



CLAYTON ACTIVITY CENTRE

Draft Precinct Plan

May 2019

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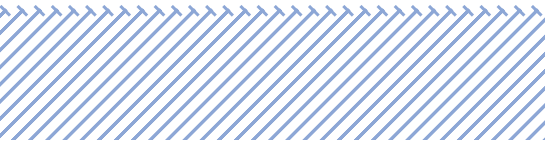
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01 INTRODUCTION

1.1 The purpose of a Precinct Plan

Located in the heart of the Monash National Employment and Innovation Cluster (MNEIC), Clayton is planned to experience a significant amount of growth and change in employment, land use, housing, built form and transport infrastructure.

As a suburb Clayton is forecast to grow from 21,219 people in 2018 to up to 29,000 people by 2036 (Forecast ID and UE, 2017). A large proportion of this growth will occur within the Activity Centre, which incorporates the Commercial Area of Clayton, the railway station, the Monash Medical Centre and adjoining residential areas. Refer to Figure 2

The Clayton Activity Centre Precinct Plan provides a clear framework outlining where and how this growth and change should occur.

Part of the plan is a transformative and long term Vision, which articulates how the Activity Centre should look, feel and function into the future. The Precinct Plan makes recommendations for future land uses in the centre including retail, commercial and residential uses. It also sets out preferred building heights, built form character and public realm improvements, along with transport improvements including walking, cycling, public transport and motor vehicle networks.

All stakeholders have been informed about the Vision for the Activity Centre, and the plan specifies how it will be achieved. The Precinct Plan provides clear guidance for the community, business owners, developers and planning applicants around the key infrastructure projects, level of change and type of development that can be expected across the Clayton Activity Centre.

1.2 How to use the Draft Precinct Plan

The Clayton Precinct Plan should be read in conjunction with the Clayton Activity Centre Background Report, which provides analysis of the key issues and opportunities, and supporting strategic and technical information.

The Precinct Plan comprises the following sections:

1. INTRODUCTION

Provides an overview of the project, this document and the key opportunities for the Activity Centre

2. THE CLAYTON ACTIVITY CENTRE

Provides a description of the Activity Centre, its context, infrastructure projects and development opportunities

3. VISION

Provides a 30 year vision for the growth and development of the Clayton Activity Centre

4. STRATEGIC RESPONSE

A strategic framework providing recommendations across the entire Activity Centre under four themes to achieve the Vision

5. THE PRECINCTS

Outlines specific outcomes and built form recommendations across three precincts

6. IMPLEMENTATION

An overview of the next steps required for implementation of the Precinct Plan

1.3 The project process

The Precinct Plan is being developed across four key stages. The Preparation of the Draft Precinct Plan marks a significant milestone in the project providing an opportunity for the broader community to give their feedback on the Vision and proposals outlined in this document.

Following this feedback, the Precinct Plan will be finalised and a planning scheme amendment prepared to implement some of its key findings.

The diagram below outlines the key stages of the project:

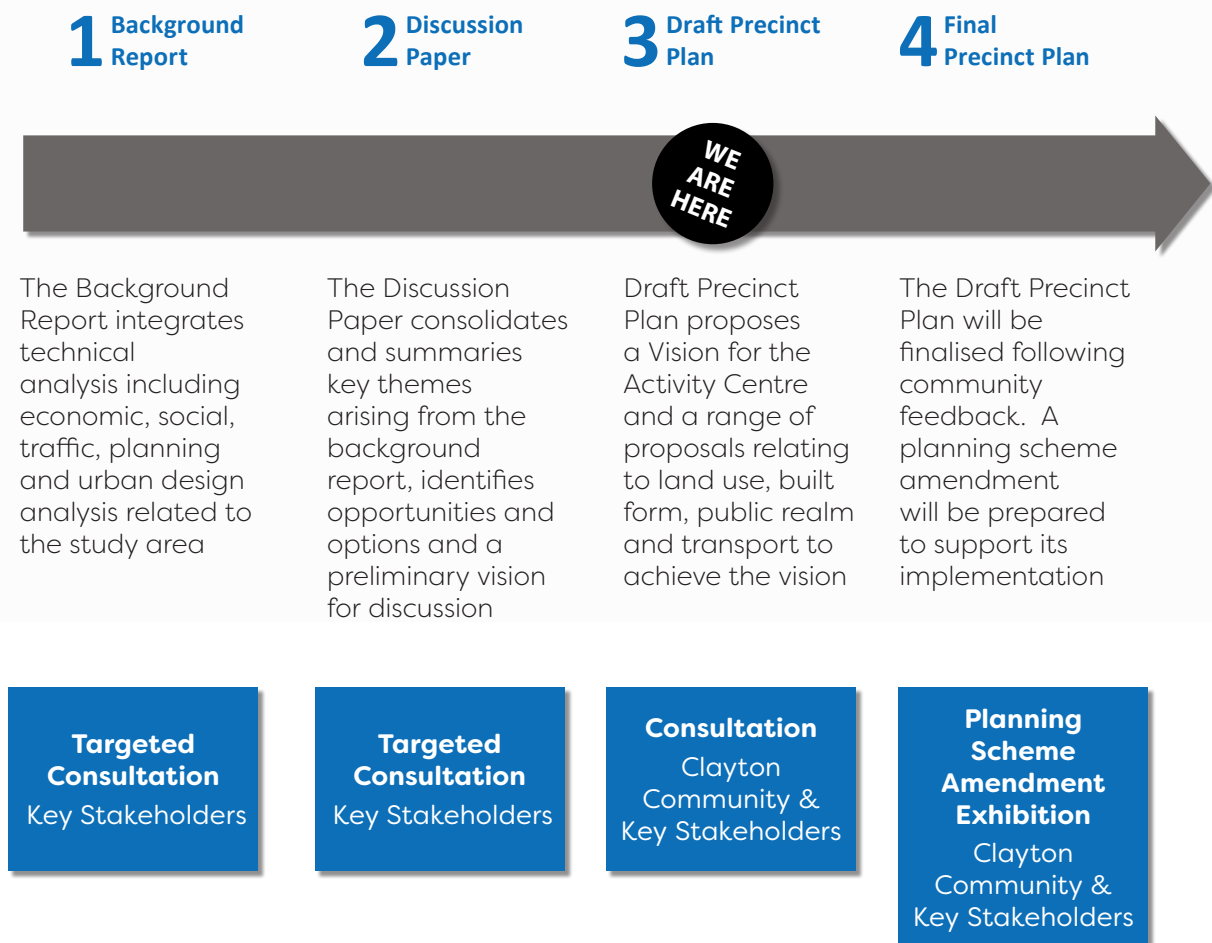


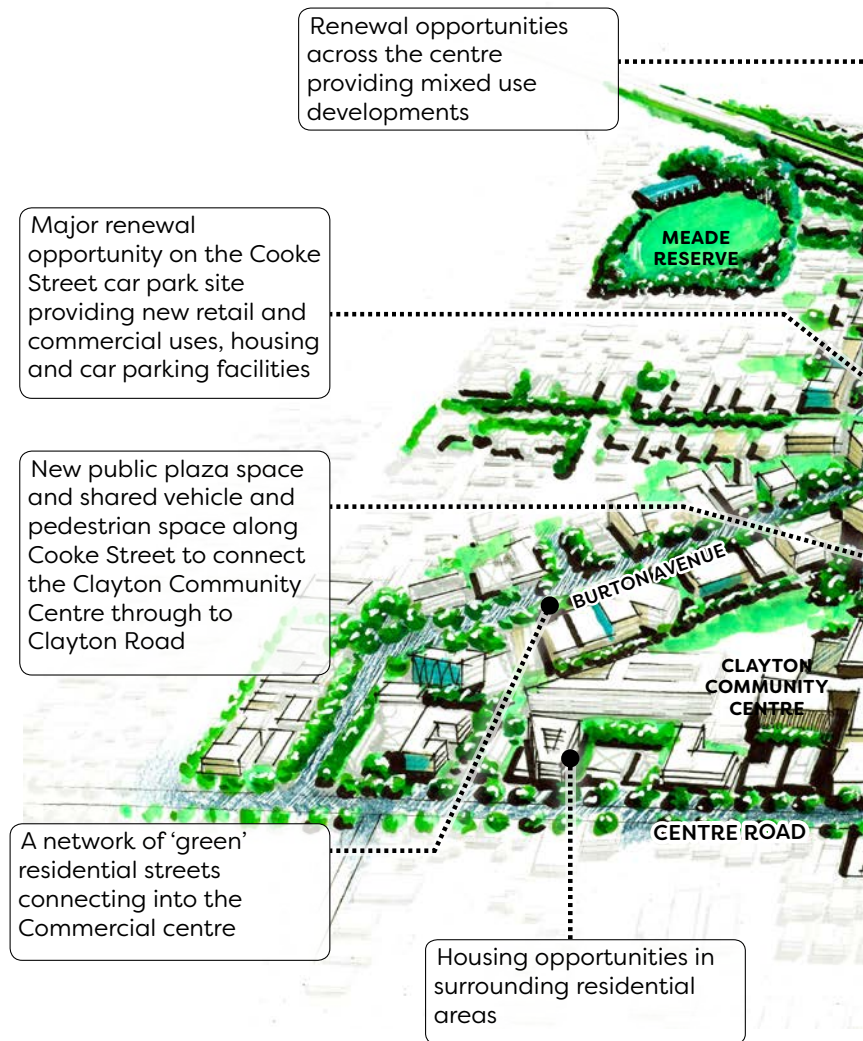


Image 1. Existing play areas provided as part of the level crossing removal



1.4 Key Opportunities

The Precinct Plan identifies a number of transformative opportunities to achieve the Vision for the Clayton Activity Centre outlined in Chapter 3. The key opportunities are outlined opposite:



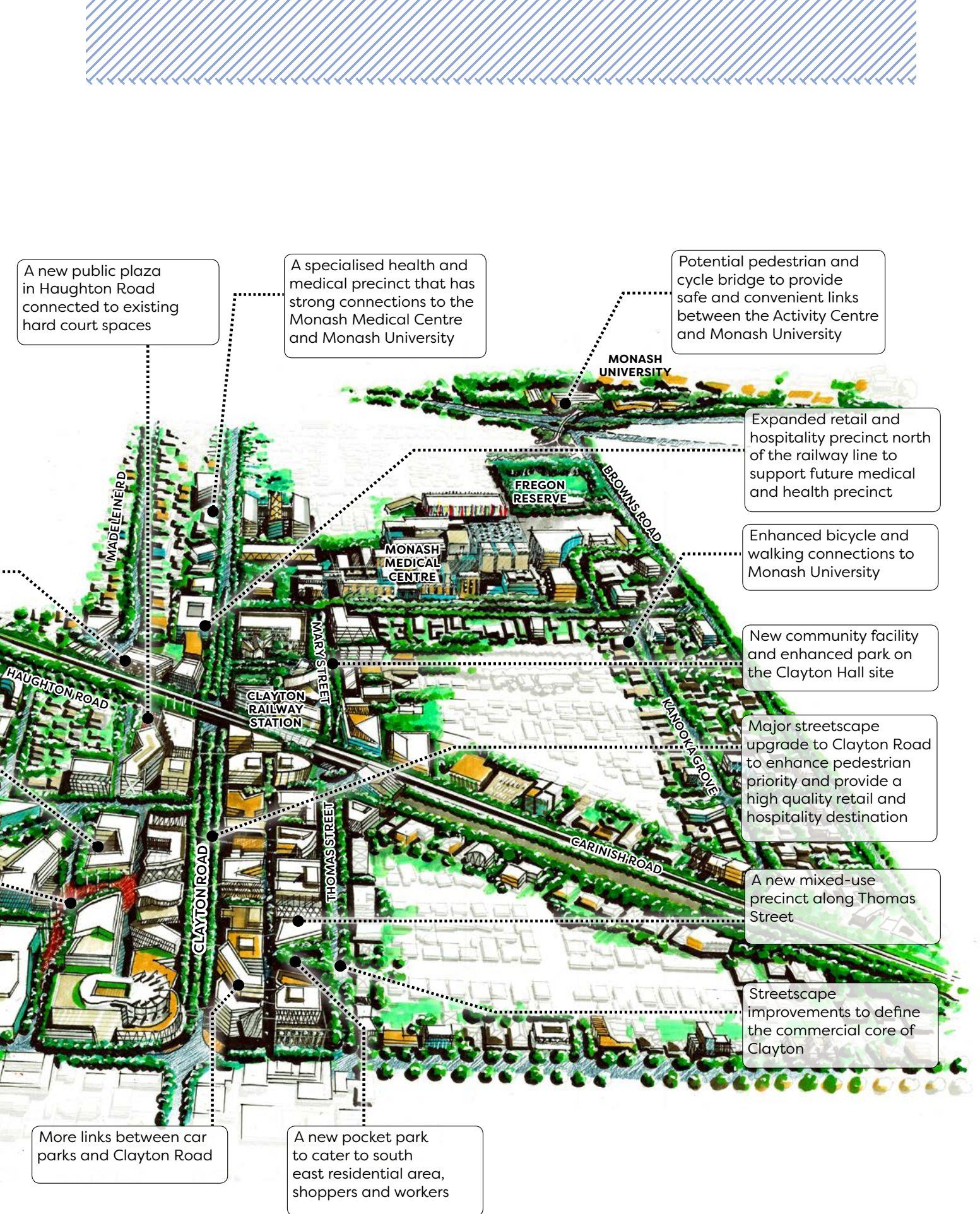


Figure 1. Artists impression of the future opportunities in the Clayton Activity Centre

1.5 The Clayton Activity Centre boundary

The Clayton Activity Centre incorporates the retail and commercial areas of Clayton, the railway station, community facilities, the Monash Medical Centre and adjoining residential areas. It extends up to North Road and Princes Highway highlighting its important relationship to the Monash University.

The delineation of the boundary has been guided by the State Government's Practice Note 58, which outlines a number of criteria for determining the boundary. These include incorporating key public uses, surrounding residential areas, and public open space that have a strong relationship to the Activity Centre.

The boundary serves an important role in providing a focus for the application of future projects, planning policies and controls. Some areas within the Activity Centre may experience limited change whilst other areas may experience greater transformation.



