

**1.2 1-5 BOGONG AVENUE, GLEN WAVERLEY
PART DEMOLITION AND CONSTRUCTION OF BUILDINGS AND WORKS TO AN
EXISTING CAR PARKING FACILITY (ADDITIONAL FOUR LEVELS) AND DISPLAY OF
ELECTRONIC SIGNAGE
(TPA/54087)**

EXECUTIVE SUMMARY:

This application proposes an extension of the existing Council owned car parking facility and display of signage. Katz Architecture are the applicant acting on behalf of the owner, City of Monash.

The application was subject to public notification. One (1) objection to the proposal has been received.

Key issues to be considered relate to building height and scale in relation to neighbourhood character, traffic generation and amenity impacts.

This report assesses the proposal against the provisions of the Monash Planning Scheme including the relevant state, regional and local planning policy framework and issues raised by objectors.

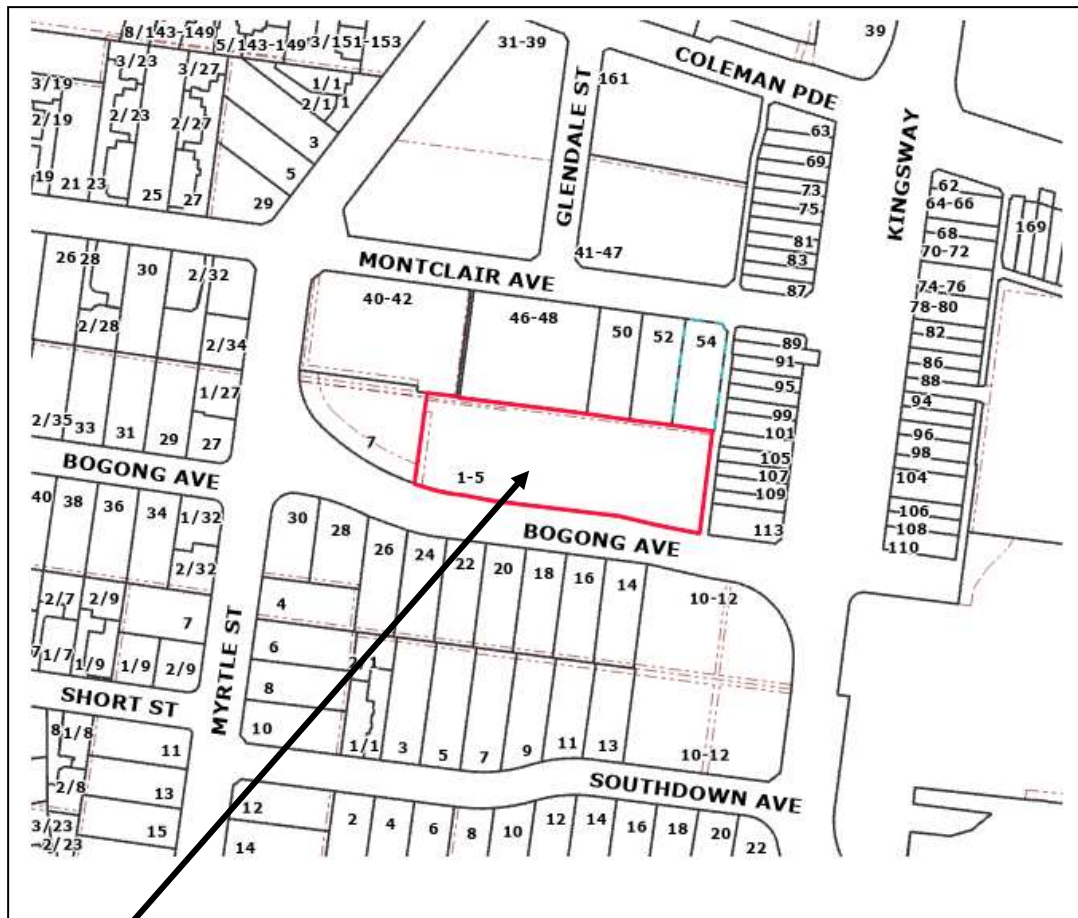
The reason for presenting this report to Council is the proposed development cost is greater than \$3.5 Million.

The proposed development is considered appropriate having regard to the relevant provisions of the Monash Planning Scheme. It is recommended that Council resolve to issue a Notice of Decision to Grant a Planning Permit, subject to conditions.

RESPONSIBLE DIRECTOR:	Peter Panagakos
RESPONSIBLE MANAGER:	Catherine Sherwin
RESPONSIBLE PLANNER:	Anne Maree Roberts / Alexandra Wade
WARD:	Glen Waverley
PROPERTY ADDRESS:	1-5 Bogong Avenue, Glen Waverley
EXISTING LAND USE:	Car Park
PRE-APPLICATION MEETING:	Yes
NUMBER OF OBJECTIONS:	One (1)
ZONING:	Commercial 1 Zone
OVERLAY:	Design and Development Overlay, Schedule 12 Specific Controls Overlay Schedule 14

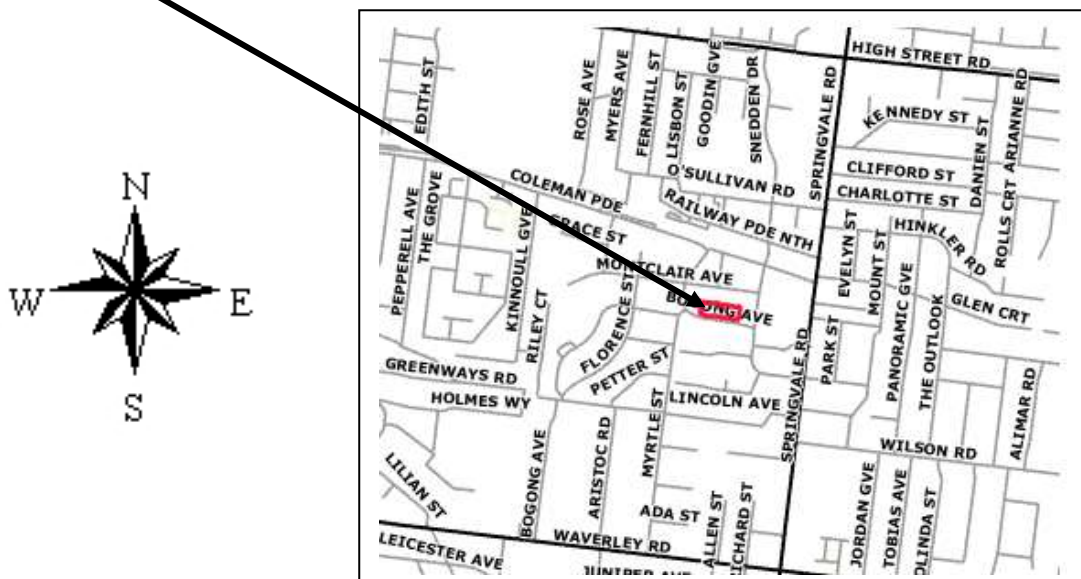
	Specific Controls Overlay Schedule 15
RELEVANT CLAUSES: <u>Planning Policy Framework</u> Clause 11.01-1R- Settlement – Metropolitan Melbourne Clause 11.02-1S- Supply of Urban Land Clause 11.03-1R Activity Centres Metropolitan Melbourne Clause 13.07-1S- Land Use Compatibility Clause 15.01-1S&R- Urban Design Clause 15.01-2S- Building Design Clause 15.01-4S & R- Healthy Neighbourhoods Clause 15.01-5S- Neighbourhood Character Clause 15.02-1S- Energy and Resource Efficiency Clause 18.02-1S & R- Sustainable Personal Transport Clause 18.02-2R– Principal Public Transport Network Clause 18.02-4S- Car Parking Clause 19.03-3S- Integrated Water Management	<u>Local Planning Policy Framework</u> Clause 21- Municipal Strategic Statement) Clause 21.06 – Major Activity and Neighbourhood Centres Clause 21.08- Transport and Traffic Clause 21.13- Sustainability and Environment Clause 22.03- Industry and business development and character policy Clause 22.04- Stormwater Management Policy Clause 22.08 – Outdoor Advertising Policy Clause 22.14 – Glen Waverley Major Activity Centre Structure Plan Clause 22.13- Environmentally Sustainable Development Policy <u>Particular Provisions</u> Clause 52.05 - Signs Clause 52.06- Car Parking Clause 53.18- Stormwater Management in Urban Development Clause 65 – Decision Guidelines
STATUTORY PROCESSING DATE:	22 October 2022
DEVELOPMENT COST:	\$25 Million

LOCALITY PLAN



SUBJECT SITE

NEIGHBOURHOOD PLAN



1-5 Bogong Avenue, Glen Waverley- Part Demolition And Construction of Buildings And Works To An Existing Car Parking Facility (Additional Four Levels) And Display Of Electronic Signage

RECOMMENDATION:

That Council resolves to issue a **Notice of Decision to Grant a Planning Permit (TPA/54087)** for part demolition and construction of buildings and works to an existing car parking facility (additional four levels) and display of electronic signage, at 1-5 Bogong Avenue, Glen Waverley subject to the following conditions:

Amended Plans Required

1. Before the development starts, amended plans drawn to scale and correctly dimensioned must be submitted to the satisfaction of and approved by the Responsible Authority. When approved, the plans will be endorsed and then form part of the Permit. The plans must be generally in accordance with the plans submitted to Council prepared by Katz Architecture , dated 9 August 2022 (Revision A) but modified to show:
 - a) The location of any substation or electricity meter boxes required by the power company for this development. Any substation must be incorporated within the building to ensure minimal impact on the visual amenity of the public realm;
 - b) Clarification that the canopy structure will be set back at least 0.75m from the edge of the kerb line to avoid vehicle damage;
 - c) The porosity of the cladding to be a minimum of 20 to 40% to ensure no adverse wind impacts;
 - d) Details of the Bogong Moth Imagery proposed within the perforated metal façade;
 - e) All identified existing and proposed small car spaces to include clear signage for patrons;
 - f) Detail of all electronic parking signage to the Bogong Avenue façade including above the western entry point Provision for “Give Way’ signage (line-marking) at all vehicle access points as vehicles leave the site.
 - g) Relocation of fire hose and reels and associated structures at Levels 4 to 7 to avoid reduction of the access aisle widths to the satisfaction of the Responsible Authority.
 - h) Line marking to be provided to spaces north of the stairwell (ST02) on Level 6.
 - i) The roof area is to be connected to the rain water tank and used for irrigation associated with landscaping;
 - j) The size of the water tank and capacity/size of the solar PV system;
 - k) All excavation and footing details in accordance with the Engineering Report required by SRLA Condition 19;

- l) Excavation depths to be shown at Australian Height Datum (AHD) on floor plans and elevations;
 - m) Foundations and footing designs of all buildings, structures and retaining walls;
 - n) A Landscape Plan in accordance with Condition 3;
 - o) A Tree Management Plan in accordance with Condition 4.
 - p) A Sustainable Management Plan in accordance with Condition 5.
- All to the satisfaction of the Responsible Authority.

Layout not to be Altered

2. The development as shown on the endorsed plans must not be altered without the prior written consent of the Responsible Authority.

Landscape Plan

3. Concurrent with the endorsement of any plans requested pursuant to Condition 1, a Landscape Plan prepared by a Landscape Architect or a suitably qualified or experienced landscape designer, drawn to scale and dimensioned must be submitted to and approved by the Responsible Authority. When endorsed, the plan will form part of the Permit. The Landscape Plan must be generally in accordance with the Landscape Concept Plan prepared by Michael Smith and Associates, dated 1 July 2022 (Revision A) except that the plan must be modified to show:
- a) The location and size of the proposed water tank;
 - b) Planting along the northern boundary to be a non-invasive root system species;
 - c) A notation that the water tank is to be used for irrigation associated with maintenance of landscaping; and
 - d) Any changes as required by Condition 1 of this Permit.

Before the occupation of any of the buildings allowed by this permit, landscaping works as shown on the endorsed plans must be completed to the satisfaction of the Responsible Authority and thereafter maintained to the satisfaction of the Responsible Authority.

Tree Management Plan

4. Concurrent with the submission of amended plans required by Condition 1 and prior to any demolition or site works, a Tree Management Plan (TMP) must be submitted to and approved by the Responsible Authority. The TMP must be prepared by a suitably qualified and experienced Arborist and must set out recommendations and requirements in relation to the management and maintenance of trees to be retained within the front setback of the site.

The TMP must be approved by the Responsible Authority prior to the commencement of any works, including demolition and/or levelling of the site. The TMP must make specific recommendations in accordance with the Australian Standard AS4970: 2009 - Protection of Trees on Development Sites and detail the following to the satisfaction of the Responsible Authority ensuring the trees to be retained remain healthy and viable during construction:

- a) A Tree Protection Plan drawn to scale that shows:
 - I. Tree protection zones and structural root zones of all trees to be retained,
 - II. All tree protection fenced off areas and areas where ground protection systems will be used;
 - III. The type of footings within any tree protection zones;
 - IV. Any services to be located within the tree protection zone and a notation stating all services will either be located outside of the tree protection zone, bored under the tree protection zone, or installed using hydro excavation under the supervision of the Project Arborist; and
 - V. A notation to refer to the Tree Management Plan for specific detail on what actions are required within the tree protection zones.
- b) Details of how the root system of any tree to be retained will be managed. This must detail any initial non-destructive trenching and pruning of any roots required to be undertaken by the Project Arborist.
- c) Supervision timetable and certification of tree management activities required by the Project Arborist to the satisfaction of the responsible authority; and
- d) Any remedial pruning works required to be performed on tree canopies located within subject site. The pruning comments must reference Australian Standards 4373:2007, Pruning of Amenity Trees and a detailed photographic diagram specifying what pruning will occur.

The recommendations contained in the approved tree management plan must be implemented to the satisfaction of the Responsible Authority.

Sustainable Management Plan

5. Concurrent with the endorsement of plans requested pursuant to Condition 1, a Sustainable Management Plan (SMP) must be submitted to and approved by the Responsible Authority. The plan must be generally

in accordance with the SMP prepared by Compass Dated 25 July 2022, except that the plan must be modified to show:

- a) The roof area is to be connected to the water tank;
- b) Details of the water demands that will be met from the rainwater tank;
- c) Details of any water sensitive design initiatives within the landscaping and surrounding paved areas;
- d) Performance of Water Sensitive Urban Design initiatives to be modelled using STORM Assessment;
- e) Any changes required by Condition 1 of this planning permit.

Upon approval the Sustainable Management Plan will be endorsed as part of the planning permit and the development must incorporate the sustainable design initiatives outlined in the SMP to the satisfaction of the Responsible Authority.

Wind Impact Assessment

6. The provisions, recommendations and requirements of the endorsed Wind Assessment Report prepared by Vipac dated 11 October 2022 must be implemented and complied with to the satisfaction of the Responsible Authority.

Construction Management Plan

7. Prior to the commencement of any site works (including any demolition and excavation), a Construction Management Plan (CMP) must be submitted and approved by the Responsible Authority. The Plan must also be submitted to the satisfaction of the Suburban Rail Loop Authority. No works are permitted to occur until the Plan has been endorsed by the Responsible Authority. Once endorsed, the CMP will form part of the permit and must be implemented to the satisfaction of the Responsible Authority. The CMP must address the following issues:

Component of the plan relevant to Suburban Rail Loop Authority:

- a) the demolition and construction program;
- b) any demolition, excavation or construction mitigation measures identified in the Engineering Report (or approved in accordance with Condition SC3 (Engineering Report);
- c) preferred arrangement for trucks delivering to the land, including delivery and unloading and expected duration and frequency;
- d) protection of infrastructure to ensure Suburban Rail Loop East and associated infrastructure is not damaged during demolition or construction;

- e) an outline of requests to occupy public footpaths or roads, Suburban Rail Loop Authority area land or anticipated disruptions to local services;
- f) management of drainage, effluent, material stockpiles, fencing and hoardings to ensure Suburban Rail Loop Authority area land or Suburban Rail Loop East and associated infrastructure is not used for, or impacted by these activities;
- g) measures to ensure that all works on the land will be carried out in accordance with the Demolition and Construction Management Plan;

Other requirements of the Construction Management Plan:

- h) Appropriate measures to control noise, dust and water and sediment laden runoff;
- i) Appropriate measures for the prevention of silt or other pollutants from entering into the Council's underground drainage system or road network;
- j) Appropriate measures relating to removal of hazardous or dangerous material from the site, where applicable;
- k) A plan showing the location and design of a vehicle wash-down bay for construction vehicles on the site so as to prevent material leaving the site and being deposited on Council's road network;
- l) A program for the cleaning and maintaining surrounding road surfaces;
- m) A site plan showing the location of any site sheds, on-site amenities, building waste storage and the like, noting that Council does not support the siting of site sheds within Council road reserves;
- n) Measures to provide for public safety and site security;
- o) A plan showing the location of parking areas for construction and sub-contractors' vehicles on and surrounding the site, to ensure that vehicles associated with construction activity cause minimum disruption to surrounding premises. Any basement car park on the land must be made available for use by sub-constructors/tradespersons upon completion of such areas, without delay;
- p) A Traffic Management Plan showing truck routes to and from the site;
- q) A swept path analysis demonstrating the ability for trucks to enter and exit the site in a safe manner for the largest anticipated truck associated with the construction;
- r) Appropriate measures to ensure that sub-contractors/tradespersons operating on the site are aware of and adhere to the requirements of the CMP;

- s) The provision of contact details of key construction site staff; and
- t) Include a requirement that except with the prior written consent of the Responsible Authority, a requirement that demolition, excavation or construction works must only be carried out during the following hours:
 - Monday to Friday (inclusive) – 7.00am to 6.00pm;
 - Saturday – 9.00am to 1.00pm;
 - Saturday – 1.00pm to 5.00pm (Only activities associated with the erection of buildings that does not exceed the EPA guidelines)
 - No works are permitted on Sundays or Public Holidays.

The provisions, recommendations and requirements of the endorsed Construction Management Plan must be implemented and complied with by all contractors to the satisfaction of the Responsible Authority.

Car Parking and Driveways to be Constructed

8. Before the use starts or any building is occupied, areas set aside for parked vehicles and access lanes as shown on the endorsed plans must be:
- a) constructed to the satisfaction of the Responsible Authority;
 - b) properly formed to such levels that they can be used in accordance with the plans;
 - c) surfaced with an all-weather sealcoat to the satisfaction of the Responsible Authority;
 - d) drained, maintained and not used for any other purpose to the satisfaction of the Responsible Authority;
 - e) line-marked to indicate each car space and all access lanes to the satisfaction of the Responsible Authority.

Parking areas and access lanes must be kept available for these purposes at all times.

Lighting of carparks and accessways

9. Low intensity / baffled lighting must be provided to ensure that car park areas and pedestrian accessways are adequately illuminated without any unreasonable loss of amenity to the surrounding area, to the satisfaction of the Responsible Authority.

Amenity of Area

10. The amenity of the area must not be detrimentally affected by the use or development, through the:
- a) transport of materials, goods or commodities to or from the land;

- b) appearance of any building, works or materials;
- c) emission of noise, artificial light, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit or oil; and
- d) presence of vermin.

To the satisfaction of the Responsible Authority.

Ongoing Architect Involvement

11. As part of the ongoing consultant team, Katz Architects or an architectural firm which is acknowledged to have comparable skill and expertise to the satisfaction of the Responsible Authority must be engaged to:
- a) oversee design and construction of the development; and
 - b) ensure the design quality and appearance of the development is realised as shown in the endorsed plans or otherwise to the satisfaction of the Responsible Authority.

Plant / Equipment or features on roof

12. No equipment, services, architectural features or structures of any kind, including telecommunication facilities, other than those shown on the endorsed plans shall be permitted above the roof level of the building unless otherwise agreed to in writing by the Responsible Authority.

Lopping of Vegetation

13. The lopping of vegetation permitted shall be done in such a way that the health, appearance or significance of the vegetation which is required to be retained is not affected.

Drainage and Stormwater

14. The site must be drained to the satisfaction of the Responsible Authority.
15. No polluted and/or sediment laden runoff is to be discharged directly or indirectly into Council's drains or watercourses during and after development, to the satisfaction of the Responsible Authority.
16. Stormwater discharge is to be detained on site to the predevelopment level of peak stormwater discharge. Approval of any detention system is required by the City of Monash prior to works commencing, or any alternate system.

