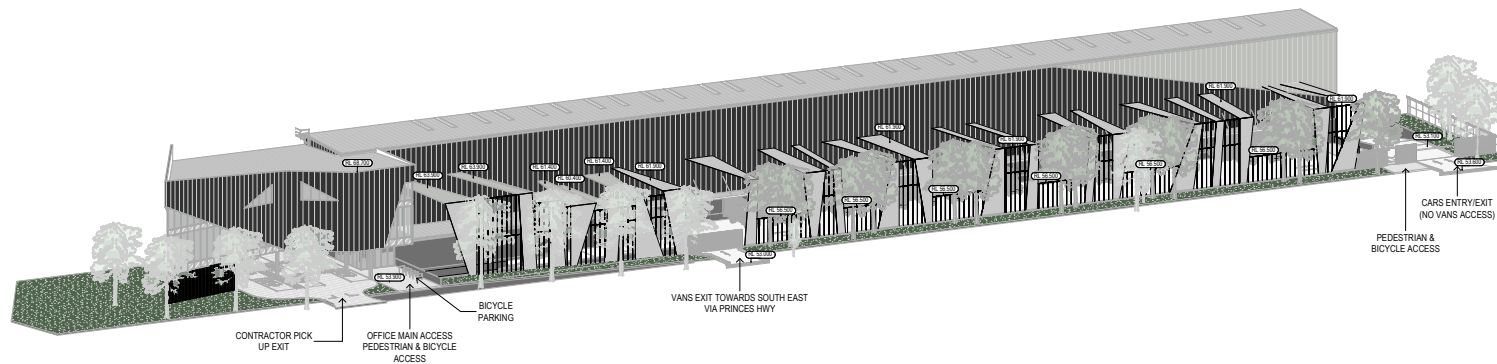
21154
TP11 E



Attachment 7.1.2.1 Attachment 1 - Proposed Development Plans - 1575 Dandenong Rd & 9-15 Park Rd



EAST ELEVATION DETAIL ALONG PARK ROAD
SCALE: 1:200



PINDICATIVE FEATURE CANOPY PERSPECTIVE ALONG PARK ROAD
SCALE: NTS

No.	DATE	REVISION	BY	CHK.
A	10/10/2023	FOR APPROVAL	JG	JF
B	05/11/2023	FOR APPROVAL	JG	JF

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PROJECT:
CHADSTONE CFC
1575 DANDENONG RD, OAKLEIGH, VIC 3166

TITLE:
EAST ELEVATION DETAIL

CLIENT:
 Woolworths Group

DATE: NOVEMBER, 2021
DRAWN BY: JG
SCALE: 1:200 @ A1
SCALE: 1:400 @ A3

21154
TP12
B

1575 DANDENONG ROAD OAKLEIGH

LANDSCAPE - CONTRACTOR PICK UP

REV - C

17.11.2023



DRAWING SCHEDULE

Landscape Plans

Ground Floor	DWG No: 1-A
--------------	-------------

Ground Floor - Indicative Section	DWG No: 1-B
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Landscape Details

Materiality and Planting	DWG No: 2-A
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Typical Details	DWG No: 3-A
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1575 DANDENONG ROAD, OAKLEIGH

LANDSCAPE PLANS

1575 DANDENONG ROAD, OAKLEIGH

Ground Floor

Landscape Plan



1575 DANDENONG ROAD, OAKLEIGH

LEGEND	
Site Boundary	
Existing Crossover	
Proposed Crossover - van entry only	
Proposed relocate sewer easement - to be confirmed	
Shade Sail (indicative only) - to future detail	
Proposed Trees - Refer to Planting Schedule	
Proposed Climber - Refer to Planting Schedule - To be mulched and irrigated	
Proposed Shrubs - Refer to Planting Schedule - To be mulched and irrigated	
Proposed Groundcovers - Refer to Planting Schedule - To be mulched and irrigated	
Proposed Medium-large Shrubs and Groundcover Mix - Refer to Planting Schedule - To be mulched and irrigated	
Proposed Asphalt - Refer to Architect's plan	
Existing Tree (to be retained)	
Existing Tree (to be removed)	
Existing Tree Protection Zone (TPZ)	
Existing Structural Root Zone (SRZ)	
Basement Boundary	
Roof Boundary (incl. Mezzanine)	
Existing crossover - To be removed and made good with naturestrip	