Image 2. Existing retail uses along Clayton Road

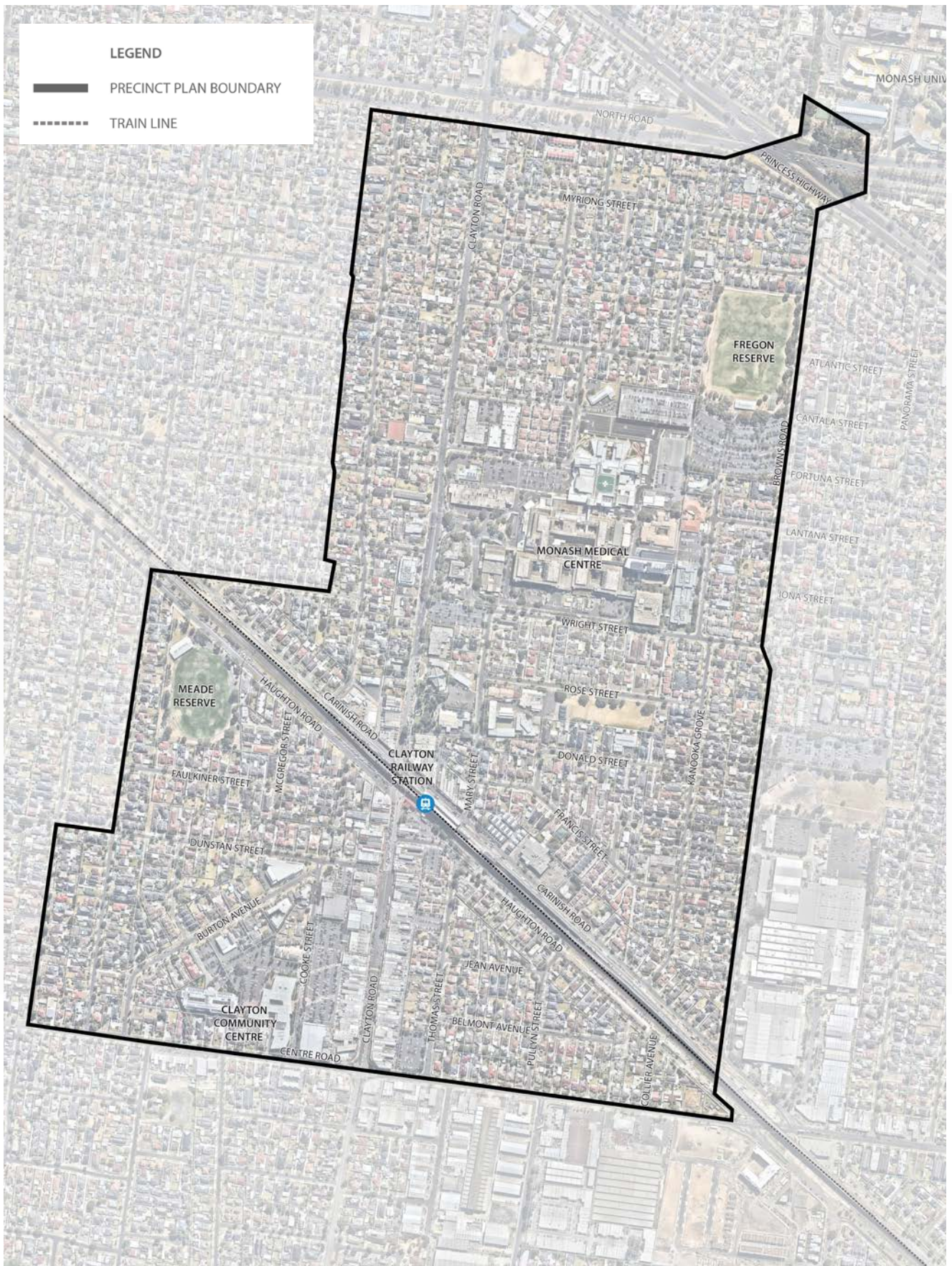
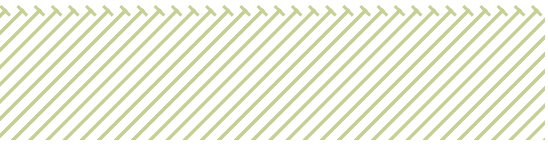


Figure 2. The Clayton Activity Centre Precinct Plan Boundary



02

THE CLAYTON

ACTIVITY CENTRE

2.1 Regional context

Clayton Activity Centre is located in the City of Monash, a municipality which covers an area of approximately 81 square kilometres and over 190,000 residents. Monash has a diverse population, with 45% of people not born in Australia and represented by over thirty different countries of origin.

The City of Monash is forecast to expect considerable population growth in the coming years, with conservative estimates increasing

from approximately 190,000 residents through to 215,000 by the year 2036 (Forecast ID, UE, 2017). Housing this number of residents has led to an increased density of living, with the current average residential density across the municipality over 24 dwellings per hectare.

In addition to residential figures, there are approximately 20,000 local businesses, 93,000 employed residents, and 130,000 local jobs. Importantly the largest industry is Health Care and Social Services, which is highly relevant to the Clayton Activity Centre Precinct Plan study area.