17. A plan detailing the drainage works must be submitted to the Engineering Division prior to the commencement of works. The plans are to show sufficient information to determine that the drainage works will meet all drainage requirements of this permit.
18. The full cost of reinstatement of any Council assets damaged as a result of demolition, building or construction works, must be met by the permit applicant or any other person responsible for such damage, to the satisfaction of the Responsible Authority.

Suburban Rail Loop Conditions (SCO15/2022/11)

19. Unless otherwise agreed in writing with the Suburban Rail Loop Authority, before the development starts (including demolition and excavation) and before the submission of plans under Condition 1 (Amended Plans), an Engineering Report from a suitably qualified engineer must be submitted to and approved by the Suburban Rail Loop Authority. The report must outline the design, management and construction techniques to be implemented prior, during and following construction to prevent any impact on Suburban Rail Loop East and associated infrastructure. Once approved, the Engineering Report will form part of the planning permit. All relevant structural and geotechnical issues must be considered and the report must demonstrate the following:
 - a) That the building footings will not compromise the structural integrity of, or damage or displace Suburban Rail Loop East and associated infrastructure. In this case, demonstrating that the increase in unfactored loading from the footings at RL 104.2 m AHD beneath the western area of the building does not exceed 50 kPa would be considered to satisfy this requirement (unloading from basement excavations should not be included in this assessment);
 - b) That the development and construction methods will appropriately manage and mitigate any impacts from construction vibration on the Suburban Rail Loop East and associated infrastructure.
20. All design, excavation and construction must be undertaken in accordance with the Engineering Report approved under Condition 19 (Engineering Report), unless alterations or modifications are approved in writing by the Suburban Rail Loop Authority.
21. Where any alterations or modifications to the plans endorsed under Condition 1 (Amended Plans) are not consistent with the Engineering Report prepared in accordance with Condition 19 (Engineering Report), the prior written consent of the Suburban Rail Loop Authority must be obtained.

22. All demolition and construction works must be carried out in accordance with the approved Demolition and Construction Management Plan. The Demolition and Construction Management Plan must be implemented at no cost to the Suburban Rail Loop Authority.
23. Unless otherwise agreed in writing with the Suburban Rail Loop Authority, the permit holder must ensure that no Suburban Rail Loop East infrastructure, assets or services are damaged or altered as a result of the development. Any damage must be rectified to the satisfaction of the Suburban Rail Loop Authority, at the full cost of the permit holder.
24. Unless otherwise agreed in writing, the permit holder must give Suburban Rail Loop Authority at least 10 business days' notice of the expected date of commencement.

Signs

25. All signs must be constructed and maintained to the satisfaction of the Responsible Authority.
26. All signs must be located wholly within the boundary of the land.
27. The intensity of lighting associated with the illumination of the advertising sign(s) must be limited so as not to cause glare or be a distraction to motorists in adjoining streets or cause a loss of amenity in the surrounding area to the satisfaction of the Responsible Authority.
28. The sign must not:
 - a) Dazzle or distract drivers due to its colouring;
 - b) Be able to be mistaken for a traffic signal because it has, for example, red circles, octagons, crosses or triangles;
 - c) Be able to be mistaken as an instruction to drivers.To the satisfaction of the Responsible Authority.
29. The sign must not obstruct the view of motorists, obscure traffic signals or constitute a road safety hazard in any way.

Satisfactory Continuation and Completion

30. Once the development has started it must be continued and completed to the satisfaction of the Responsible Authority.

Time for Starting and Completion

31. In accordance with section 68 of the *Planning and Environment Act 1987*, this permit will expire if one of the following circumstances applies:

- a) The development is not started before 4 years from the date of issue.
- b) The development is not completed before 6 years from the date of issue.

In accordance with section 69 of the *Planning and Environment Act 1987*, the responsible authority may extend the periods referred to if a request is made in writing before the permit expires, or

- (i) within six (6) months afterwards if the development has not commenced; or
- (ii) within twelve (12) months afterwards if the development has not been completed.

Council and the Victorian Civil and Administrative Tribunal are unable to approve requests outside of the relevant time frame.

Permit to Expire (Signage)

32. The approved sign(s) of this Permit will expire 15 years from the date of issue of this permit.

NOTES:

- A. Building Permit approval for the works must be obtained prior to the commencement of the approved works.
- B. Any request for a variation of this Permit shall be lodged with the relevant fee as determined under the Planning & Environment (Fees) Regulations 2016.
- C. Unless no permit is required under the planning scheme, no sign must be constructed or displayed without a further permit.

SRLA Permit Notes

- A. Any boreholes, geotechnical investigations or intrusive ground investigations must be first approved in writing by Suburban Rail Loop Authority. Any submission to Suburban Rail Loop Authority for approval must include, to the satisfaction of Suburban Rail Loop Authority, details of their location and depth, and the methods and management processes that will be adopted to avoid impacts to the Suburban Rail Loop structures.

All boreholes must be backfilled to the satisfaction of Suburban Rail Loop Authority using a grout bentonite mix (3% bentonite) or satisfactory

equivalent and in accordance with Southern Rural Water (SRW) licence conditions.

- B. Please contact the Suburban Rail Loop Authority on 1800 105 105 or infrastructure.protection@srla.vic.gov.au when 'as-constructed' details of the building's footings (and if piled, the piling records) are available, to ensure they are considered as part of the detailed design of SRL East.
- C. Should the development be constructed at the same time as SRL East (in this area), communication between the permit holder and the Suburban Rail Loop Authority is critical to coordinate the logistics of both projects. Please contact the Suburban Rail Loop Authority on 1800 105 105 or infrastructure.protection@srla.vic.gov.au

BACKGROUND:

History

Planning Permit TPA/30659 was issued on 24 September 2003 for the construction of two additional decks of car parking above the existing Council owned public car park with associated lift and covered walkway.

Planning Permit TPA/30665 was issued on 17 October 2003 for buildings and works associated with a new access ramp. The ramp associated with this Permit has been constructed, and is located on the western side of the site which provides access to the basement car park associated with the office building located at No 7 Bogong Avenue (Centrelink).

The Site and Surrounds

The subject site is located on the northern side of Bogong Avenue, approximately 37 metres west of Kingsway. The site is located within the Glen Waverley Activity Centre, approximately 157 metres south of the Glen Waverley Train Station.

This site is made up of two parcels of land:

- Lots 1 and 2 on LP 208586X being the land comprised in certificate title volume 9767 folio 458; and
- Lot 2 on PS522436A being the land comprised in certificate of title volume 10828 folio 242. Part of this lot 2 contains the ramp to the basement car park to the Centrelink building constructed on Lot 1 on PS522436A.

A 2 metre wide drainage and sewerage easement is located adjacent to the northern boundary of the site. Part of the laneway to the east of the site is located within the title boundaries of the site. The width of the laneway is being maintained as part of this application.

The site contains a 24-hour 4 level multi-deck carpark which is Council owned. The maximum height of the building is currently 9.95 metres measured to the parapet (or 11.44 metres measured to the top of the stair/lift well).

The existing building comprises a Ground, First, Second and Roof Deck levels. Two vehicle access points are provided to the car park via Bogong Avenue, with a singular lift and stair access provided for pedestrians. A secondary vehicular access point is also present to ground level parking via the laneway to the east of the site.

The site has a significant fall from the East down to the West.

There are 32 established trees within the front set back of the site along Bogong Avenue.

The building is set back a minimum of 0.97 metres when measured to the lift core/lobby/stair. The majority of the building is set back 5.2 metres, noting that the set back increases to the east as the title boundary curves away and reduces slightly to the west where the title boundary curves inwards. A curved external façade mesh screen protrudes from the building into this 5.2 metre setback by up to 900mm).

The building is set back a minimum of 2 metres to the northern boundary (due to the easement), and approximately 1.6 metres from the eastern boundary (allowing for a widening of the laneway). Existing planting is located along some of the northern boundary of the site.

This existing carpark is used to service the Kingsway retail precinct, the Century City complex as well as the civic centre and library. In addition to this, it is used by employees of the Centrelink building to the West and the City of Monash Civic Centre to the East.

There are currently 514 parking spaces on the site as follows:

Car Parking Level	Parking Restrictions	Existing Car Parking Supply
Ground Level	3P 8am-6pm Mon-Sat	128 Spaces
Level 1	4P 8am-6pm Mon-Sat	126 Spaces
Level 2	4P 8am-6pm Mon-Sat (Authorised Council Staff Excepted in Red Bays Only)	126 Spaces
Level 3 (Roof)	4P 8am-6pm Mon-Sat (Authorised Trader Permits Excepted in Yellow Bays Only)	134 Spaces
Total		514 Spaces



The key site interface conditions are as follows:

- **North:** To the immediate north of the site, along Montclair Avenue is an existing 7-storey mixed-use development at No 52 Montclair Avenue (TPA/38878), an approved 11-storey building is under construction at No 54 Montclair Avenue (TPA/46792), and there is an approved 11-storey building at 50 Montclair Avenue (TPA/46792) yet to commence construction. Further along Montclair Avenue is the Monash City Church of Christ and an office building.
- **East:** To the immediate east of the subject site, a laneway provides service access to the Kingsway fronting retail properties. This laneway runs between Bogong Avenue and Montclair Avenue. Commercial properties in this section of Kingsway are single and double storey in height. It is noted that the property at 73-75 Kingsway is currently being developed in accordance with a recent Planning Permit issued by VCAT for a 7 storey mixed use building (TPA/50168).
- **South:** To the south of Bogong Avenue is the Residential Growth Zone. This area consists of a variety of built forms and uses including established single storey and double dwellings, a 3 storey apartment building (24 Bogong Avenue) and the Glen Waverley Uniting Church.
- **West:** The western edge of the subject site abuts the Medicare and Centrelink Building. This building is serviced by private underground parking via a ramp between the subject site and the Medicare and Centrelink Building.

The proposed Suburban Rail Loop (SRL) station will be located below Glendale Street, just 0.15km North of Bogong Carpark.

An aerial photograph of the subject site and surrounding land can be found attached to this report (Attachment 2).

PROPOSAL:

It is proposed to provide an additional four levels of car parking on the existing car parking structure creating a seven storey building. This will increase the number of car parking spaces from 514 to 1032 cars.

The expansion of the car park is sought to improve the viability of the Glen Waverley Activity Centre and as a direct response to the Suburban Rail Loop Authority (SRLA) intentions to acquire Council's land in Glendale Avenue currently used for at grade car parking. It is noted that the Glendale Avenue carpark was earmarked by Council for a new multi-level mixed use car park and will instead be used as part of the Suburban Rail Loop.

The proposal can be summarised in detail as follows:

- Demolition of the following: existing stair/lobby/lift area, existing canopies within the front setback of the site, existing lighting on the rooftop and the handrails to the façade fronting Bogong Avenue.
- Removal of 6 trees within the front setback of the site. Five trees to be removed (Tree No.s 3, 5, 8, 9 and 10) will accommodate the proposed redesigned lift/lobby/stair area. Tree No 17 is to be removed due to overcrowding.
- The existing vehicle access points to the site will not be altered.
- Creation of a new main core (lift/lobby/stair area) to improve pedestrian circulation. The revised structure will be set back between 320mm and 2.5 metres from the title boundary and extend to the rooftop of the building.
- A replacement weather canopy structure is proposed to extend from the core area into the road reserve across the footpath. The structure is proposed to be a maximum of 5.4 metres in height.
- Addition of 4 levels of car parking of a similar layout as the existing, without reducing setbacks to the north, east and west. The front setback of the building is proposed to be maintained at approximately 5.2 metres.
- Reinforcement of existing concrete support columns, footings and sheer walls is proposed to support the additional levels of parking.
- An increase in parking capacity by 518 spaces (total parking capacity of 1,032 cars) across 8 levels of car parking (inclusive of disabled car parking, two electronic vehicle charging bays and 26 small car parking spaces).
- Provision of 11 motorbike/scooter parking spaces and 12 bicycle parking spaces at the ground floor.
- A maximum building height of 22.6m (measured to the parapet) with a height of 24.06 metres to the top of the main core.

- The lower levels of the building (4 storeys) are proposed to be clad with a perforated metal façade with custom Bogong Moth imagery in anodised graded tones. The levels above are proposed to be clad with a perforated anodised aluminium panels in a 'concertina' layout.
- Solar PV Panels are proposed on the rooftop, which are proposed to run above the central car spaces. The structure is approximately 4.1 metres in height. Pole lighting is also proposed on the rooftop.
- Electronic signage is proposed to identify available car parking spaces within the building. The signs will measure 2m x 0.5m, with a blue parking directional sign above measuring 2m x 2m and will be located above the two car park entrances.
- The proposed changes to parking space numbers and overall building height are detailed in the table below.

	Existing	Proposed
Car Parking	514 spaces	1032 spaces (+518)
Vehicle charging bays	0 bays	2 bays
Motorbike / scooter parking	0 spaces	11 spaces
Bicycle Parking	0 spaces	12 spaces
Maximum building Height	9.95m measured to the parapet (11.44m to top of core)	22.6m measured to parapet (24.06m to top of main core)





Attachment 1 details plans forming part of the application.

PERMIT TRIGGERS:

Zoning

The subject site is located within the Commercial 1 Zone. Pursuant to Clause 34.01-1, the use of the land for a car park falls under Section 2 – permit required. However, the proposal seeks to extend the existing car parking and as such benefits from existing use rights and therefore no Permit is required for the use.

Pursuant to Clause 34.01-4, a permit is required to construct a building or construct or carry out works under the Commercial 1 Zone.

Overlays

Design and Development Overlay

The site is affected by the Design and Development Overlay, Schedule 12. Pursuant to Clause 43.02-2, a permit is required to construct a building or construct or carry out works.

Schedule 12 of the Design and Development Overlay provides discretionary controls with respect to height, setbacks, and other design details.

Specific Controls Overlay

The site is also affected by Specific Controls Overlay Schedules 14 and 15 (SCO14 and SCO15).

Pursuant to Schedule 15, a Planning Permit is required to demolish, remove or relocate a building and construct a building or construct or carry out works (excluding the removal, destruction or lopping of vegetation) on all land shown in Project Infrastructure Protection Area A.

The Suburban Rail Loop Authority (SRLA) is a determining referral authority for an application to construct a building or construct or carry out works within the SCO15.

There is no permit trigger under the SCO14.

Particular & General Provisions

Pursuant to Clause 52.05 (Signs), a permit is required for the display of an electronic sign. The site is within a Category 1 - Commercial Areas.

There is no statutory car parking rate set out in the car parking requirements for a 'car park'. In such instances Clause 52.06-6 requires that *"car parking must be provided to the satisfaction of the Responsible Authority"*.

Clause 52.17 (Native Vegetation) is not triggered as the vegetation to be removed were planted.

Pursuant to Clause 53.18 (Stormwater Management in Urban Development), an application must be accompanied by details of the proposed stormwater management system, including drainage works and retention, detention and discharges of stormwater to the drainage system. A Sustainability Management Plan has been provided including relevant details

Attachment 3 details the zoning and overlays applicable to the subject site and surrounding land.

CONSULTATION:

Further information was requested of the Permit Applicant on 21 July 2022.

The Permit Applicant responded to this letter on 25 August 2022 by providing the requested information.

The Applicant has been verbally advised that this application is recommended for approval subject to conditions, and an outline of the conditions and the ramifications of the conditions on the proposal has been explained.

Public Notice

The application was advertised in accordance with section 52 of the *Planning and Environment Act 1987* by way of letters sent to the surrounding property owners/occupiers, and signs displayed Bogong Avenue and the adjacent laneway.

One (1) objection was received. Issues of objection included the following concerns:

- Residents living opposite the site find it difficult to turn out of driveways due to blocked sight lines from parked vehicles, which will be further impacted by additional vehicle movements on the street. The objection includes a request to delete the parking on Bogong Avenue or be made a one-way road.

Public information session

An online public information session was held on Wednesday 7 September 2022. There was no attendance by the community at this session.

Attachment 4 details the location of the objector property

Referrals

External Referral

Suburban Rail Loop (SCO15/2022/11)

The application was referred to the SRLA in accordance with Clause 66.04 of the Monash Planning Scheme.

The SRLA noted that the proposed development would be constructed over part of the proposed tunnel alignment as shown within SRLA's Surface and Tunnel Plans dated April 2022.

The SRLA has no concerns with the proposal subject to conditions relating to excavation, an Engineering Report, and a demolition Construction Management Plan among others.

Internal ReferralStrategic Planning

The justification for the increased height from the applicant is sound and appropriate in the context of an extension to an existing building for the purpose of public car parking within the major activity centre. The building adjoins higher buildings in Montclair Avenue and will have minimal visual impact.

Traffic Engineer

Conditions have been recommended to ensure compliance with the detailed design requirements of Clause 52.06. It has also been recommended additional ‘Give Way’ line-marking is to be provided to each vehicle access of the site along Bogong Avenue requiring motorists to give way to the increase in the number of pedestrians along Bogong Avenue. The proposed turning lane arrangements are adequate to accommodate projected critical peak number of vehicles expected for the Saturday midday period (11.45am – 12.45pm).

Drainage Engineer

Standard conditions requested including submission of a drainage plan for approval. Proposed tree planting in the easement will be satisfactory provided the species have non-invasive root systems.

DISCUSSION:**Consistency with State, Regional and Local Planning Policies**

The subject site is located within the Glen Waverley Major Activity Centre. Clause 11.03-1 seeks to support the development and growth of activity centres by ensuring they are able to accommodate significant growth for a broad range of land uses.

Clause 17.02-1S seeks to encourage development that meets the community’s needs for retail, entertainment, office and commercial services and to provide for sufficient infrastructure.

Various policies including Clause 18.01-1S seek to integrate the sustainable transport system with land uses and maximising the presence of existing infrastructure by increasing the diversity and density of development along the Principal Public Transport Network (Clause 18.02-2R). Whilst Clause 18.02-4S seeks to ensure an adequate supply of car parking that is appropriately designed and located, it also takes note that car parking demand is subject to access to public transport, demand for off-street car parking and road capacity. This policy also recommends consolidating car parking facilities to improve efficiency and to design public car parks to incorporate electric charging facilities to support the use of low-emission vehicles.

With respect to built form, the Planning Policy Framework seeks to achieve building design outcomes that contribute positively to the local context and

enhance the public realm (Clause 15.01-2S) whilst also creating urban environments that are safe, healthy, functional and enjoyable and that contribute to a sense of place and cultural identity.

Local Planning Policy Framework (LPPF)

The municipal strategic statement identifies that the business community is an important generator of income and employment for the local economy. Monash is characterised by employment in activity centres and also has a large proportion of economic activity generated by small business. In response to the predicted population growth, it is important to direct more intensive, higher scale development to activity centres that are well serviced by public transport, commercial, recreational, community and educational facilities closer to where people live.

Clause 21.06 identifies that Glen Waverley has experienced major renewal and redevelopment in the past few years that has revitalised the centre. It is anticipated that the centre will continue to grow and change in the following years, particularly with the anticipated future suburban rail link which will include a connector station proximate to the subject site. The Planning Scheme encourages Glen Waverley continue to develop as a major entertainment and retail centre for the eastern metropolitan region. In doing this, it is sought to ensure that parking is provided to meet the needs of the centre, whilst also enhancing public transport, pedestrian and cycling linkages.

Clause 22.03 (Industry and Business Development and Character Policy) applies to all land within the Commercial and Industrial Zones. The site is identified as being located within the Business Character Type 3 Area. The desired future character statement notes that *“high rise development within the Glen Waverley Major Activity Centre should be located towards the middle of the centre, consistent with the adopted Structure Plan”* and that development should be consistent with Clause 21.06.