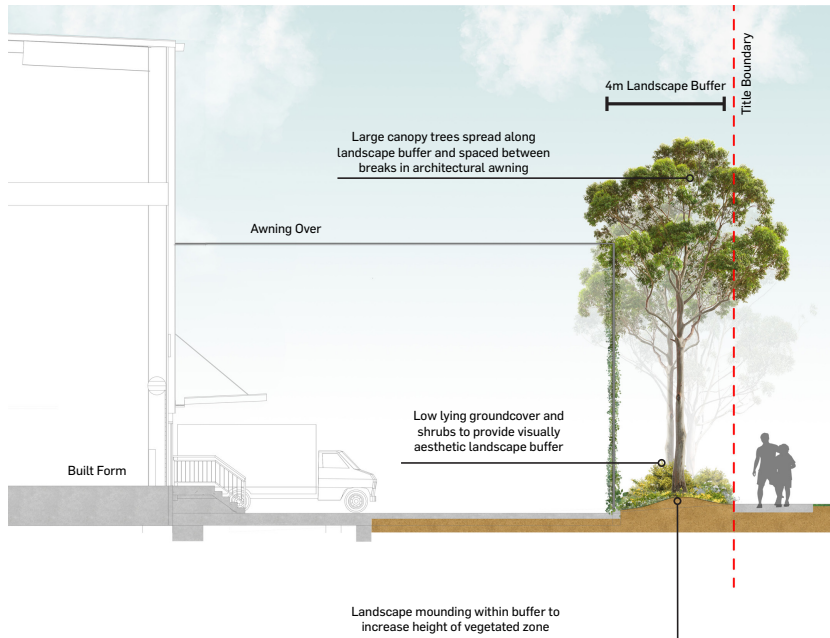
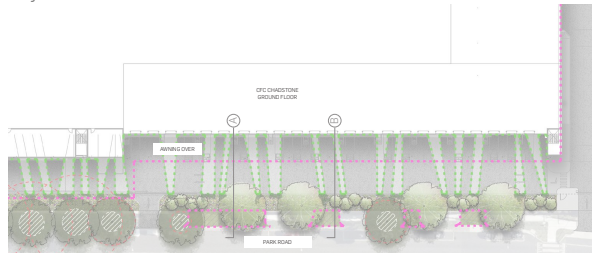
DWG NO: 1-A
 DATE: 17.11.2023
 1:200 @ A3
 JOB NO: P0041028
 REV: C