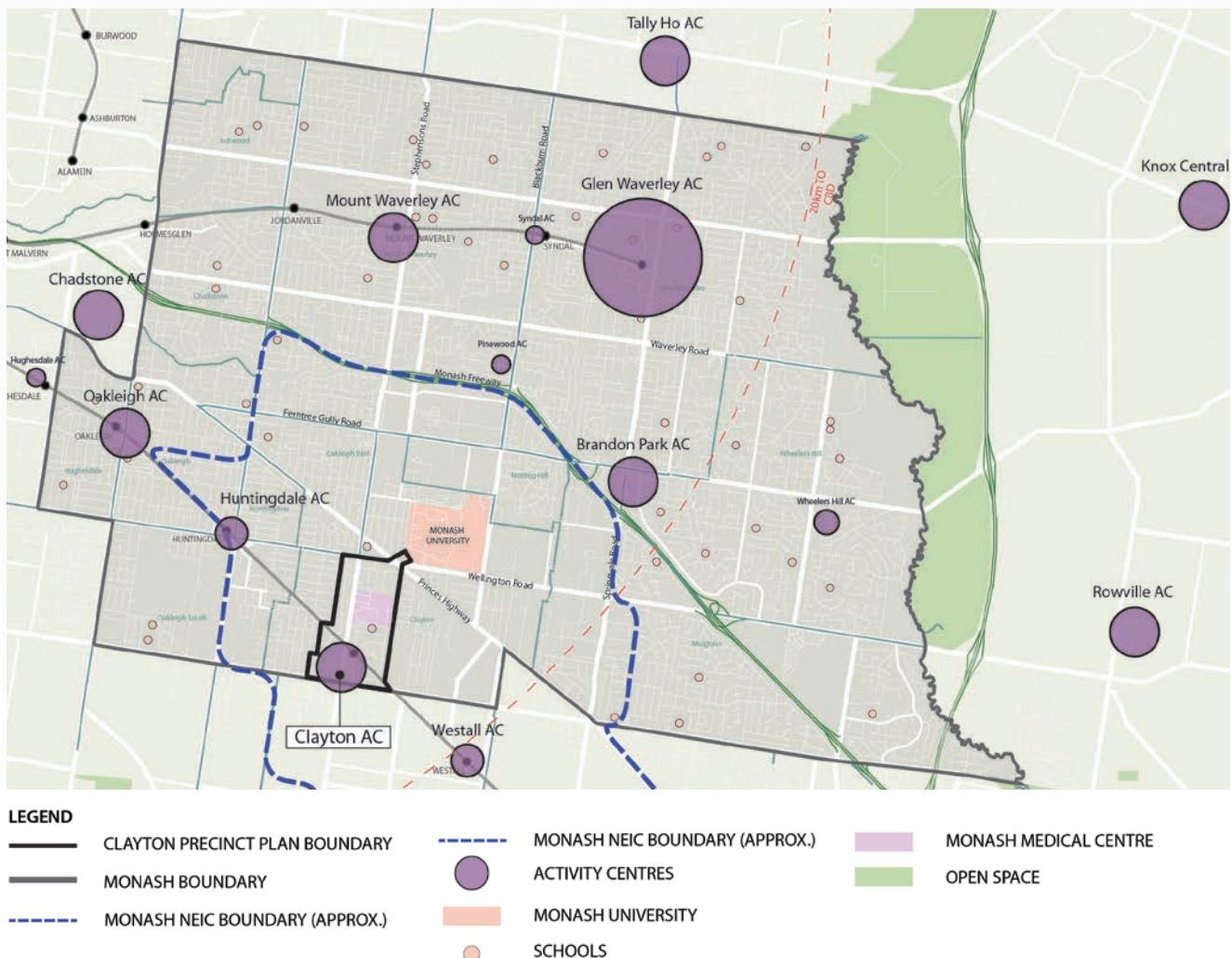
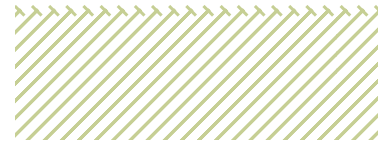


Figure 3. The Clayton Activity Centre Regional Context, opposite page

2.2 The Monash National Employment and innovation cluster



The Monash National Employment and Innovation Cluster (MNEIC) includes the Clayton Activity Centre and is host to Melbourne's largest concentration of jobs outside the CBD, supporting approximately 75,000 jobs and contributing over \$9.4 billion to Victoria's economy each year.