Clause 22.14 (Glen Waverley Major Activity Centre Structure Plan) applies to all land within the Glen Waverley Major Activity Centre (GWAC). This policy identifies the site as being within Area 4 (designated for offices at ground level with offices/housing located above) and in Precinct 2, being the Montclair Precinct. Within the Montclair precinct it is sought to:

- *“Increase opportunities for mixed use office and residential development across the precinct with activated ground level frontages.*
- *Encourage site consolidation where necessary to achieve high amenity and efficient development outcomes.*
- *Ensure a high level of pedestrian priority along all streets across the precinct.*
- *Increase local public open space to cater to the needs of existing and future residents and workers”.*

The Structure Plan suggests that a ring road be created (along Bogong Avenue), The Structure Plan (updated June 2016) is a reference document to the Planning Scheme. The structure plan notes that *“Although the structure plan encourages a shift to more sustainable and active modes of transport such as walking and cycling, it is recognised that vehicles will continue to play a key role in providing access to businesses and services in the GWAC”* allowing for Kingsway traffic to be reduced to be primarily pedestrian in nature. The parking analysis prepared in conjunction with the Structure Plan reveals that certain car parking precincts are nearing capacity while others are relatively underutilised. Therefore it is important that future car parking is strategically located to relate to the level of activity, is easy to find from key vehicle access roads and there are activated, safe and convenient pedestrian links between the car park and the shops. It is suggested that the car parking lost from Kingsway will be replaced with car parking in large car parking structures.

Clause 22.05 (Tree Conservation Policy) seeks:

- *To maintain, enhance and extend the Garden City Character throughout Monash by ensuring that new development and redevelopment is consistent with and contributes to the Garden City Character as set out in the Municipal Strategic Statement.*
- *To promote the retention of mature trees and encourage the planting of new canopy trees with spreading crowns throughout Monash.*

Clause 22.08 (Outdoor Advertising Policy) identifies the objectives for signage within the Glen Waverley Activity Centre to seek to *“identify and promote retailing, entertainment activities and other commercial uses in a vibrant, colourful and integrated manner”*. Electronic signage is not specifically captured in this policy being a relatively new technology, however with respect to internally illuminated signage it notes that they are *“acceptable in areas of high activity or for business identification purposes provided there is no detrimental effect on nearby properties. May include a logo”*.

Clause 22.13 (Environmentally Sustainable Development Policy) requires a Sustainable Management Plan (SMP) to be prepared with an application of this scale. A report was provided with the application, prepared by Compass. Green Star and BESS tools are not applicable to car parks and therefore have not been applied. The SMP has been prepared using the relevant requirements as a basis. The report suggests that initiatives including a 5kl rainwater harvesting, solar photovoltaic (PV) systems, bicycle and electric vehicle parking and responsible building materials. A condition will require the size of the rainwater tank and PV system to be detailed on development plans including a STORM assessment to be undertaken.

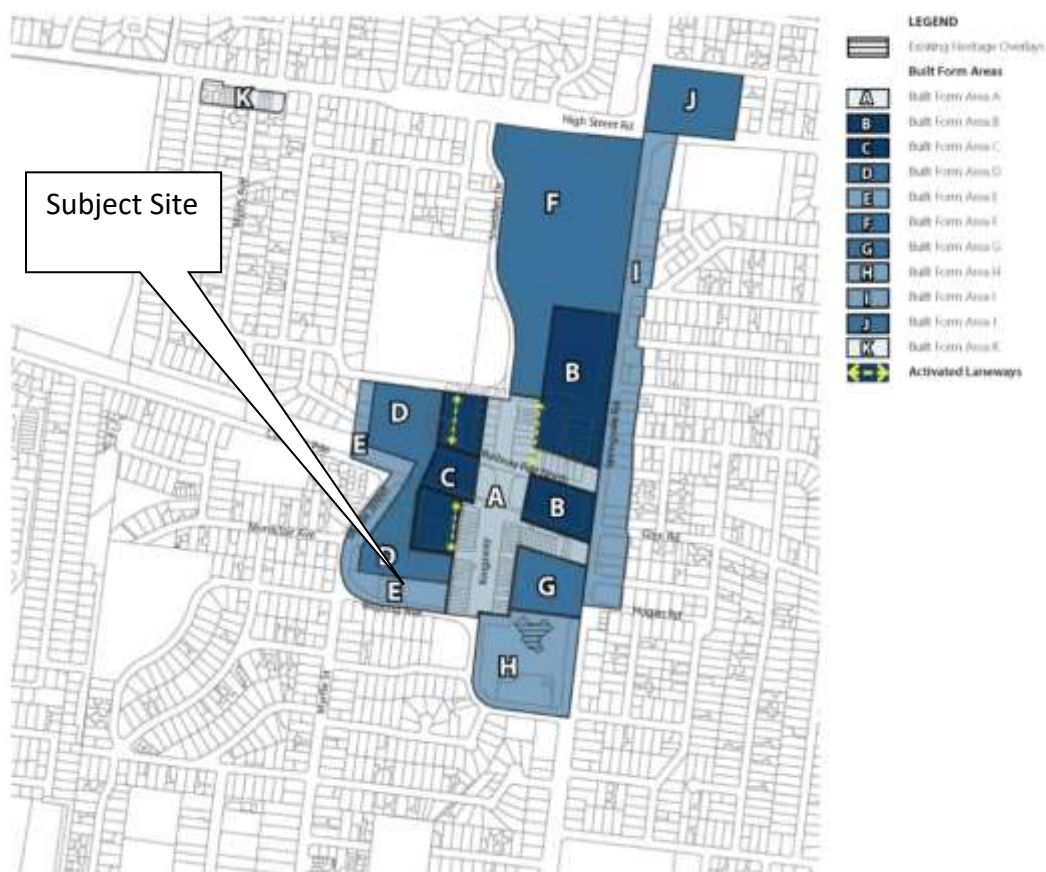
Commercial 1 Zone

The purpose of the Commercial 1 Zone is:

- *“To implement the Municipal Planning Strategy and the Planning Policy Framework.*
- *To create vibrant mixed use commercial centres for retail, office, business, entertainment and community uses.*
- *To provide for residential uses at densities complementary to the role and scale of the commercial centre”.*

Design and Development Overlay

The site is identified as being within ‘Built form Area E’ within the Design and Development Overlay Schedule 12 (DDO12) which has a preferred building height of 4 storeys (15 metres), with a 5 metre front street setback. Floor to floor dimensions of 3.2-3.5 metres should apply. An application to exceed the preferred maximum building height must demonstrate how the development will continue to achieve the Design Objectives, Development Outcomes and all other relevant requirements of this schedule.



The policy seeks the following development outcomes:

- *Active frontage to Bogong Avenue, Myrtle Street, O'Sullivan Road and Railway Parade North.*
- *Respect the scale of the surrounding residential area.*

- *Facilitate a scale of development along Myrtle Street and bogong Parade of up to 4 storeys to create a consistent streetscape scale with the Residential Growth Zone to the west and south.*
- *Contribute to the provision of a treed ring road within landscaped front setbacks supporting deciduous trees.*

The policy also outlines preferred landscaping outcomes, and wind and weather protection

Assessment against the Glen Waverley Activity Centre Structure Plan and Design and Development Overlay, Schedule 12

Building Height

The maximum building height proposed is 20.2 metres (noting that the screening feature is a maximum of 22.6 metres and the stairwell and lift element is 24.06m). This is 3 storeys and 5.2 metres above the preferred building height.

The built form area directly to the north of the site (Area D) allows for a height of 8-10 storeys (29-36 metres). As noted earlier in this report, construction is underway for the development at the following properties north of the site:

- No. 54 Montclair Avenue which is currently under construction and proposes an 11 storey building (36 metres),
- No. 50 Montclair is currently vacant and has approval for an 11 storey building (37.1 metres); and
- No. 52 Montclair Avenue contains a 7 storey building (29 metres).

The proposed development will be significantly lower in height than all of these adjacent buildings to the north of the site.

The maximum preferred building height within the Glen Waverley Activity Centre is 12 storeys, 43 metres within Built Form Area F (The Glen shopping centre location).

The built form controls for the subject site seek to provide a transition from this precinct down to the Residential Growth Zone to the south, which has a preferred height of 13.5 metres. As such, it is considered that the proposed development will achieve an appropriate transition in height from Area D and will provide a building design that contributes positively to the local context and enhance the public realm.

The height allows for the efficient and sustainable consolidation of car parking within the Glen Waverley Activity Centre, minimising the impact of parking on the centre more broadly and will result in less car parking demand on Kingsway as sought by the Structure Plan. The proposal also provides additional parking within a site which is already used as a multi-level car parking facility without compromising amenity of adjoining properties or properties along the south side of Bogong Avenue.

The proposed building scale and form is considered compatible with the future character of the Activity Centre. It will have minimal impact on the amenity of adjacent and nearby residential areas. The additional height will not cast shadow on any habitable room windows or secluded private open space of residential properties to the south.

The non-mandatory nature of the controls reinforces that they provide guidance beyond just a consideration of whether it complies or not with a numerical standard in considering an application. All applications must be considered on merit, including within a changing context, and whilst different outcomes may be proposed and supported, does not mean that it would offend the intent of the control. This is particularly evident when considering that a structure plan is developed in an arbitrary nature in the absence of a specific proposal, and a proposal of a high standard that meets the intent of a control (whilst not necessarily adhering to it) can still be considered appropriate.

It should be noted that consideration of heights and development across the precinct will differ. Whilst the height proposed as part of this application are not considered inappropriate, this may not be the case if a development was proposed which had greater sensitive interfaces.

For the reasons outlined above the proposed development height is considered appropriate.

Building Setbacks

As noted above, the site is located within Built Form Area E which has a preferred front setback of 5 metres. The development is generally consistent with this requirement with the façade of the building being set back a minimum of 5.2 metres. A variation to this setback is sought for the stair/lobby/lift and canopy structure which protrudes into this front setback and is set back a minimum of 320mm from the edge of the title boundary. The core area is required to be in this location to avoid the need to reconstruct the full existing structure, and provides easy access for patrons. This portion of the building is required to be expanded from the existing due to the increased capacity of the car park. The width of the element within this setback is 12.3 metres. Given the building length of approximately 108 metres, this is considered to be a relatively narrow element of non-compliance. The core is to be primarily glazed to the street, reducing the bulk of this structure. Combined with the existing landscaping across the frontage, the proposed setback of the stair/lobby/lift is considered appropriate in this instance.

Building Form and Design

The Design and Development Overlay seeks to enable buildings that incorporate a podium level with taller elements setback from the front and side streets, creating a human scale to buildings. The Overlay suggests that a building 4 storeys in height would have a singular (sheer) setback to the street (with the exception of a canopy cover).

The application of this policy would apply comfortably with a traditional mixed use or commercial building, however as the proposed building relates to an essential piece of infrastructure these parameters are difficult to apply and not relevant for a car parking structure.

Notwithstanding this, the addition has been designed to reduce bulk impacts to Bogong Avenue as much as possible with the use of materials defining the lower 3 levels from the upper levels and creating visual interest with the use of the concertina cladding and central core area.

The setbacks allows the building to be clad in a consistent way across all facades to the building, limiting the presentation of any blank walls.

Wind and Weather Protection

The Design and Development Overlay specifies that wind tunnel testing is required for all buildings over 6 storeys in height. The Wind Impact Assessment prepared by Vipac indicates that the development complies with the wind comfort levels set out in DDO12, with the footpath areas and accessways to meet walking comfort criteria and the entrance lobby to meet standing comfort criteria. The report recommends that the porosity of the cladding be maintained at 40% as a minimum. This will be included as a proposed condition of the permit. It is not considered that a full wind tunnelling assessment is required in this instance given the compliance shown with the assessment provided and the open nature of the structure.

A canopy is provided over the entrance lobby. Whilst the policy suggests that canopies should be continuous and consistent with adjoining properties, given the alignment of the street and title boundary, in addition to the presence of large native trees to be retained on the site, it is not considered that this is practical nor required. The proposed canopy structure will be located within the identified road reserve as per the certificate of title. A condition will require the canopy structure will be set back at least 0.75m from the kerb line as required by the Design and Development Overlay, to avoid vehicle damage. Plans indicatively show that the setback is 1 metre.

Landscaping

The Design and Development Overlay seeks for development to contribute to the 'buildings in landscape' character, ensuring that existing significant trees be retained and protected. It is also noted that buildings along the future ring road should contribute to a green enveloping edge along the road by including landscaped setbacks that incorporate deciduous trees providing for summer shade and winter sun.

The site provides a significant contribution to the greening along Bogong Avenue with 32 existing large native trees along its frontage. The proposal requires the removal of 6 trees. Five (5) for the redesign of the lift/lobby/stairs, including one

(1) tree that has a poor structure and the arborist recommends removal. A sixth tree is to be removed due to overcrowding.

The retention of 26 trees ranging in height from 5 to 14 metres, will continue to provide the greening envelope envisaged by the DDO and provide for significant canopy cover along the frontage of the site.

No additional trees or landscaping is proposed within the front setback of the site to replace the 6 trees to be removed due to the constraints of landscaping space. The existing landscaping beds, including small shrubs, ground covers and canopy trees are at capacity. It is considered that this is acceptable given the maintenance of the existing well established plants. There is no unnecessary hard paving sought.

Due to the limits of replacement planting along the frontage, it is proposed to provide additional planting along the rear (northern) boundary of the site including an additional 22 Ornamental Pear Trees, 9 Blueberry Ash Trees and Spreading Correa (ground cover) below. It is noted that planting is proposed in the designated location of the water tank as shown on the development plans. A condition will require the water tank to be shown on the landscape plan and planting provided around the water tank area.

External Amenity Impacts

Visual Bulk

The location of the proposed building within in a Commercial 1 Zone without any direct sensitive residential interfaces minimises any off site impacts from the development. The closest residential property is approximately 18 metres from the title boundary of the site. Therefore visual bulk impacts will be limited typically to residential properties on the southern side of Bogong Avenue. Areas of secluded private open space are greater than 38 metres from the site, limiting any visual bulk impacts to these spaces. The proposed development will maintain a generous front setback of 5.2 metres with a substantial amount of existing canopy planting to be retained. Visual interest has been provided to the building with the use of mesh screening and internal colouring to the lift/stair core which will be visible external to the building. Whilst the building is functional, the architectural design ensures that visual bulk has been appropriately treated with building materials, design, landscaping, articulation and setbacks.

Overshadowing and daylight to windows

Shadow diagrams have been prepared with the application at 9am, 12 noon and 3pm during the September Equinox. The plan shows that at 9am, the shadowing is limited to the street, with the exception of a small area within the front setback of 20 Bogong Avenue. At 12 noon, the shadow from the core area shifts and a small portion of additional shadowing is presented within the front setback of 16 Bogong Avenue, and at 3pm a small portion of additional shadowing is located at 10-12 Bogong Avenue. In all of these instances, the shadow is limited to the front

setback of the site (maximum 2.7sqm) and does not affect any secluded private open space.

Whilst the proposal does overshadow the footpath directly in front of the building, it is noted that the existing building shadows most of the footpath during the September Equinox. Shadowing to the southern side of the footpath is limited to those associated with the lift/stair core. The proposed shadowing is considered appropriate given the public realm benefits that the proposal provides and that Bogong Avenue is not identified as a key public walkway.

Overlooking

The proposed building will be located at least 18 metres from any adjoining residential property and will therefore have no overlooking impacts.

Equitable Development Potential

The site abuts commercial properties to the north, east and west. The building is set back from all boundaries of the site. The building is set back a minimum of 1.72 metres from the northern boundary of the site which is the most sensitive interface in terms of future development. The proposed building is not for a sensitive use, and therefore shadows cast onto the site in the future are not problematic.

Car Parking, Traffic and Access

Vehicle Access

The vehicle access arrangements to the site remain unchanged. Vehicle access to the upper levels will remain via the eastern access to Bogong Avenue and internally via the continuation of the circulation ramp which will connect levels 1-7.

Car Parking Design

The proposed car parking spaces, accessways and ramps have generally been designed in accordance with Clause 52.06 with the exception of the some existing 'small car spaces'.

These small parking spaces measure 4.9m long x 2.3m wide. This is 0.1m less than the length required by AS/NZS 2890.1:2004 for small car parking spaces and are impacted by internal structural walls. Some of these spaces are identified on plans and a condition will require clear signage to alert drivers that these spaces are smaller than standard. There are a total of 25 small car parking spaces proposed.

It has been identified by Council's traffic engineers that some additional existing spaces do not meet standard car parking design requirements and are therefore to be noted as 'small car spaces'. These include spaces on the south side of the isle adjacent to solid structural walls where accessibility is reduced. It is anticipated that this will result in an additional 11 spaces being identified as small spaces. Additional conditions have been recommended by Council's traffic

engineers requiring relocation of fire hose and reels and associated structures at Levels 4 to 7 to avoid reduction of the access aisle widths and line marking to be provided to spaces north of the stairwell (ST02) on Level 6 which appears to be a drafting error.

Bicycle Parking

A total of 6 bicycle hoops are proposed, allowing for 12 parked bicycles at the ground floor. These spaces have slightly different dimensions to the current requirements of Clause 52.34 of the Monash Planning Scheme, but as they are to be retrofitted into the existing car park this has been assessed as satisfactory by the Council Traffic Engineers.

Traffic Generation

Parking restrictions within the existing car park vary level by level. The future parking restrictions are subject to determination by Council. Given this, for each peak hour the highest traffic generation rate of either ground level or upper-level parking was utilised. This provides for a conservative assessment, noting that the exact future car parking restrictions are not known at this time.

Existing studies for the car park revealed that Upper levels had the highest traffic generation rate during the Weekday AM peak hour, this is likely due to all day staff/trader parking representing a greater proportionality of movements during this period. The ground level had the highest traffic generation rate during the Weekday PM and Saturday Midday peak hours, this is likely due to this parking being the most proximate/attractive to motorists and that ground level has the shortest time restriction within the car park thus resulting in a higher turnover of spaces.

The existing and anticipated traffic generation for the development is outlined in the table below:

	Current Traffic Generation (vehicle movements)	Proposed Traffic Generation (vehicle movements)
Weekday AM Peak	33	61
Weekday PM Peak	99	318
Saturday Midday Peak	145	650

A traffic analysis was undertaken which indicated the additional traffic generation can be incorporated within the existing surrounding road network and intersections, with all post development queues and delays at site access points relatively modest and considered to be within acceptable limits.

Council's Traffic Engineers have recommended additional "Give Way" signage be provided at all vehicle access points along Bogong Avenue to ensure motorists give way to the expected increase in pedestrians walking to and from the car park along Bogong Avenue. This can be addressed as a permit condition.

Signage

The proposed electronic signage will provide information related to the number of available car parking spaces within the building. The sign will be flat mounted to the building above the two entrances. The signs serve a functional purpose, identifying available car spaces, and does not include any promotional or business identification.

The signs will be internally illuminated and will not result in any unreasonable light spill to residential properties on the opposite side of Bogong Avenue. Proposed conditions will manage light spill from the sign and ensure no unreasonable impacts.

Given the above, the signs are considered acceptable.

Objections not previously addressed

The objection received requests Council to delete existing car parking spaces on the southern side of Bogong Avenue (noting that no objection was lodged in relation to the actual development of the building). Concern is raised with respect to existing parking preventing sight lines when exiting their property onto Bogong Avenue. Concern is raised that additional traffic will worsen this situation. Although this issue is not affected by the car park extension, Council's Traffic Engineers have advised that as Bogong Avenue is two lanes in both directions, a driver is able to exit the residential property by edging out into the lane closest edge of the road to see past a parked vehicle and no changes are recommended to on street parking.

It is noted that the existing car parking spaces are not located within the subject site and does not form part of this application. The existing local ring road for the Glen Waverley Activity Centre consists of Bogong Avenue, Myrtle Street and Coleman Parade (between Myrtle Street and Kingsway). Bogong Avenue will be maintained as a two-way traffic in order for the activity centre to function well. On-street parking along Bogong Avenue is not only providing parking for guests coming to the activity centre, but it also provides parking for visitors to the dwellings also. Council's engineering department have noted that they will review the traffic and parking situations after the completion of the car park extension and the proposed changes to the road network by the Suburban Rail Loop.

CONCLUSION:

The proposal is appropriate given the zoning of the land and the proposed design response which is generally consistent with the objectives and requirements of the relevant state, regional and local policies of the Monash Planning Scheme.

The proposed car park will allow for consolidation of existing car parking on the site within the Glen Waverley Activity Centre. The building design in addition to the maintenance of the landscaped front setback and high quality robust materials

will allow for the building to sit comfortably within the streetscape and will have minimal impact on the amenity of adjacent and nearby residential areas.

It is recommended that a Notice of Decision to Grant a Permit be issued subject to conditions.