Ground Floor

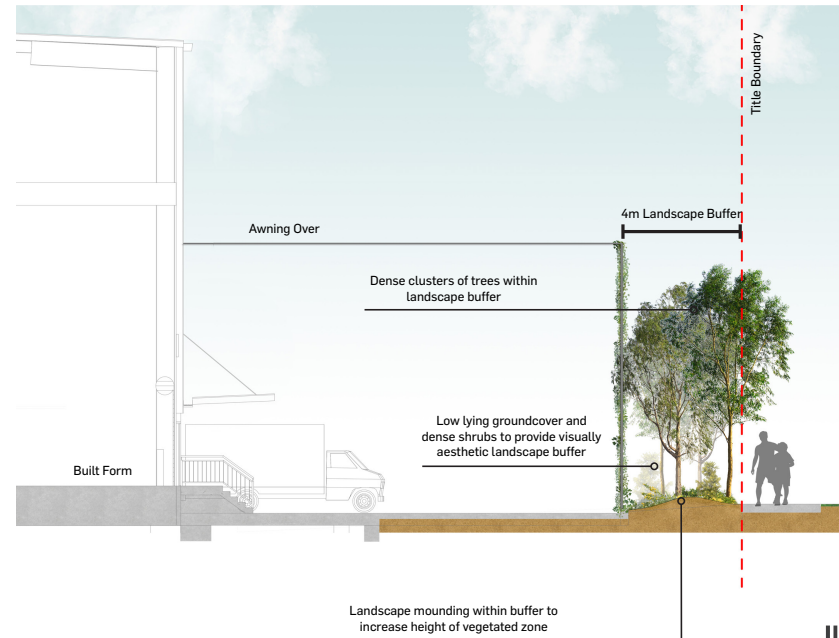
Indicative Landscape Sections

Key Plan



Section AA - Indicative Landscape Buffer with Large Canopy Trees

1575 DANDENONG ROAD, OAKLEIGH



Section BB - Indicative Landscape Buffer with Dense Tree Clusters



DWG NO: 1-B
 DATE: 17.11.2023
 JOB NO: P0041028
 REV: C

1:300 @ A3



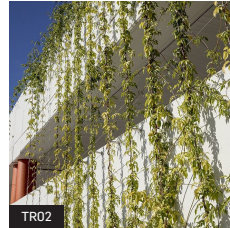
Materiality and Planting

Indicative Landscape Palettes

PRECEDENT IMAGERY

CLIMBING TRELLIS

- TR01** Stainless Steel Mesh
- TR02** Cable Trellis



PLANT SCHEDULE

TREES	QTY
COR cit <i>Corymbia citriodora</i> 'Scentuous'	12
COR mac <i>Corymbia maculata</i>	2
EUC mel <i>Eucalyptus melliodora</i>	18
SYZ aus <i>Syzygium australe</i> 'Pinnacle'	24

SHRUBS (To be selected from)

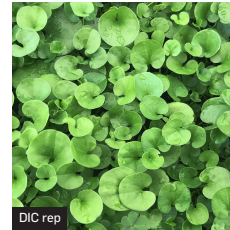
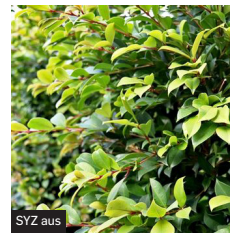
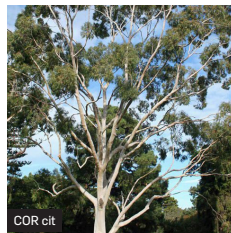
- ACA myr** *Acacia myrtifolia**
- BUR spi** *Bursaria spinosa**
- COR ref** *Correa reflexa**
- GOO ova** *Goodenia ovata**
- LEP con** *Leptospermum continentale**

GROUNDCOVERS (To be selected from)

- BOS pro** *Bossiaea prostrata**
- CHR sem** *Chrysocephalum semipapposum**
- DIC rep** *Dichondra repens**
- KEN pro** *Kennedia prostrata**

CLIMBER

- CLE mic** *Clematis microphylla**



* Denoted plants categorised as indigenous to the local area as per 'City of Monash, Monash Gardens for Wildlife 2015'