Notably, employment within the Monash Cluster is expected to double over the next 30 years, necessitating careful and coordinated public and private investment to ensure the challenges of increasing road congestion, changing business needs and evolving worker and community expectations are managed effectively.

Specifically, the MNEIC Framework Plan identifies that in the long term Clayton will be home to a diverse mix of uses that will support the local and regional community. Increased access to services and facilities, improved housing diversity and vastly improved public spaces will elevate the importance of this centre as a major destination.

Clayton has a further opportunity to contribute significantly to the employment targets for the MNEIC through strengthening its health and medical role. This will build on the Monash Medical Centre and the emerging medical uses along Clayton Road.

Clayton provides a desirable location to live within the MNEIC, due to its good public transport access and thriving strip-based retail offering. It has the potential to be further enhanced with a greater retail and hospitality mix, and diversity of businesses that help to create a destination for workers.

'The Monash National Employment and Innovation Cluster will transform and modernise over the next three decades as a connected and exciting place for employment, education, innovation, leading-edge technology and research. It will be positioned as a globally competitive value-creating economy.'

The Monash National Employment and Innovation Cluster Framework Plan

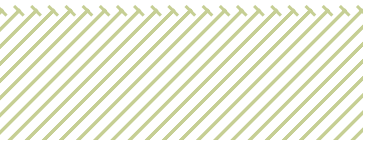


Figure 4. Monash National Employment and Innovation Cluster, (Source: DELWP 2017)

2.3 The local context

The Clayton Activity Centre is 164Ha in size, and generally focused along Clayton Road extending from Centre Road through to North Road. The Study Area includes key residential, employment and medical precincts located either side of Clayton Road. These areas play a key role in the function of the centre.

Retail uses along Clayton Road are located south, and immediately north of the Railway Line and generally characterised by narrow shop frontages offering a variety of tenancies. The shops provide a vibrant and varied interface with the street, including alfresco dining, street trading and other valuable pedestrian experiences, which benefit from the existing wide footpaths. Two supermarkets are located in the Cooke Street car park providing an anchor for adjoining shops.

Clayton Railway Station is located centrally in the Study Area and has been redeveloped as part of the Level Crossing Removals project. The station is elevated and integrated with a bus interchange, a variety of public spaces and a shared walking and cycling path. North-south movement across the railway line has been significantly improved since the removal of the crossing

The Monash Medical Centre is located north of shopping precinct and is a major destination and employment hub for Clayton. A number of medical practitioners are situated nearby along Clayton Road located amongst higher density housing.

Monash University is another major destination and is a driver for student housing in surrounding residential areas. It is located approximately 2.3km from the Clayton Railway Station and access by foot or by bike is difficult due to an impermeable local street network and a major road barrier formed by North road and the Princes Highway.

The surrounding residential areas have a strong relationship to the retail core of Clayton. The topography is relatively flat allowing for easy walking however a number of major roads, including Clayton Road create access barriers. Housing in the surrounding areas is predominantly post war detached dwellings mixed with more recent townhouse developments, older walk-up flats and some newer apartment buildings.