LIST OF ATTACHMENTS:

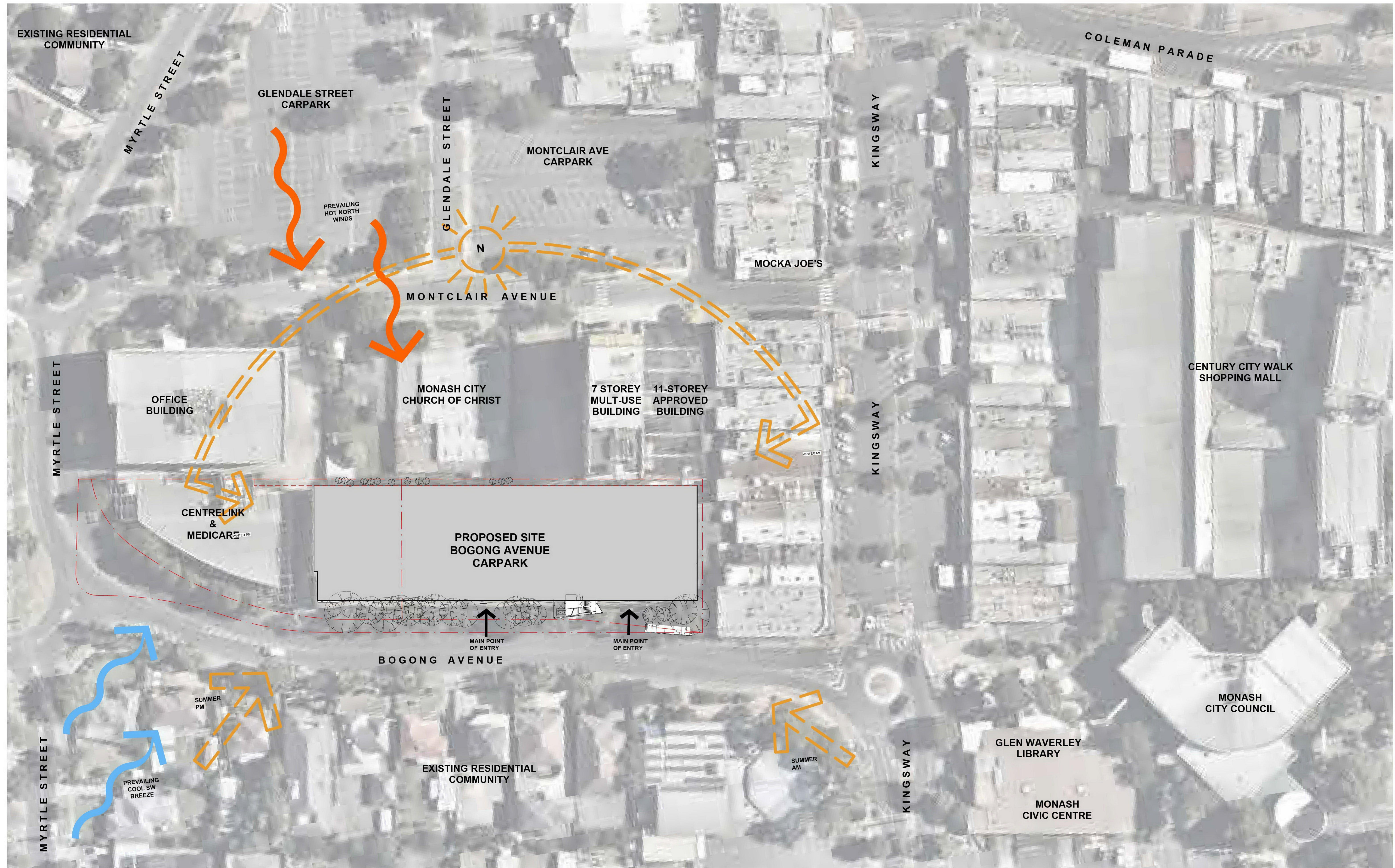
Attachment 1 – Proposed Development Plans.

Attachment 2 – Aerial Photograph (February 2022).

Attachment 3 – Zoning and Overlays Map.

Attachment 4 – Objector Properties Location Map.



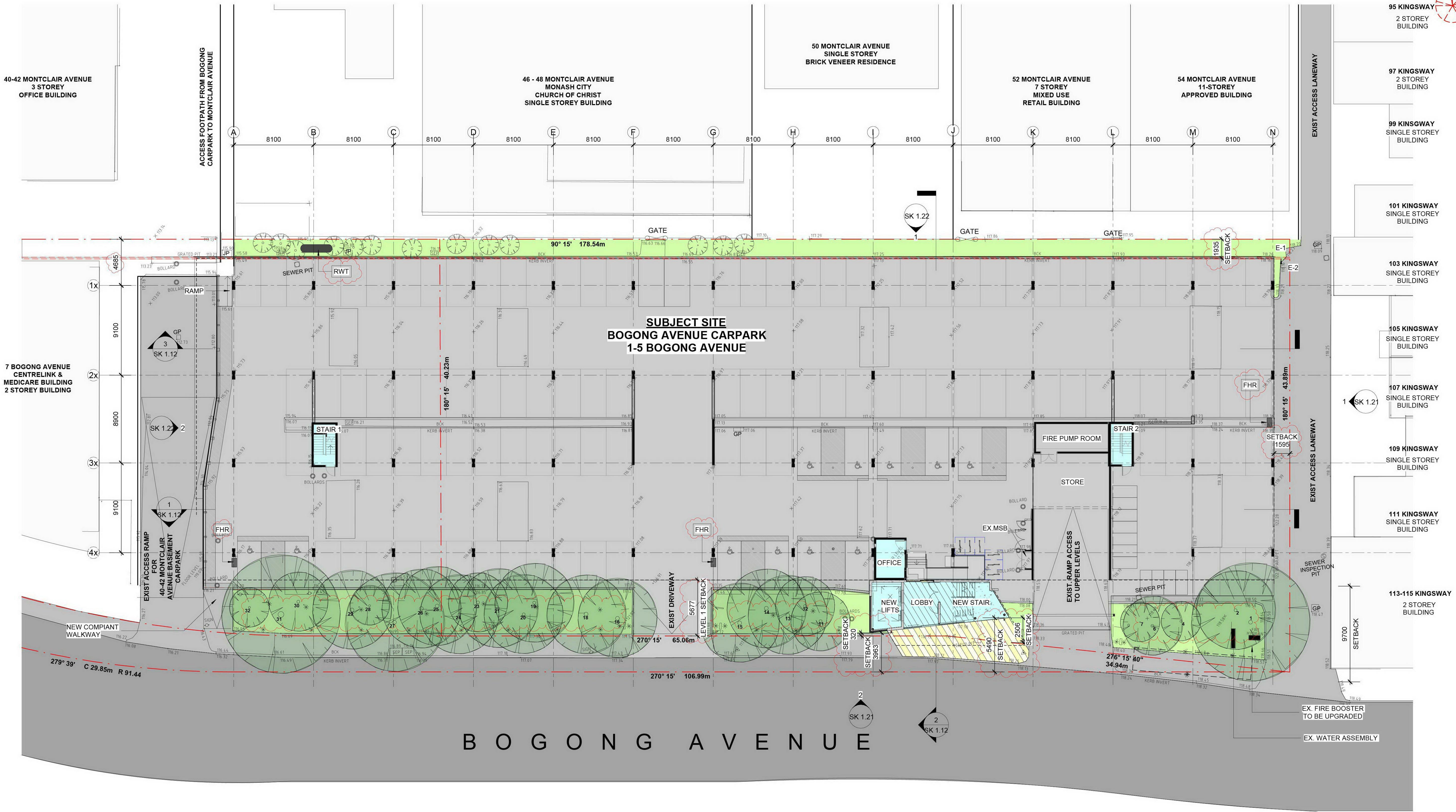


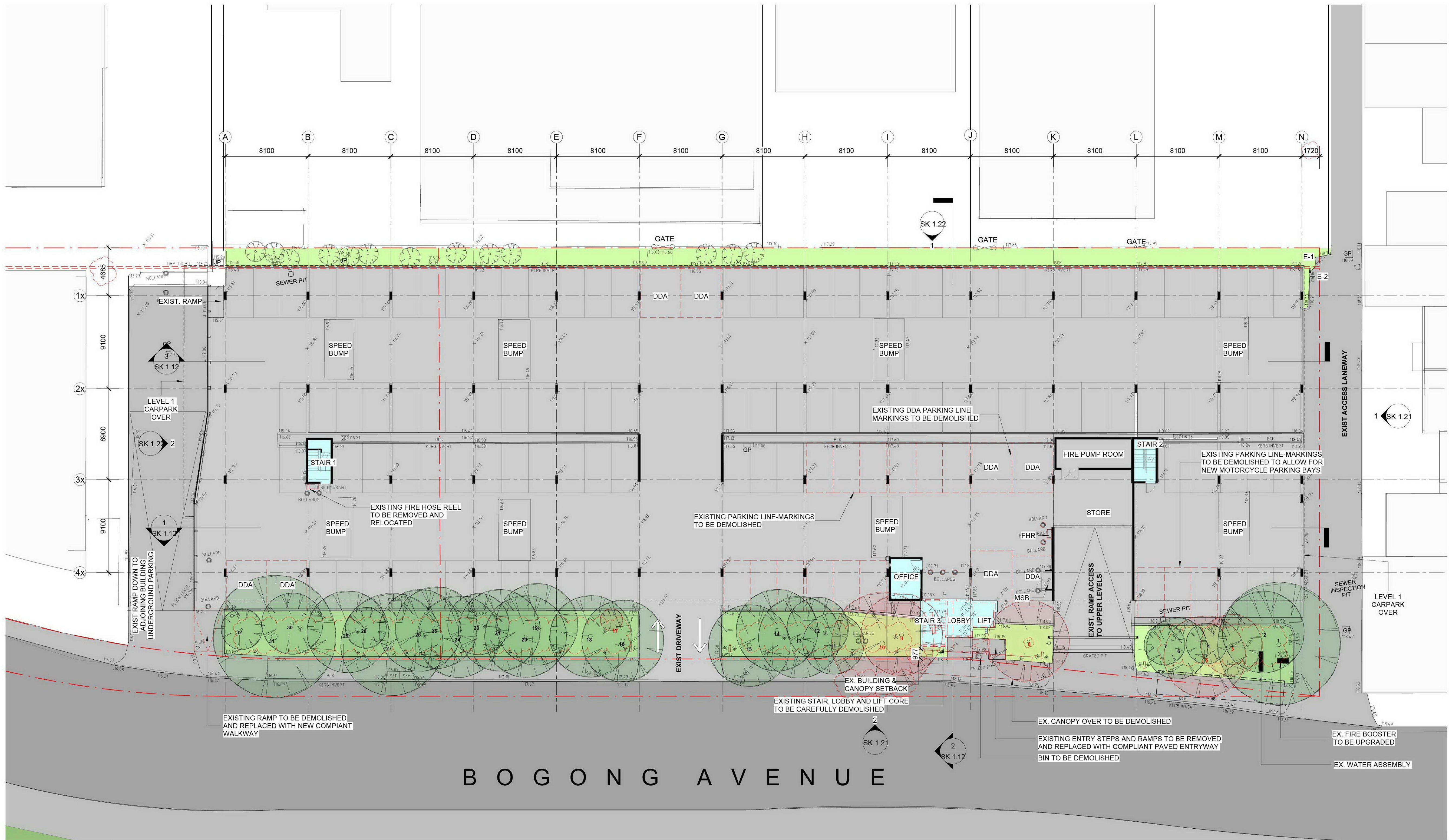
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- LEGEND
- B EXISTING BOLLARD
 - DP EXISTING DOWNPIPE
 - EJ CONSTRUCTION JOINT
 - MSA MAIN ELECTRICAL SWITCHBOARD
 - EDB ELECTRICAL DISTRIBUTION BOARD
 - FHR FIRE HOSE REEL

EXISTING TREE TO BE RETAINED

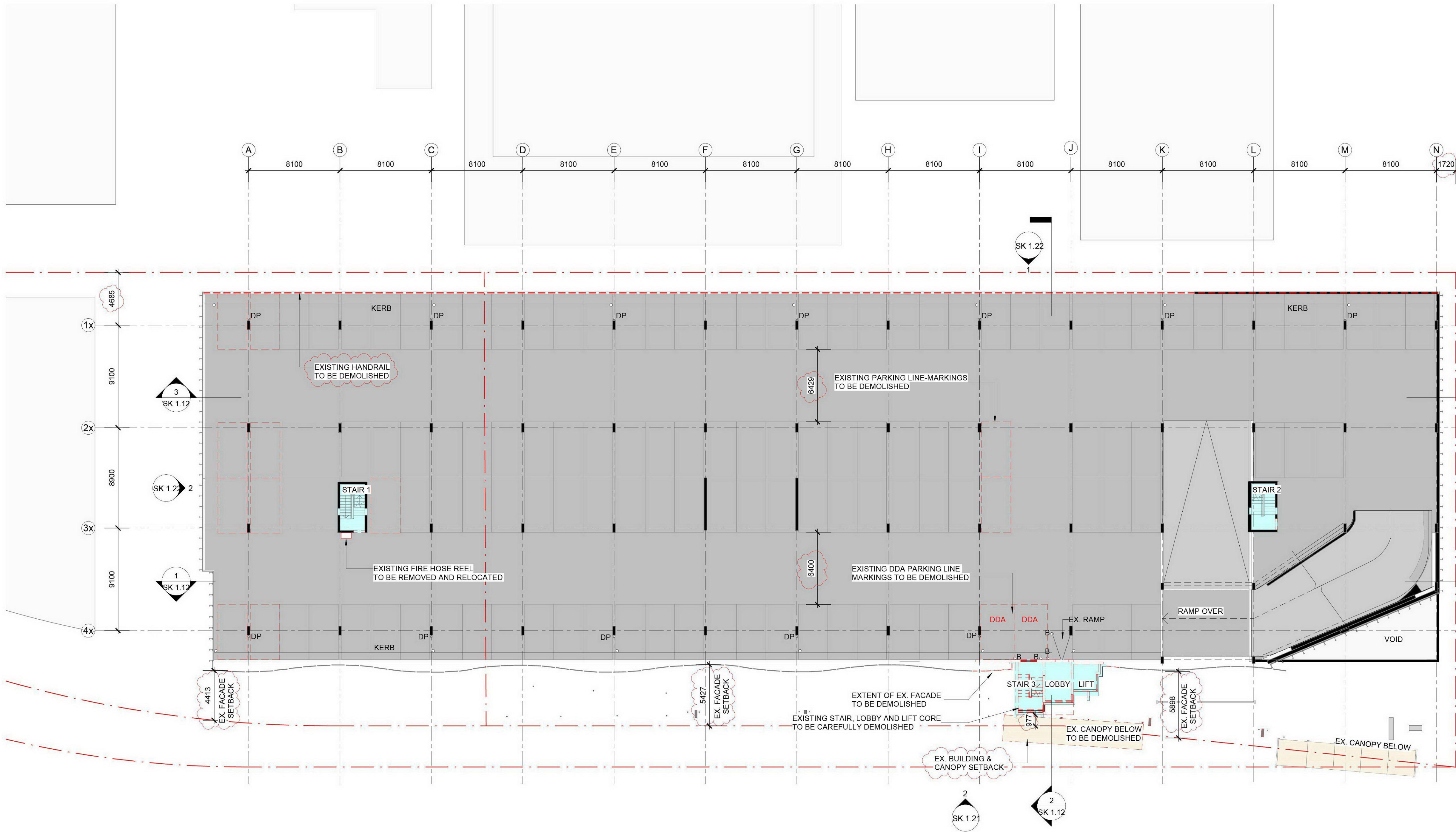
EXISTING TREE TO BE DEMOLISHED



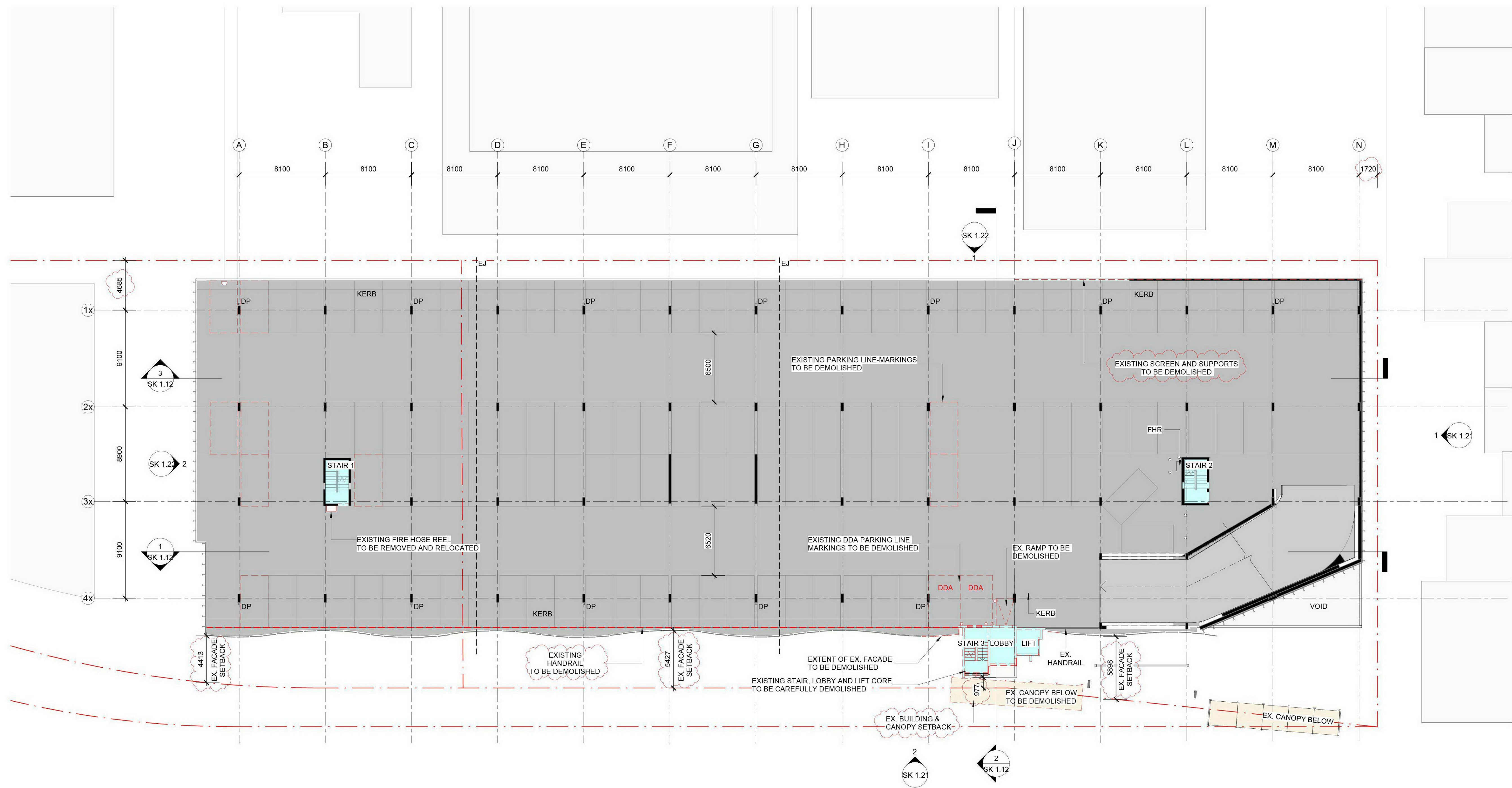


- LEGEND**
- B EXISTING BOLLARD
 - DP EXISTING DOWNPIPE
 - EJ CONSTRUCTION JOINT
 - MSA MAIN ELECTRICAL SWITCHBOARD
 - EDB ELECTRICAL DISTRIBUTION BOARD
 - FHR FIRE HOSE REEL
 - (Green Circle) EXISTING TREE TO BE RETAINED
 - (Red Star) EXISTING TREE TO BE DEMOLISHED

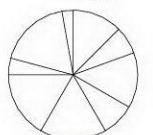
TREE PROTECTION NOTE:
GARDEN BEDS SURROUNDING EXISTING TREES TO SOUTH BOUNDARY OF SITE ARE TO BE TREATED AS TREE PROTECTION AREAS.
NO ITEMS/ PLANT/ MACHINERY ARE TO BE STORED BENEATH TREES OR IN GARDEN BEDS.