1575 DANDENONG ROAD, OAKLEIGH

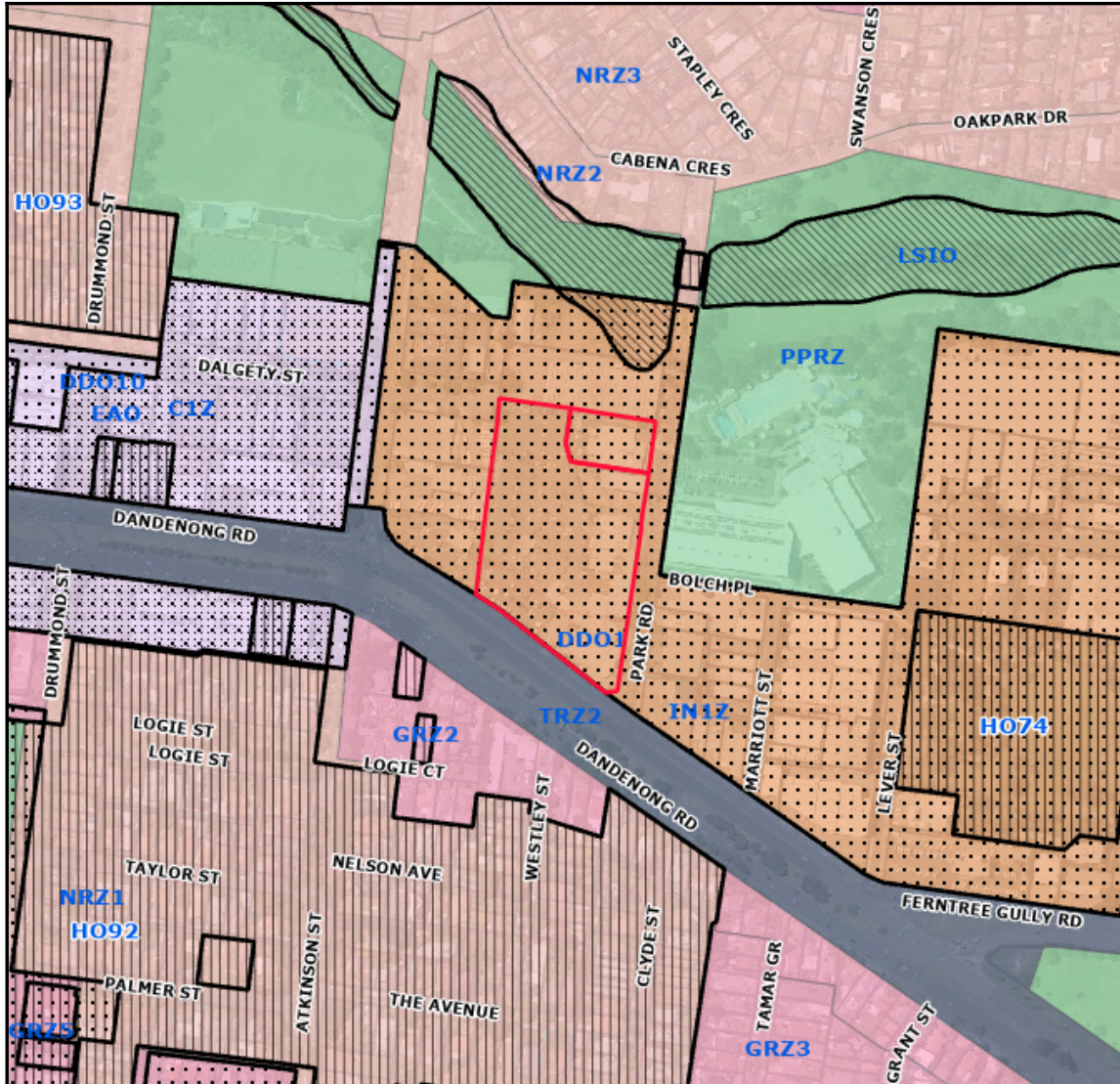


DWG NO: 2-A
DATE: 17.11.2023
JOB NO: P0041028
REV: C





Planning Overlays and Zones



Legend

Planning Zones

C1Z	GRZ10	GRZ7	MUZ2	PCRZ	PUZ4	RGZ1	SUZ3
C2Z	GRZ2	GRZ8	NRZ1	PPRZ	PUZ5	RGZ3	SUZ4
CA	GRZ3	GRZ9	NRZ2	PUZ1	PUZ6	RGZ4	SUZ5
CD21	GRZ5	IN1Z	NRZ3	PUZ2	PUZ7	RGZ5	SUZ6
GRZ1	GRZ6	MUZ	NRZ4	PUZ3	RDZ1	SUZ2	UFZ

Planning Overlays

DDO	EAO	IPO	NCO	PO
DPO	HO	LSIO / SBO	PAO	VPO



Address

1575 Dandenong Road & 9-15 Park Road OAKLEIGH VIC 3166

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