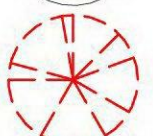
LEGEND	
B	EXISTING BOLLARD
DP	EXISTING DOWNSIDE
EJ	CONSTRUCTION JOINT
MSA	MAIN ELECTRICAL SWITCHBOARD
EDB	ELECTRICAL DISTRIBUTION BOARD
FHR	FIRE HOSE REEL
	EXISTING TREE TO BE RETAINED
	EXISTING TREE TO BE DEMOLISHED


LEGEND

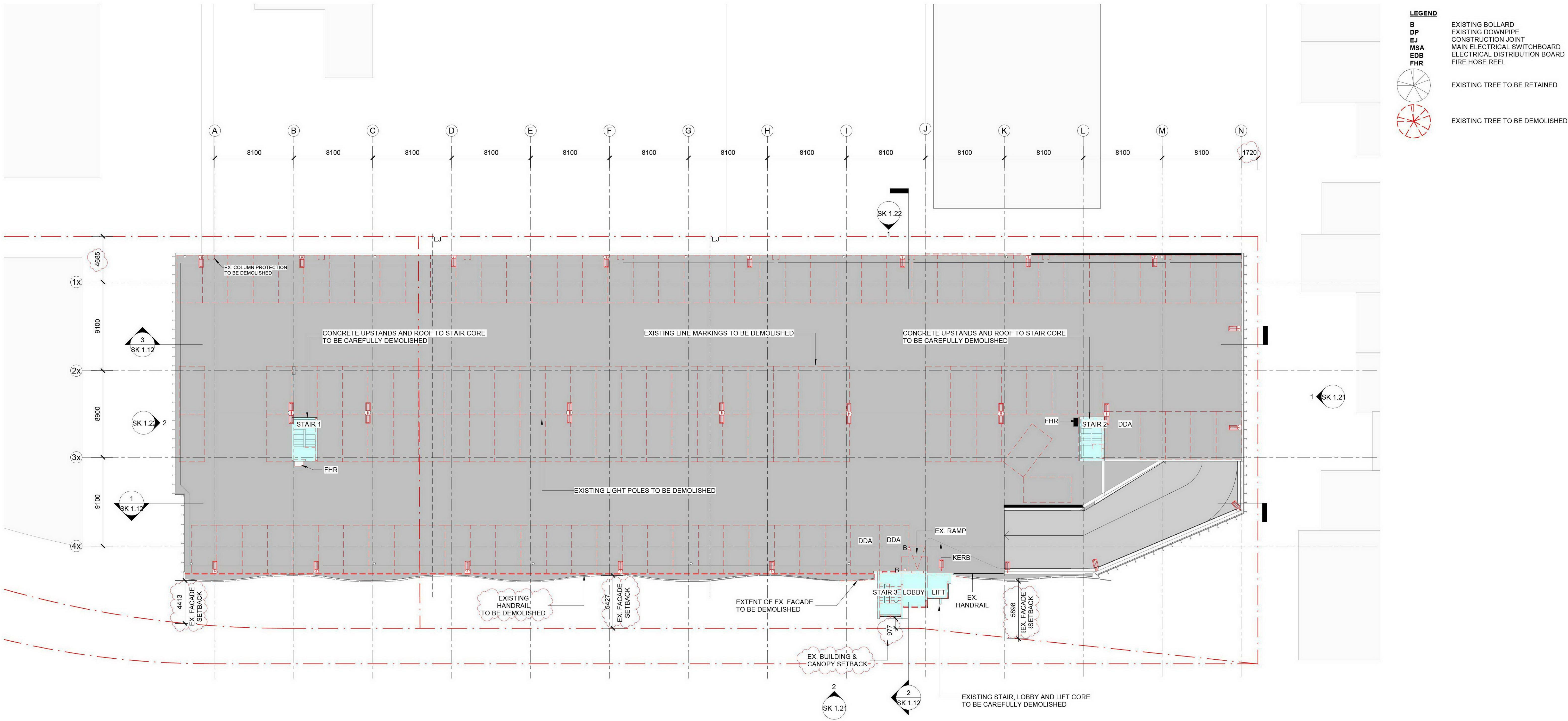
B EXISTING BOLLARD
DP EXISTING DOWNPIPE
EJ CONSTRUCTION JOINT
MSA MAIN ELECTRICAL SWITCHBOARD
EDB ELECTRICAL DISTRIBUTION BOARD
FHR FIRE HOSE REEL

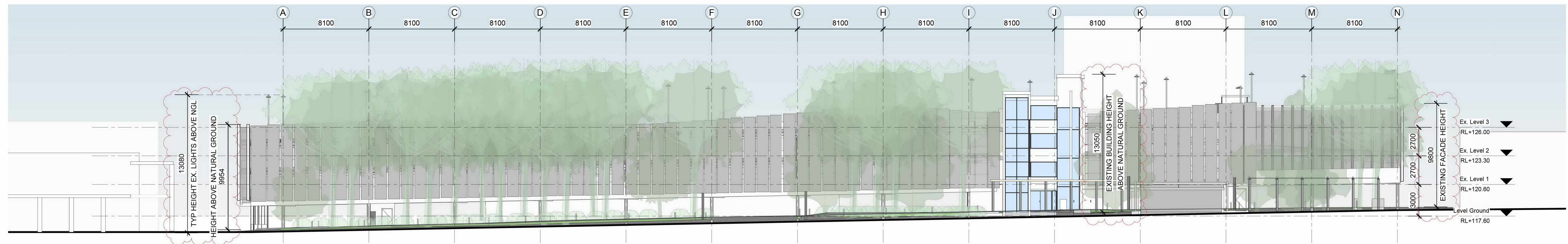


EXISTING TREE TO BE RETAINED

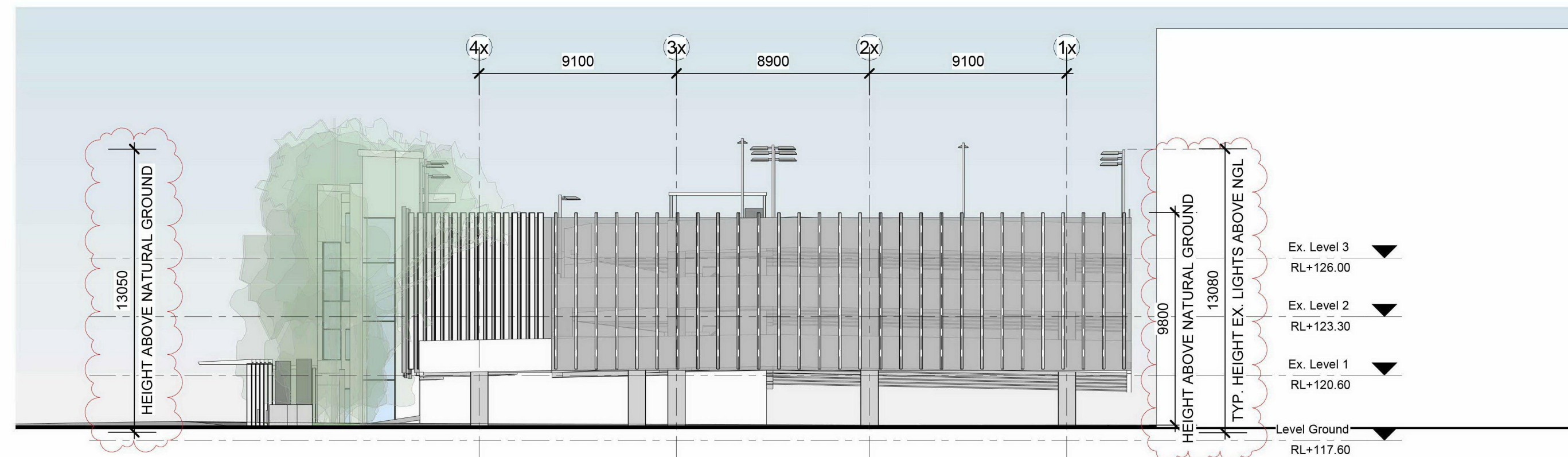


EXISTING TREE TO BE DEMOLISHED



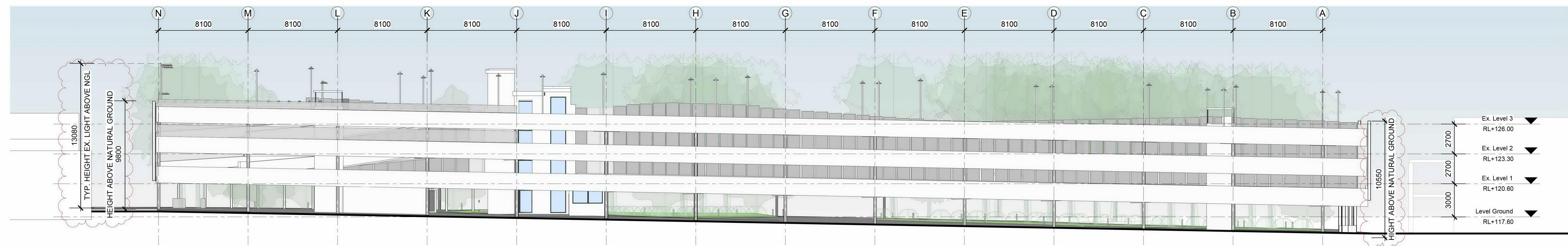


① South Elevation
1 : 200

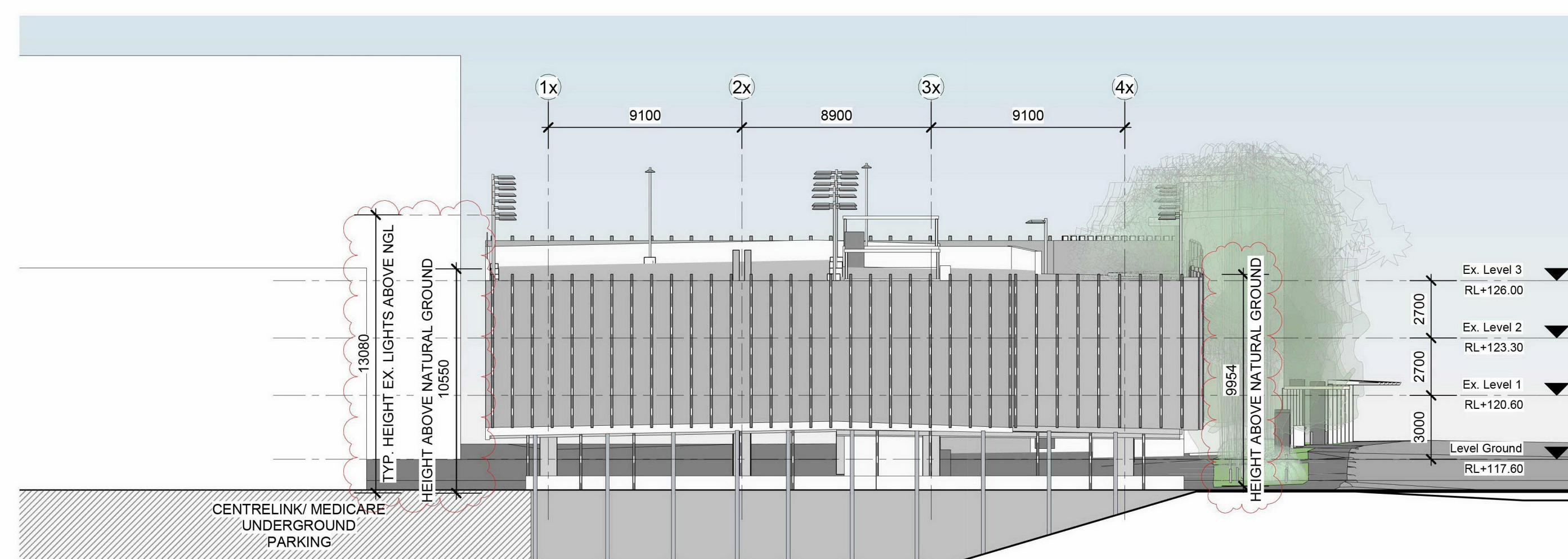


② East Elevation
1 : 200

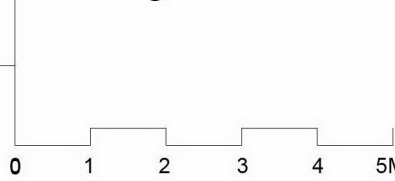
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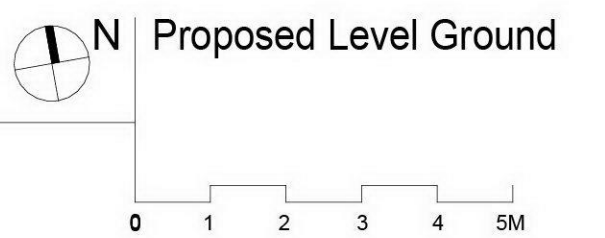


③ North Elevation
1 : 200



④ West Elevation
1 : 200



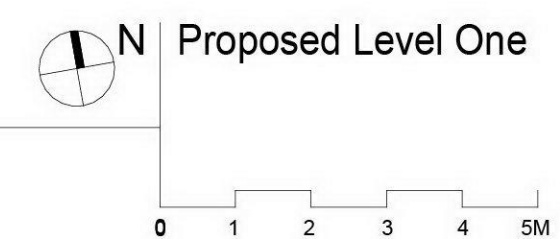




BOGONG AVENUE

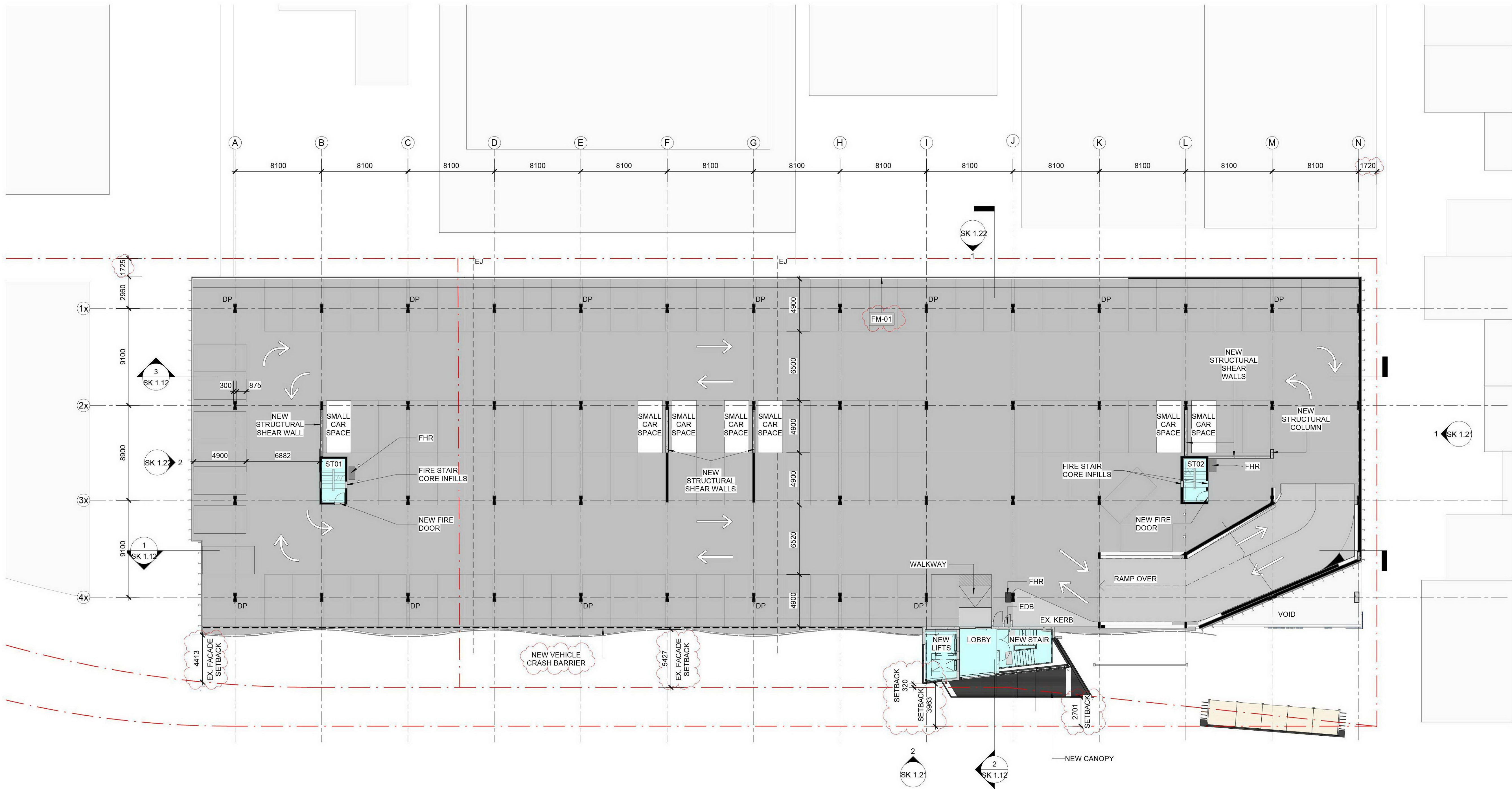
Bogong Carpark Extension

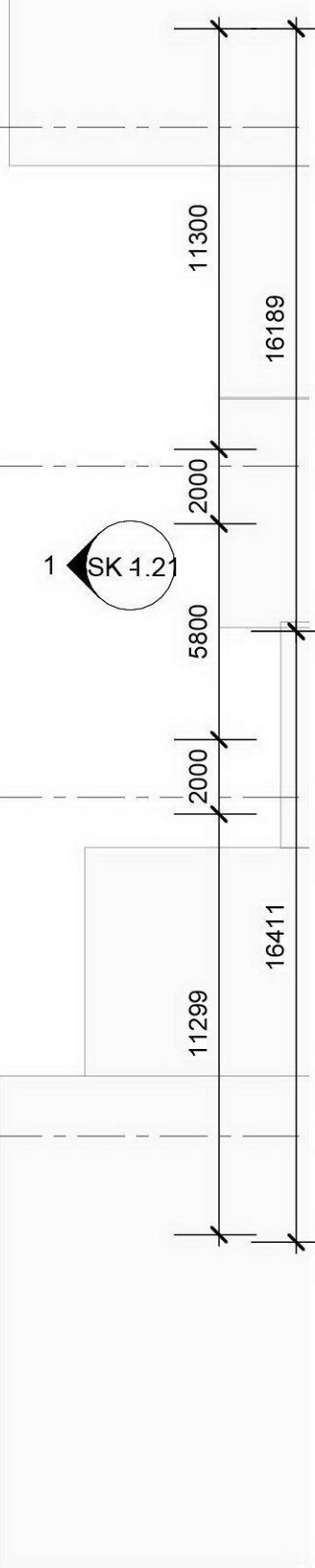
1-5 Bogong Avenue, Glen Waverley

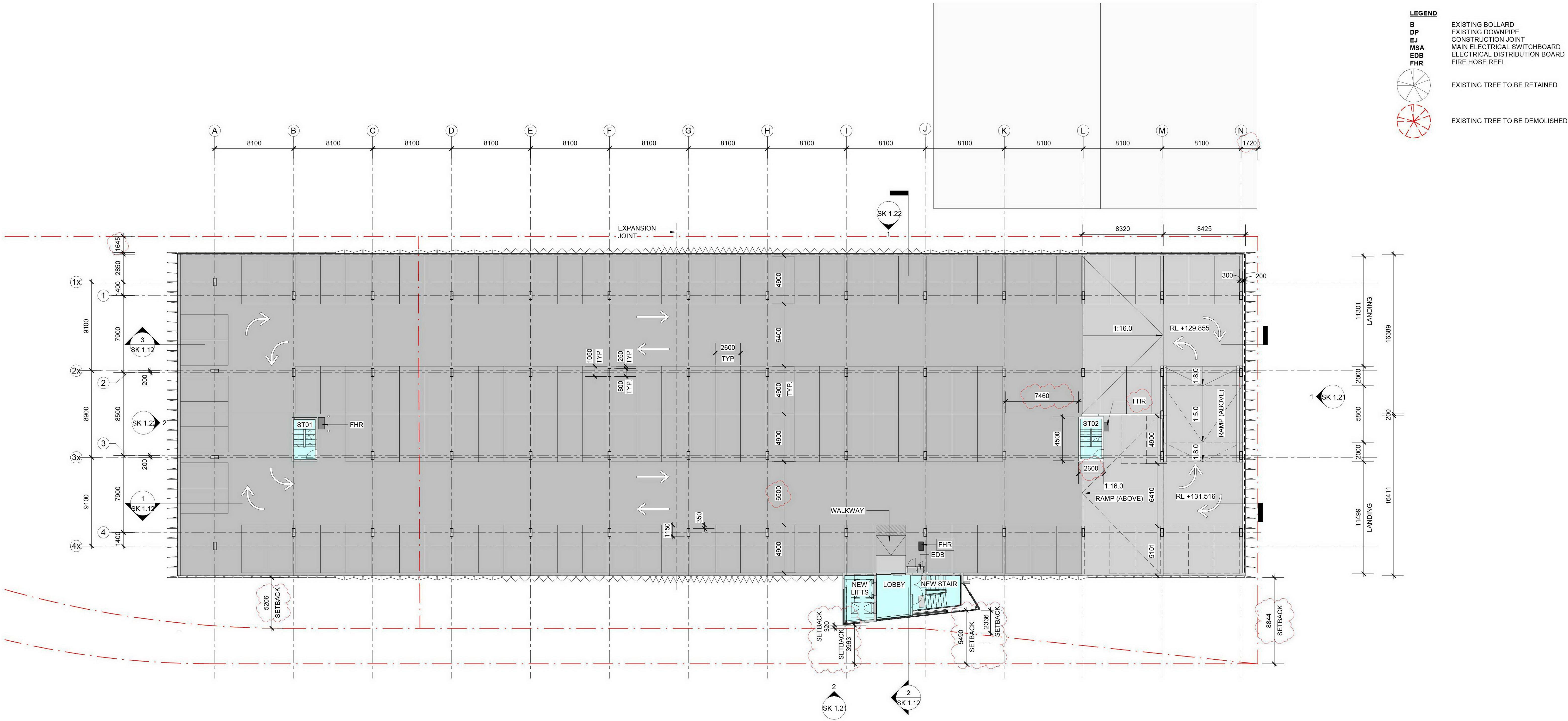


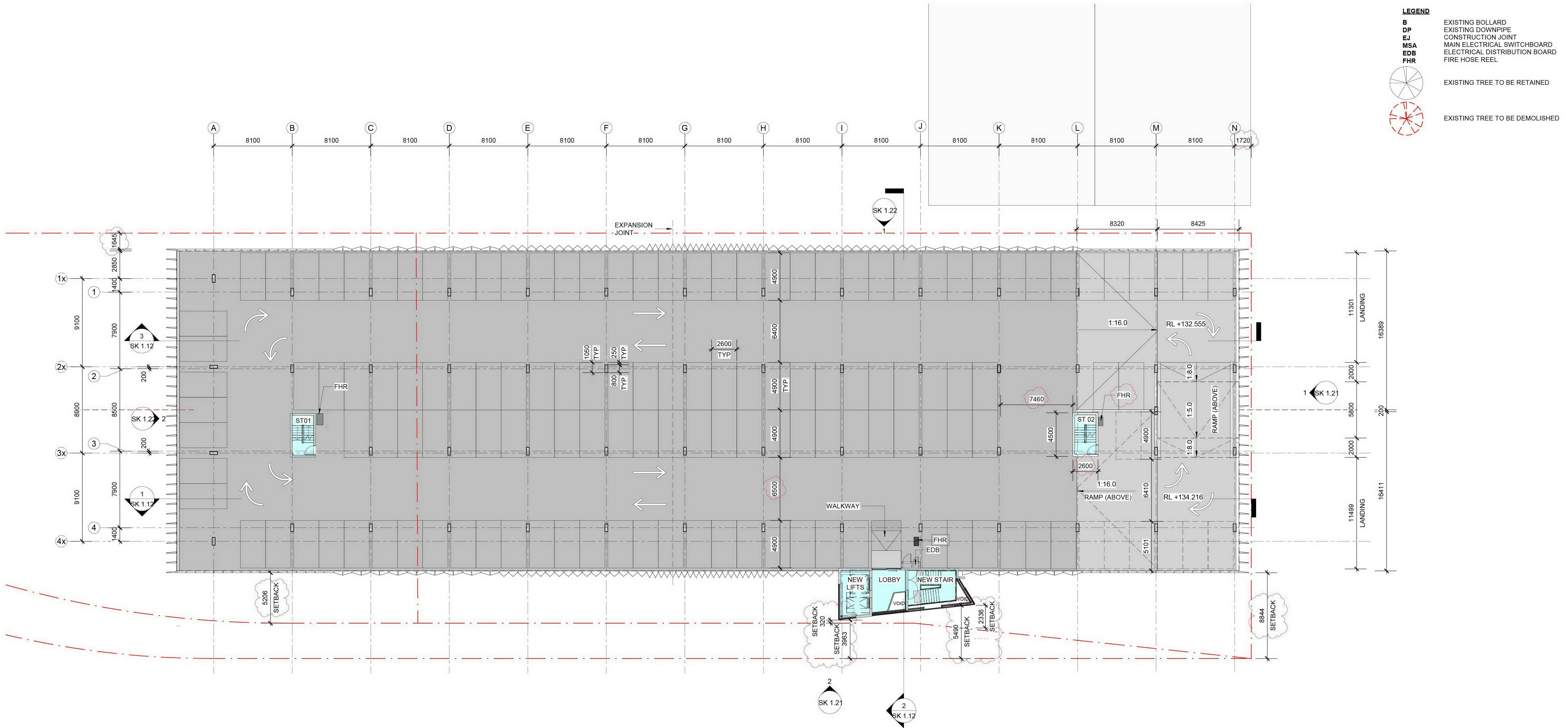
SK 1.02

Planning RFI
Revision: A
9/08/22
1 : 200@A1

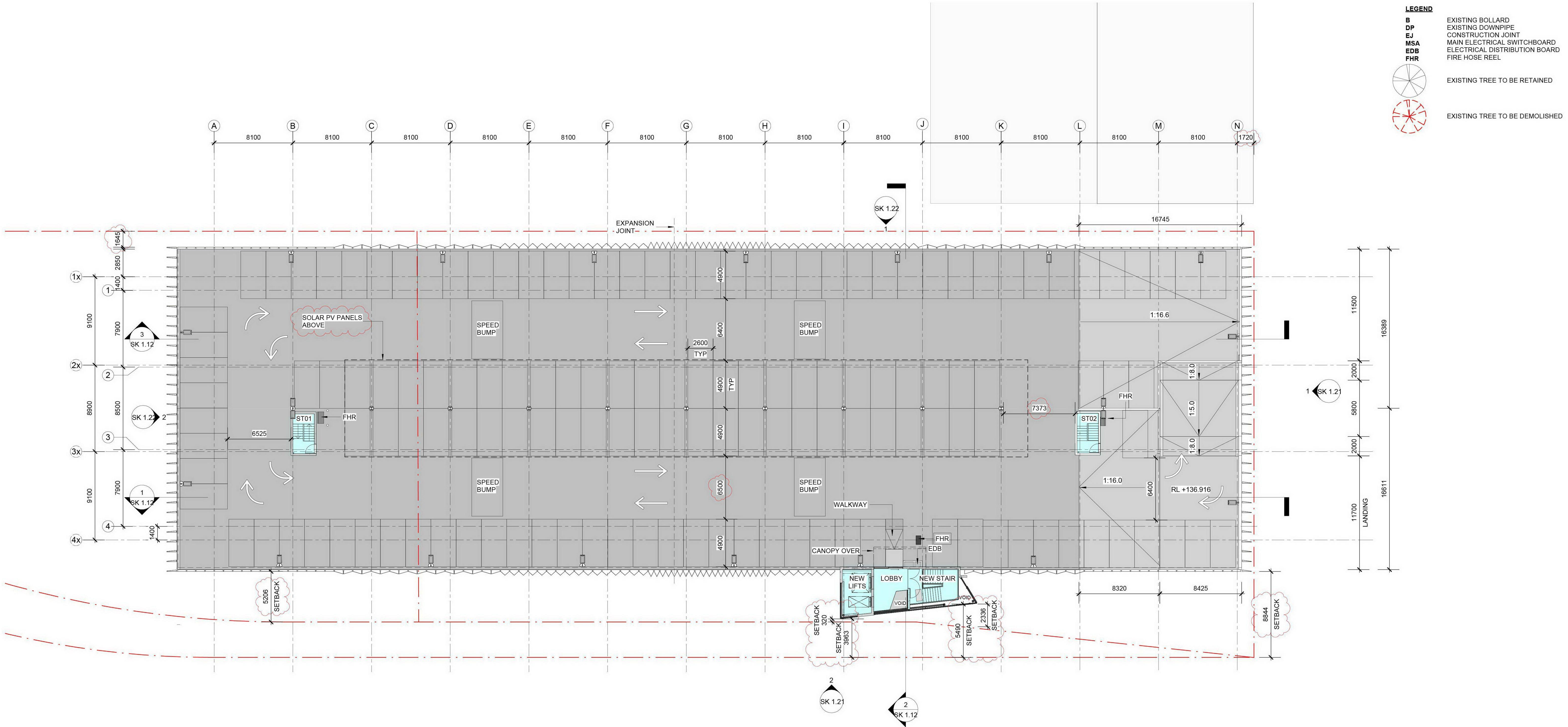




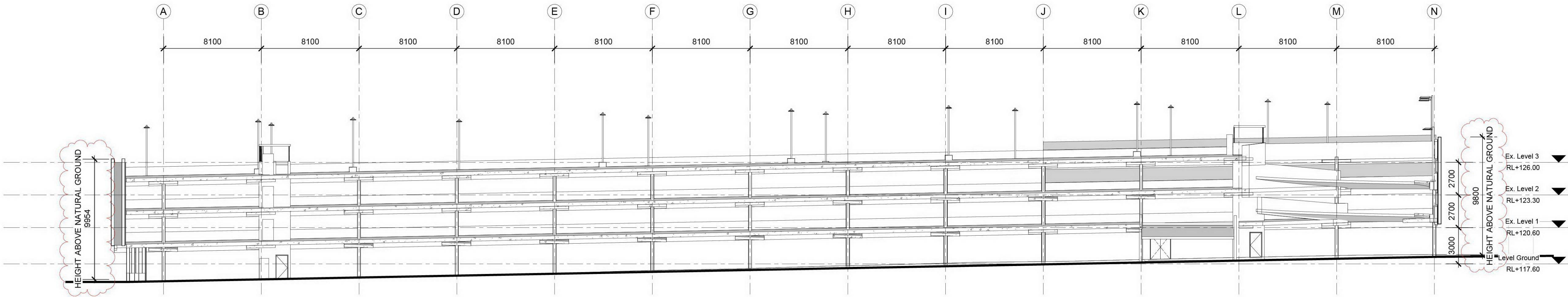




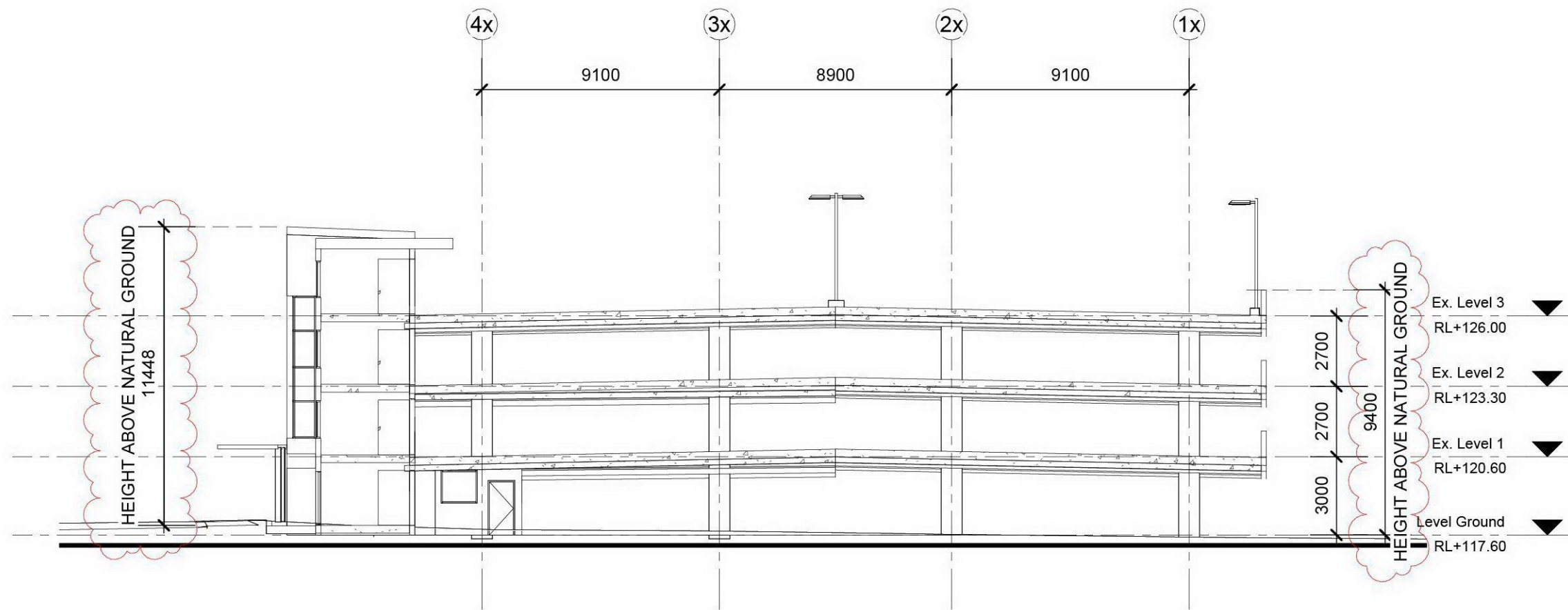




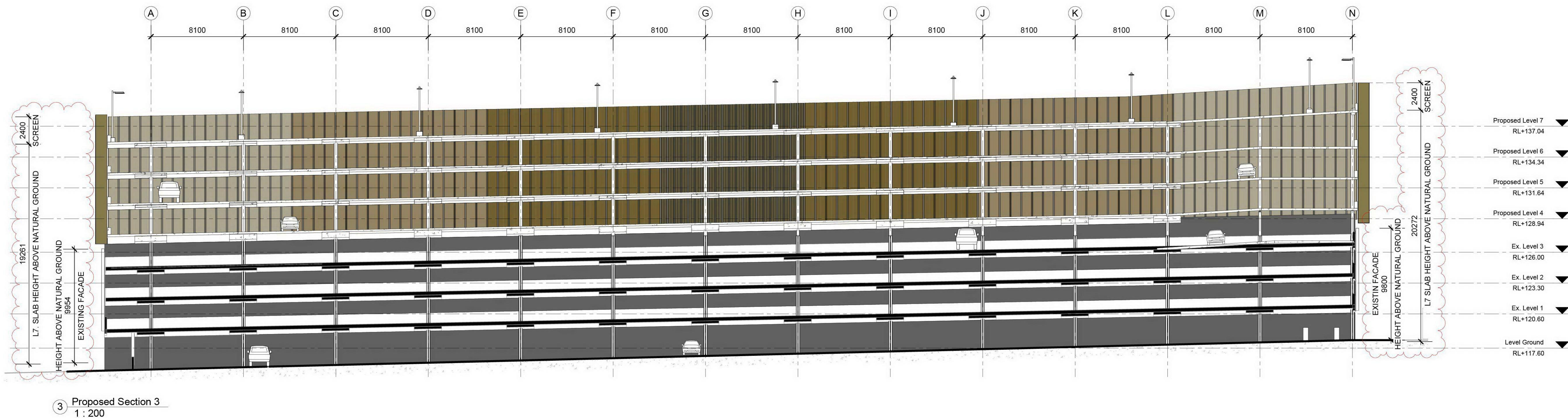
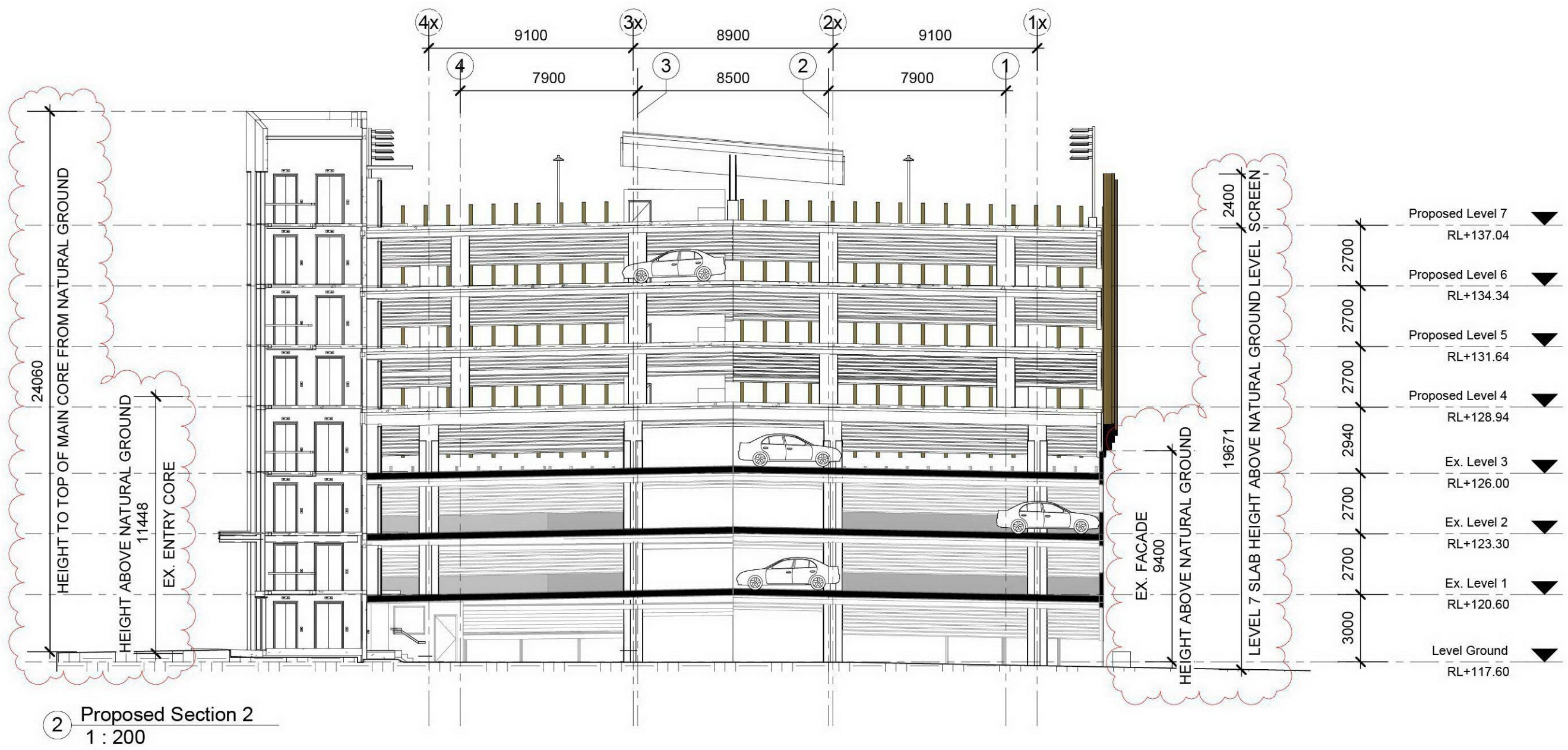
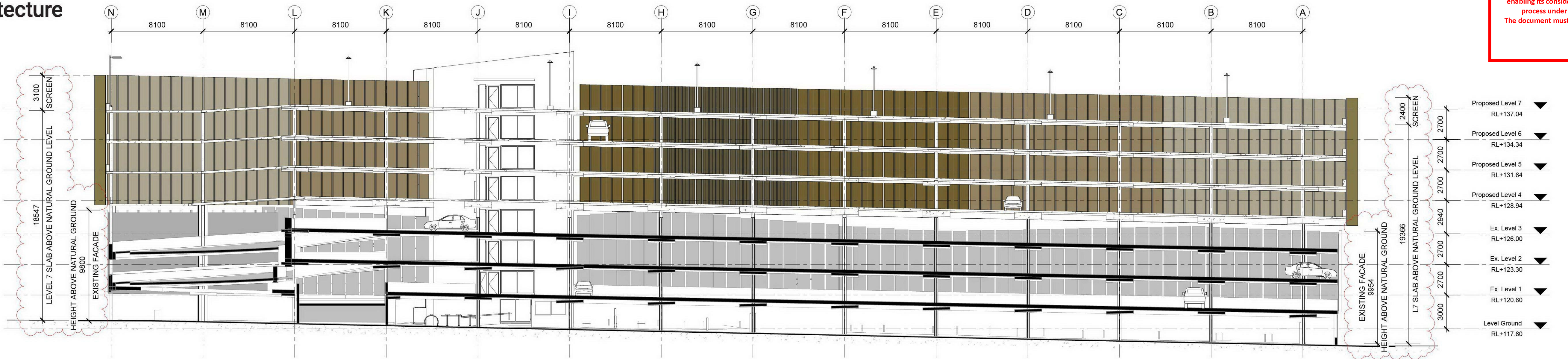
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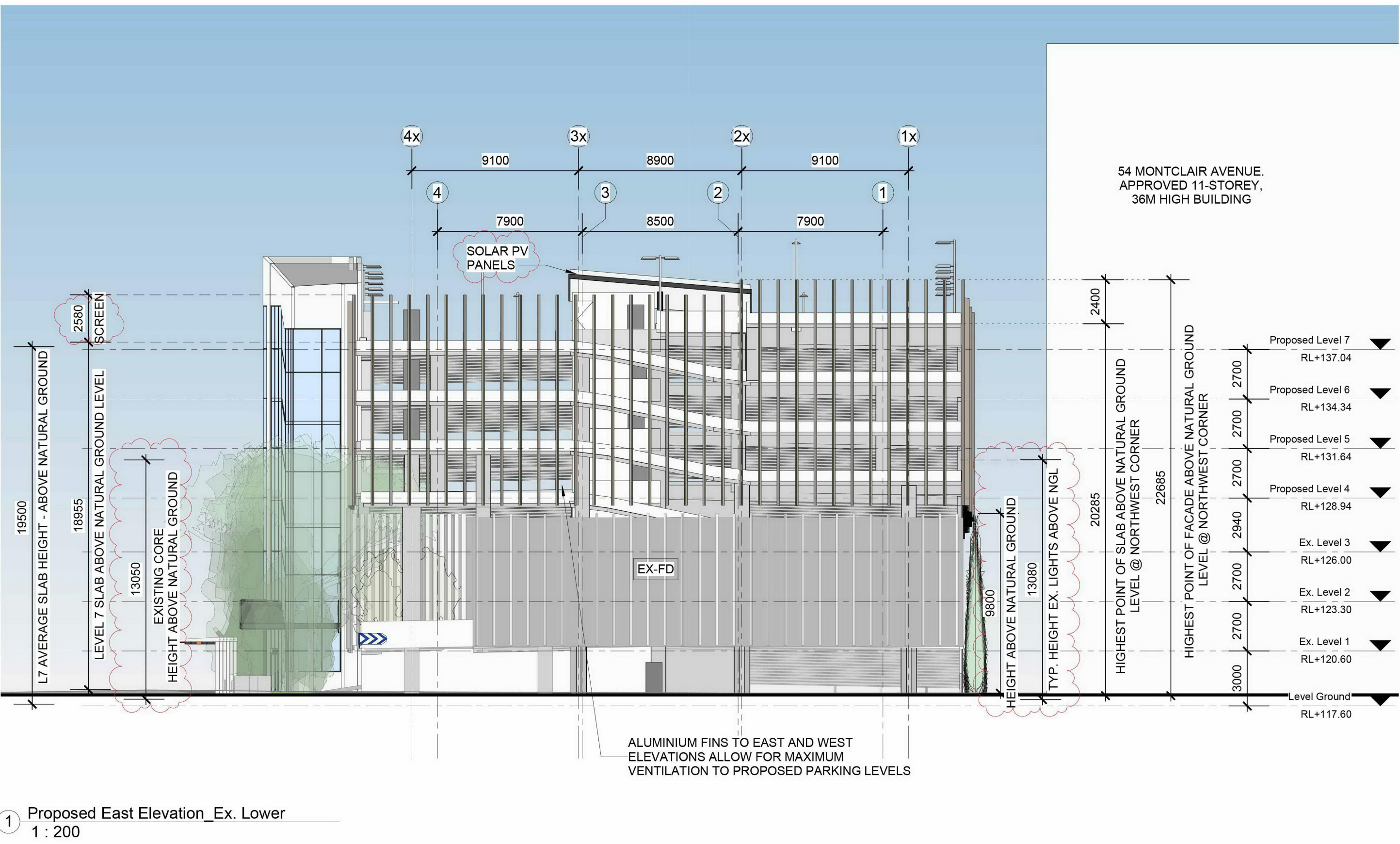
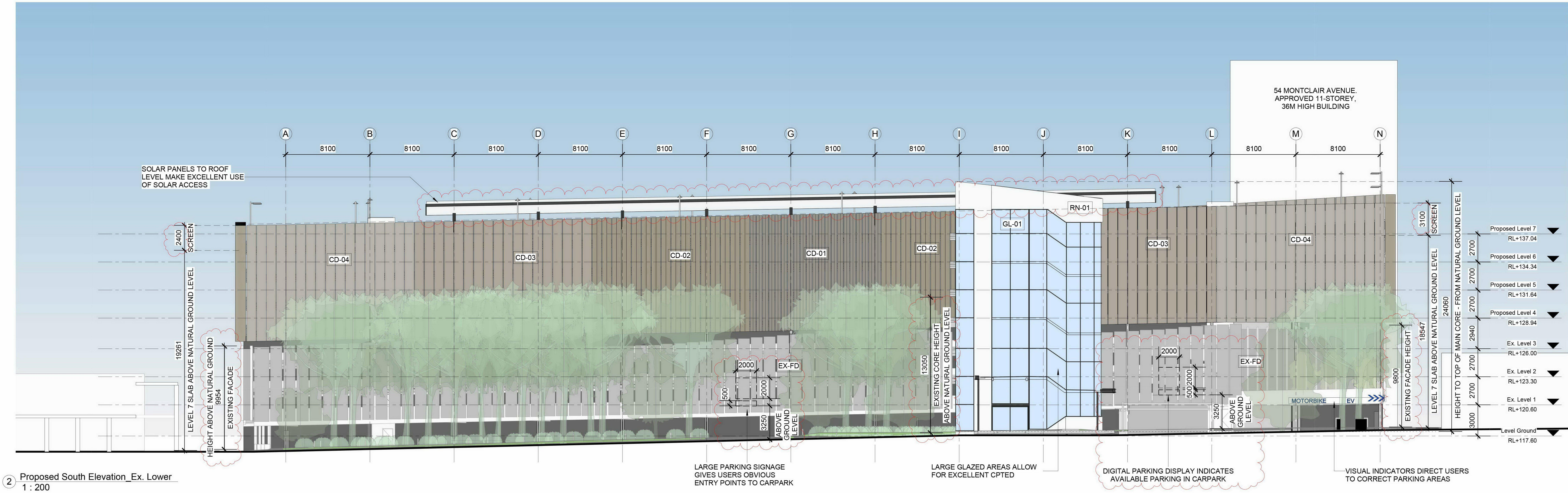


2 Ex. Section 1
1 : 200

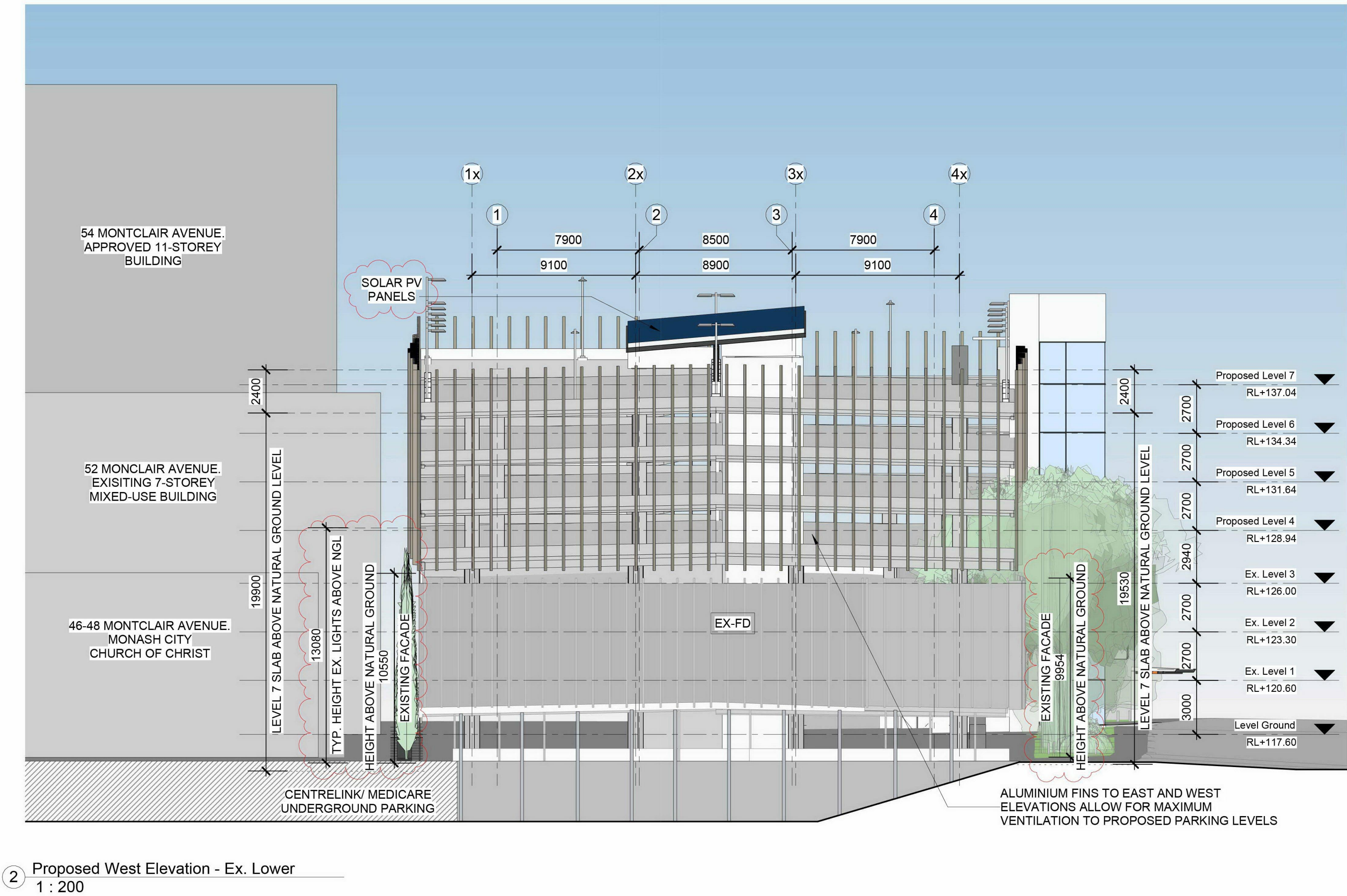
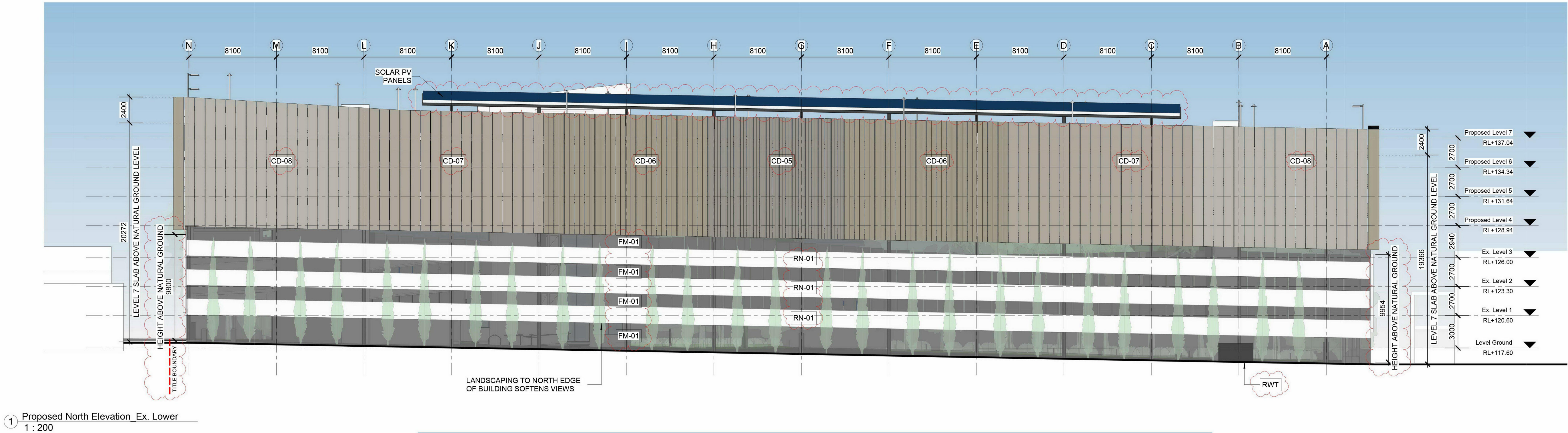


1 Ex. Section 2
1 : 200



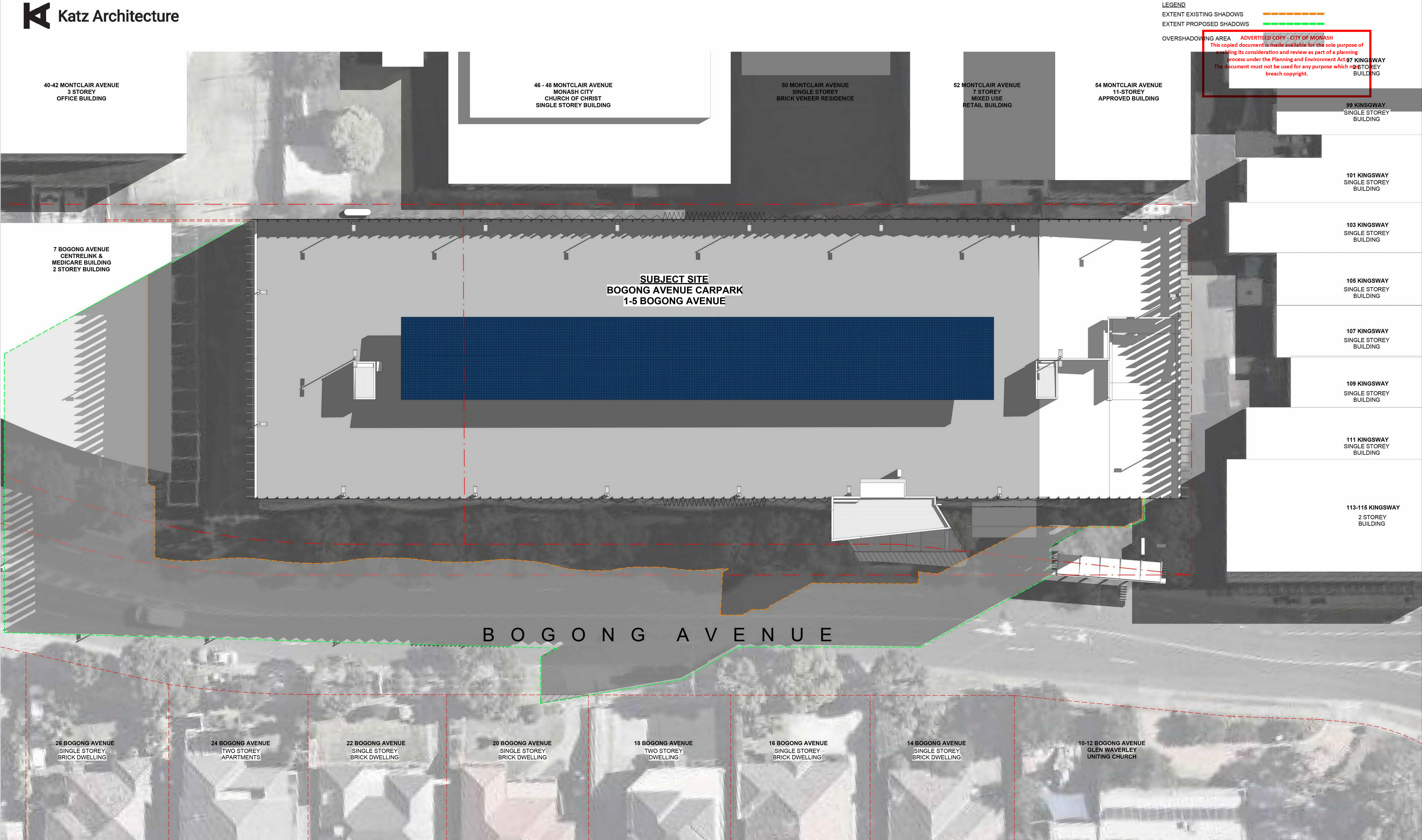


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NOTE:
SHADOW OF PROPOSED EXTENSION HAS NEGLIGEABLE IMPACT ON PROPERTIES SOUTH OF BOGONG AVENUE.
ENCROACHMENT AREA - 2.7m2

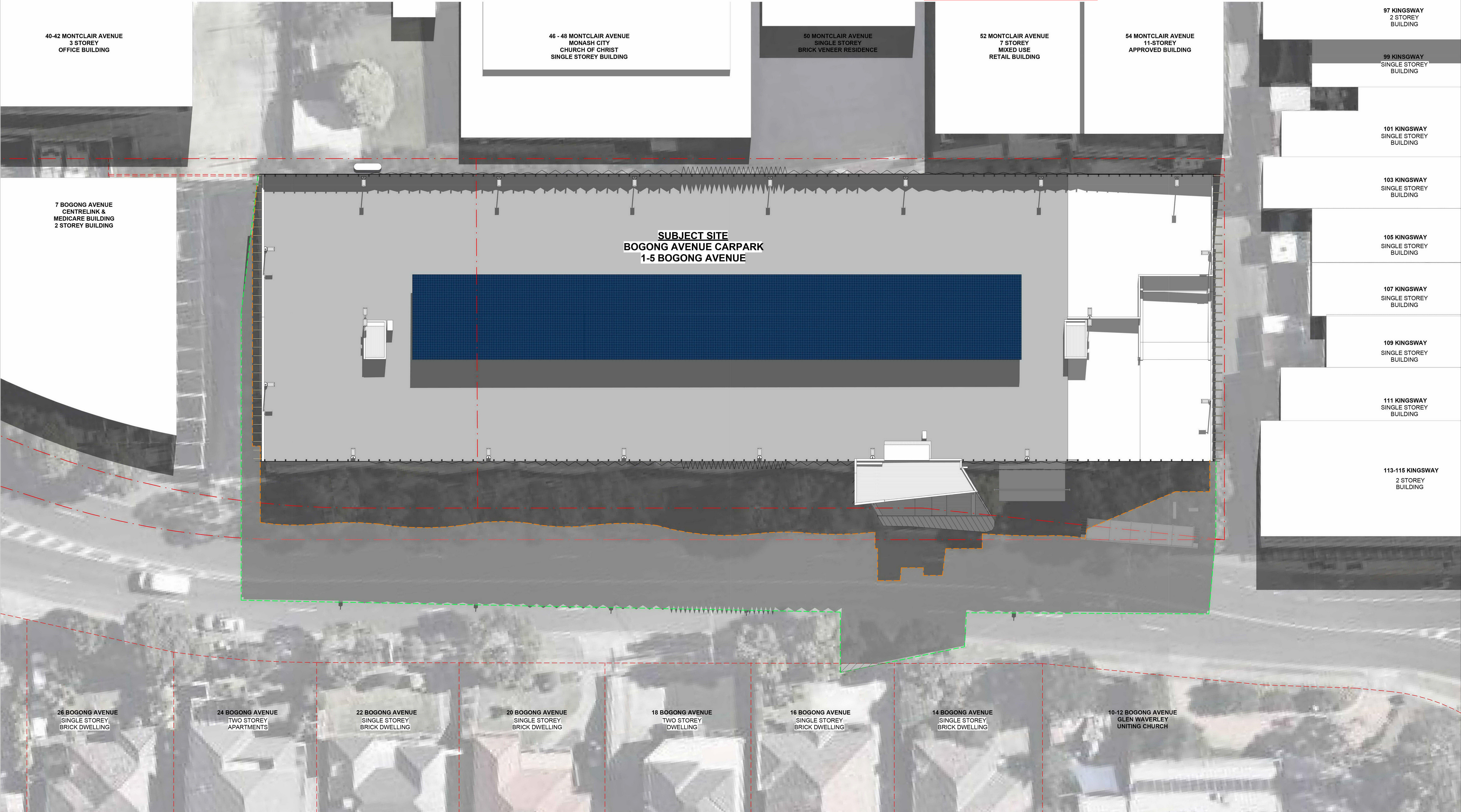
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LEGEND

EXTENT EXISTING SHADOWS

EXTENT PROPOSED SHADOWS

OVERSHADOWING AREA



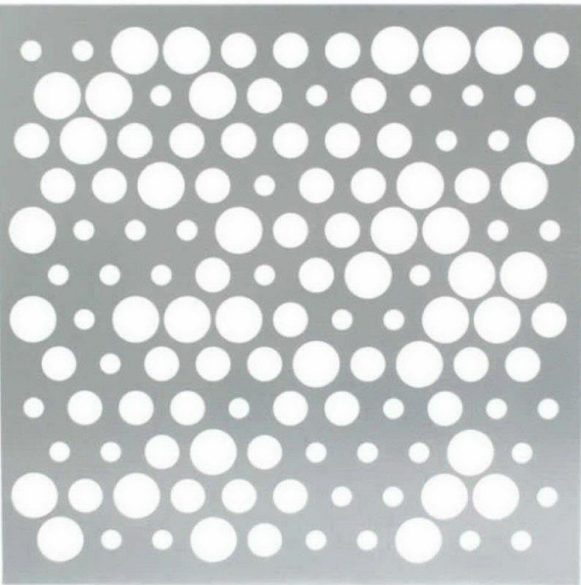
NOTE:
SHADOW OF PROPOSED EXTENSION HAS NEGLIGIBLE IMPACT ON PROPERTIES SOUTH OF BOGONG AVENUE.
NO NEW SHADOW IMPACTS TO NEIGHBOURING PROPERTIES (NEW SHADOW FALLS ON EXISTING ESTABLISHED STREET TREES)
ENCROACHMENT AREA - 2.5m2

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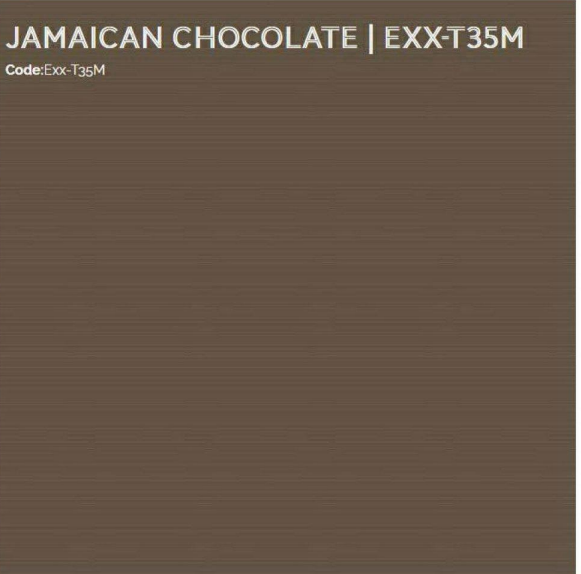
LEGEND
EXTENT EXISTING SHADOWS
EXTENT PROPOSED SHADOWS
OVERSHADOWING AREA



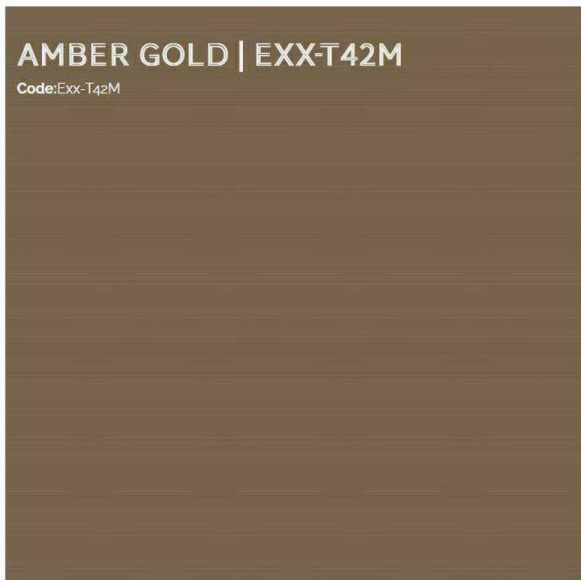
NOTE:
SHADOW OF PROPOSED EXTENSION HAS NEGLIGEABLE IMPACT ON PROPERTIES SOUTH OF BOGONG AVENUE.
ENCROACHMENT AREA - 0.25m2



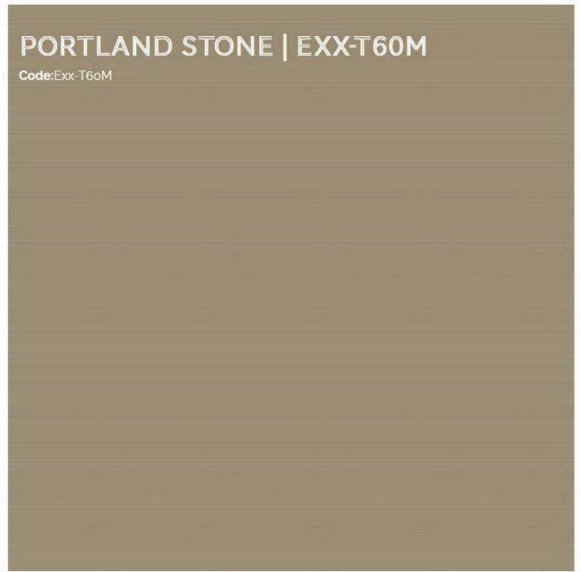
PERFORATED METAL FACADE WITH CUSTOM BOGONG MOTH IMAGERY IN ANODISED GRADED TONES NOTED BELOW.



CD-01 / CD-05 ANODISED ALUMINIUM IN FINISH: JAMAICAN CHOCOLATE



CD-02 / CD-06 ANODISED ALUMINIUM IN FINISH: AMBER GOLD



CD-03 / CD-07 ANODISED ALUMINIUM IN FINISH: PORTLAND STONE



CD-04 / CD-08 ANODISED ALUMINIUM IN FINISH: SEA BREEZE (NOT VISIBLE IN VIEW)

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GL-01
CLEAR GLASS



RN-01
RENDERED PRE-CAST PANEL WITH PC-01 FINISH

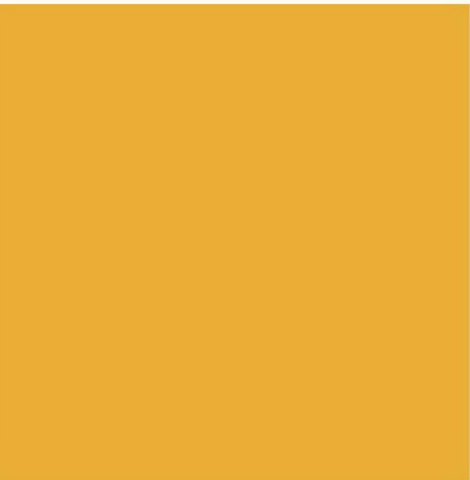
CD-01 - CD-04
PERFORATED ALUMINIUM SHEETS IN ANODISED FINISH WITH FEATURE CUSTOM PERFORATION PATTERN

CD-05 - CD-08
PERFORATED ALUMINIUM SHEETS IN ANODISED FINISH WITH MAXIMUM 20% OPEN AREA TO MEET FIRE ENGINEERING REQUIREMENTS

EX-FD
EXISTING FACADE

FM-01
PERFORATED ALUMINIUM SCREEN WITH MAXIMUM 20% OPEN AREA TO COMPLY WITH FIRE ENGINEERING REQUIREMENTS

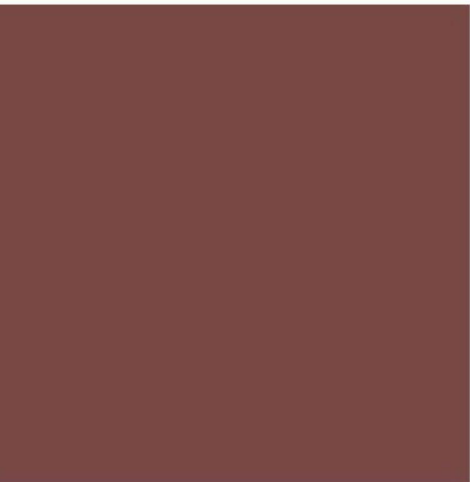
PAINT FINISHES BELOW



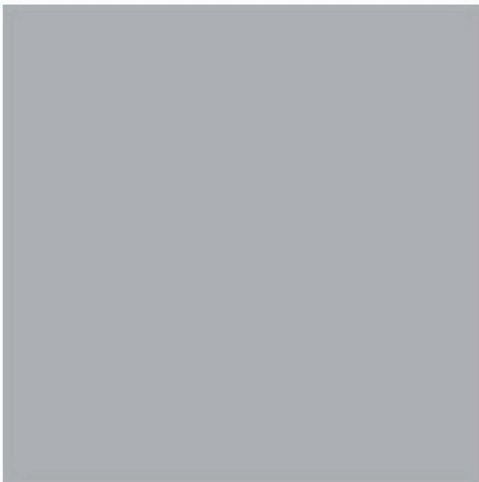
CD-05
DULUX - INTENSITY
SUNSHINE GLOSS
FEATURE COLOUR



CD-06
DULUX - STORM SATIN
FEATURE COLOUR



CD-07
DULUX - MANOR RED SATIN
FEATURE COLOUR



CD-08
DULUX - ZEUS SULVER GREY
MATT
FEATURE COLOUR



CD-09
DULUX - REEF GLOSS
FEATURE COLOUR



CD-10
DULUX - INTENSITY SUMMER
GLOSS
FEATURE COLOUR



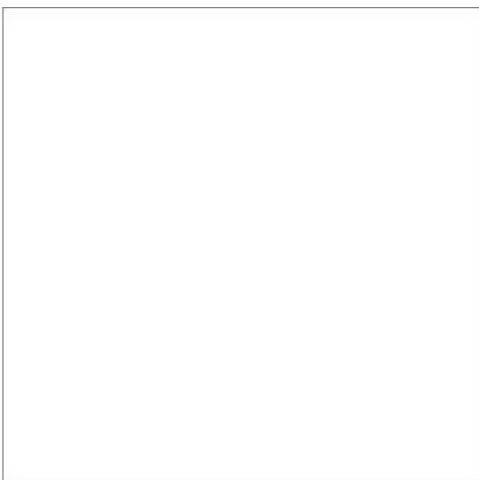
CD-11
DULUX - MISTLETOE
FEATURE COLOUR



CD-13
DULUX - C/C MONUMENT
SATIN
EXPRESSED STEEL TRIM

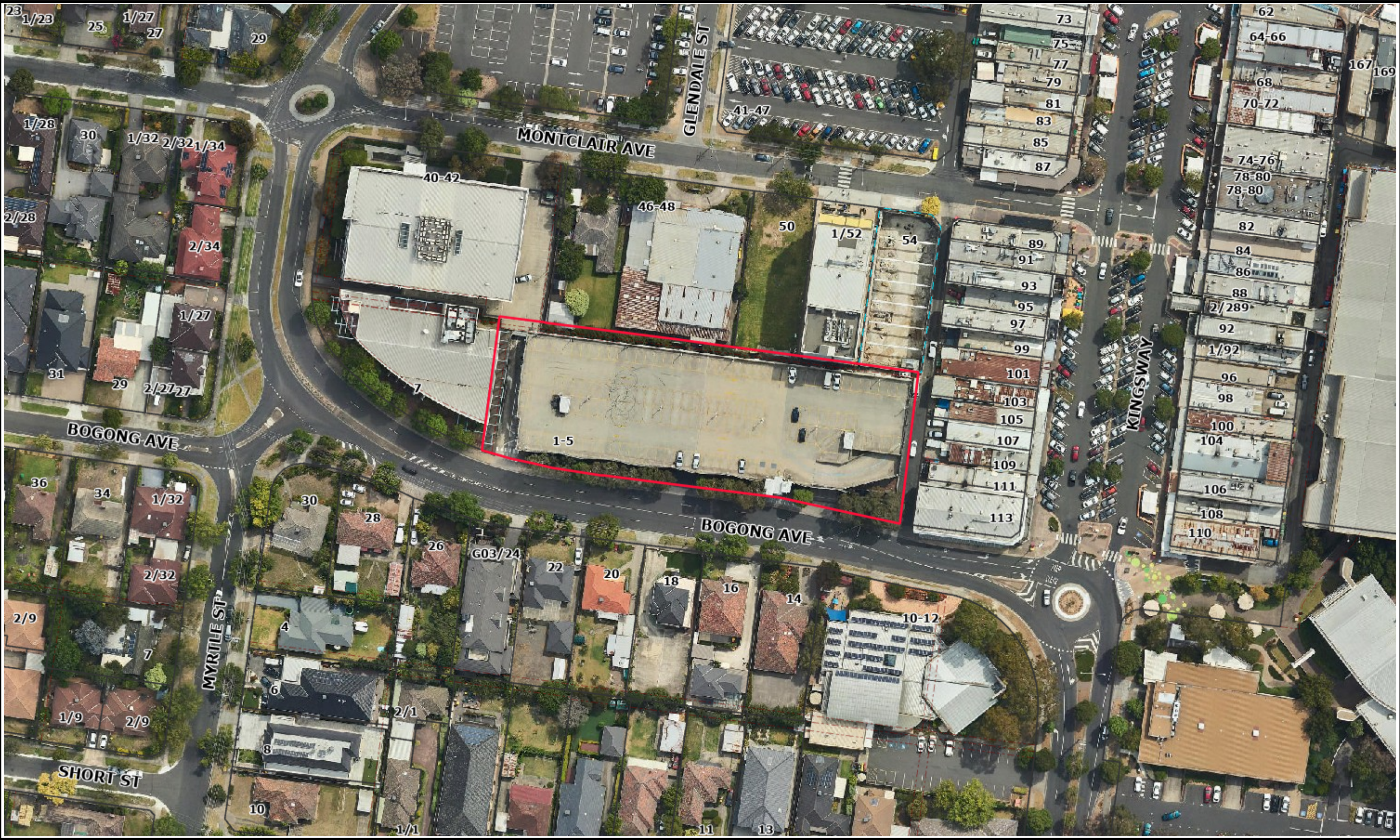


DULUX - BLACK SATIN (C/B
NIGHT SKY)
WINDOW FRAMES &
MULLIONS



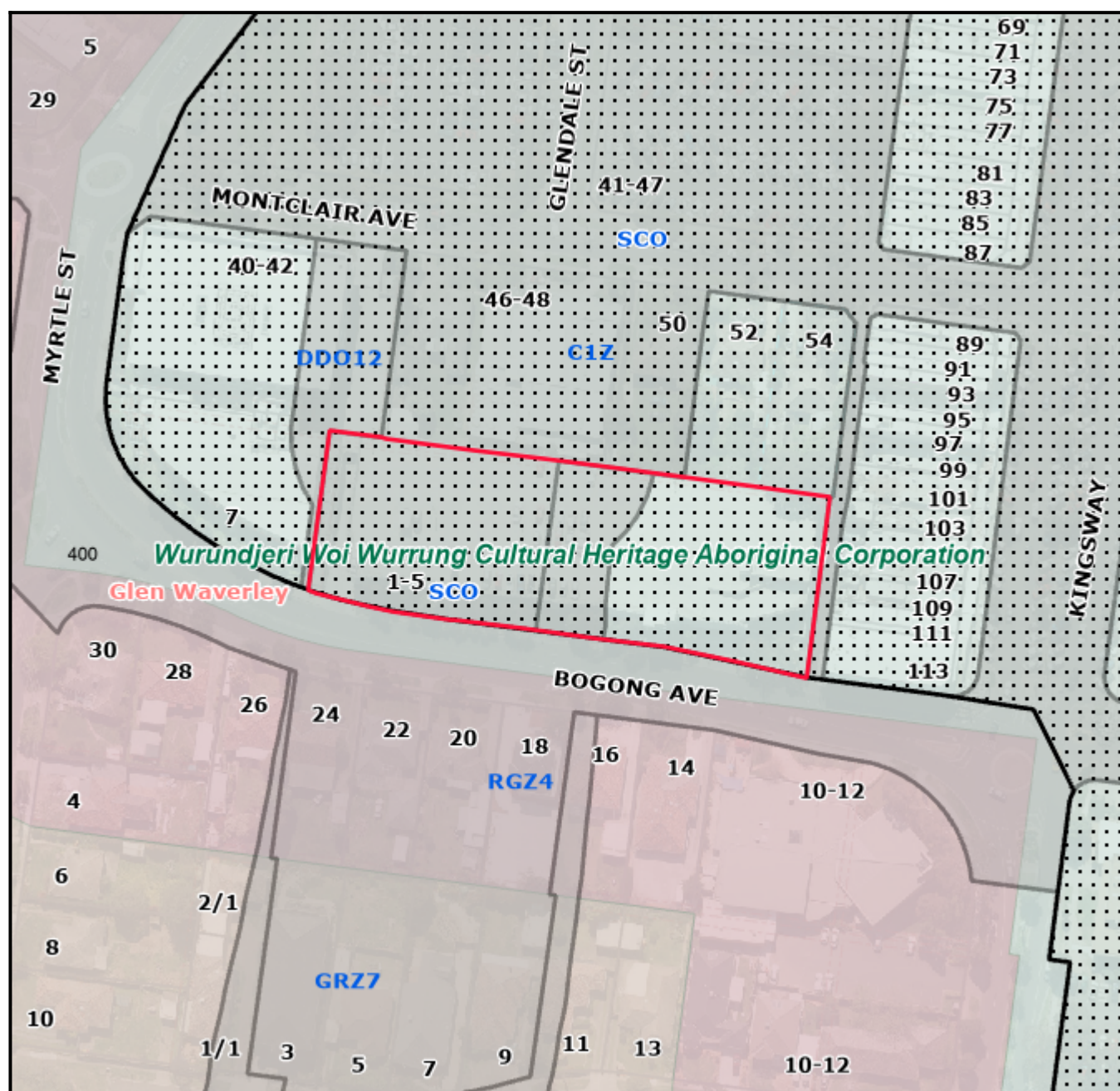
PC-01
DULUX - NATURAL WHITE
PAINT COLOUR

Attachment 2: 1-5 Bogong Avenue, Glen Waverley





Planning Overlays and Zones



Legend Planning Zones

C1Z	GRZ10	GRZ7	MUZ2	PCRZ	PUZ4	RGZ1	SUZ3
C2Z	GRZ2	GRZ8	NRZ1	PPRZ	PUZ5	RGZ3	SUZ4
CA	GRZ3	GRZ9	NRZ2	PUZ1	PUZ6	RGZ4	SUZ5
CDZ1	GRZ5	IN1Z	NRZ3	PUZ2	PUZ7	RGZ5	SUZ6
GRZ1	GRZ6	MUZ	NRZ4	PUZ3	RDZ1	SUZ2	UFZ

Planning Overlays

DDO	EAO	IFO	NCO	PO
DPO	HO	LSIO / SBO	PAO	VPO

1:1500

Address: 1-5 Bogong Avenue GLEN WAVERLEY VIC 3150

Area: 4919.00000 sqm

Base data is supplied under Licence from Land Victoria. This map is for general use only and may not be used as proof of ownership, dimensions or any other status. The information must be verified before taking any action which may be affected by a planning scheme requirement. This can be done by visiting the website: <http://services.land.vic.gov.au/landchannel/content/>

The City of Monash endeavours to keep the information current, and welcomes notification of omissions or inaccuracies.