Image 3. The Clayton Community Centre



Image 4. Monash House Private Hospital

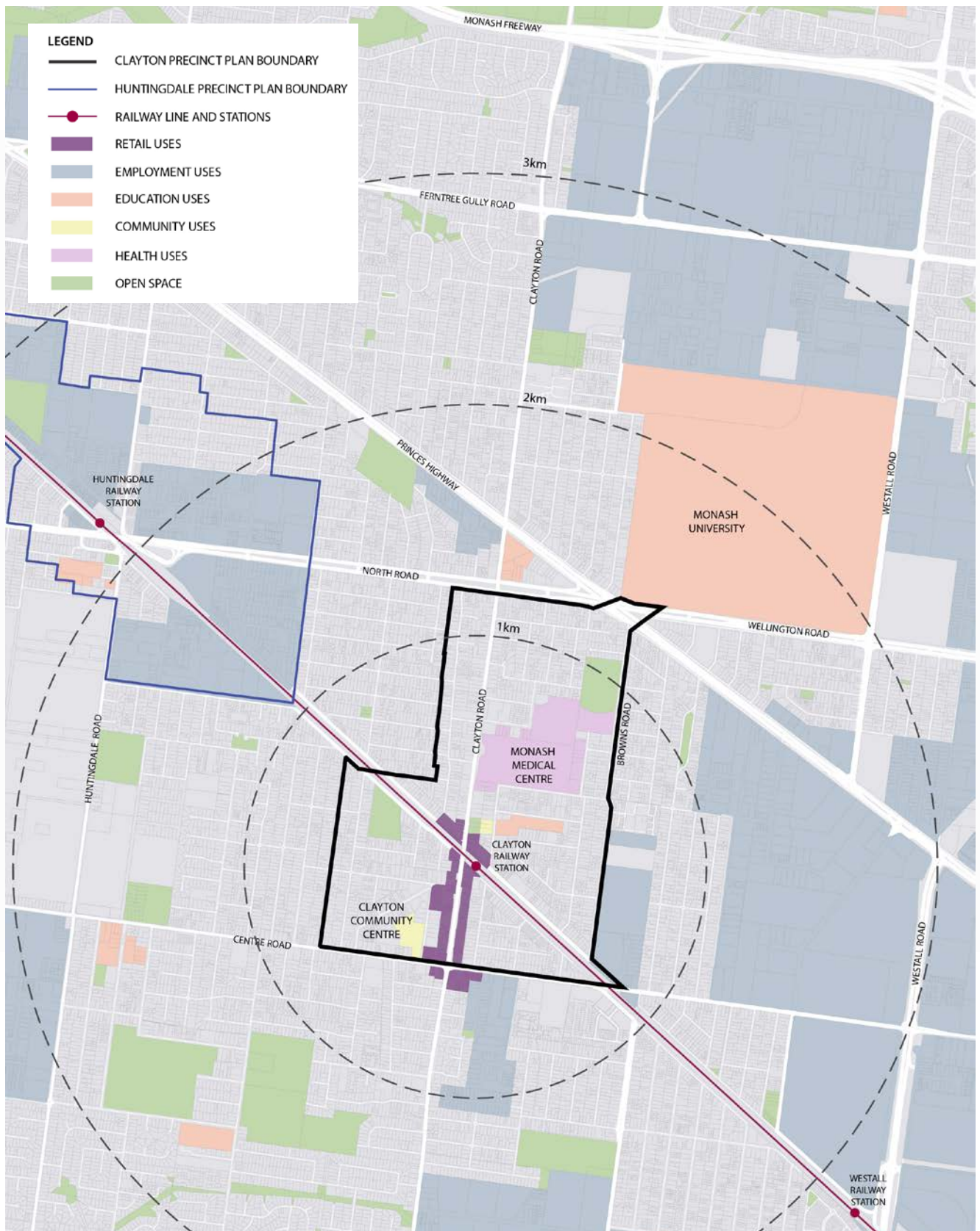


Figure 5. The Clayton Activity Centre Local Context

2.4 Community profile

Clayton's community profile is largely influenced by the growing student population at Monash University, with more than 50% of residents in Clayton identifying as a student.

Between 2001 and 2017 the population of the precinct has experienced significant population growth (2.7% per annum on average), which is higher than the average population growth experienced by Greater Melbourne (2.1% per annum).

The student population of Clayton also skews results in other fields, reducing the median weekly household income to \$1,070 (Melbourne average \$1,542) and lowering the median age to 25 (Melbourne median 36).

Clayton is a diverse community with a range of ethnicities across its population. Of its 21,219 residents, 69.9% were born overseas. The key birthplace countries include China comprising 25.6% of the total population, India with 9.9%, and Malaysia with 4.1% (Profile ID, 2016). This is reflected in the high proportion of Asian based food retailing and hospitality within the Activity Centre.



Image 5. Public art in a laneway of Clayton



Image 6. Existing Asian based food retailing and hospitality in Clayton Road

2.5 Major projects

A number of critical major projects and infrastructure items have shaped, or will shape the future of Clayton, and how it connects to the immediate, and surrounding network.

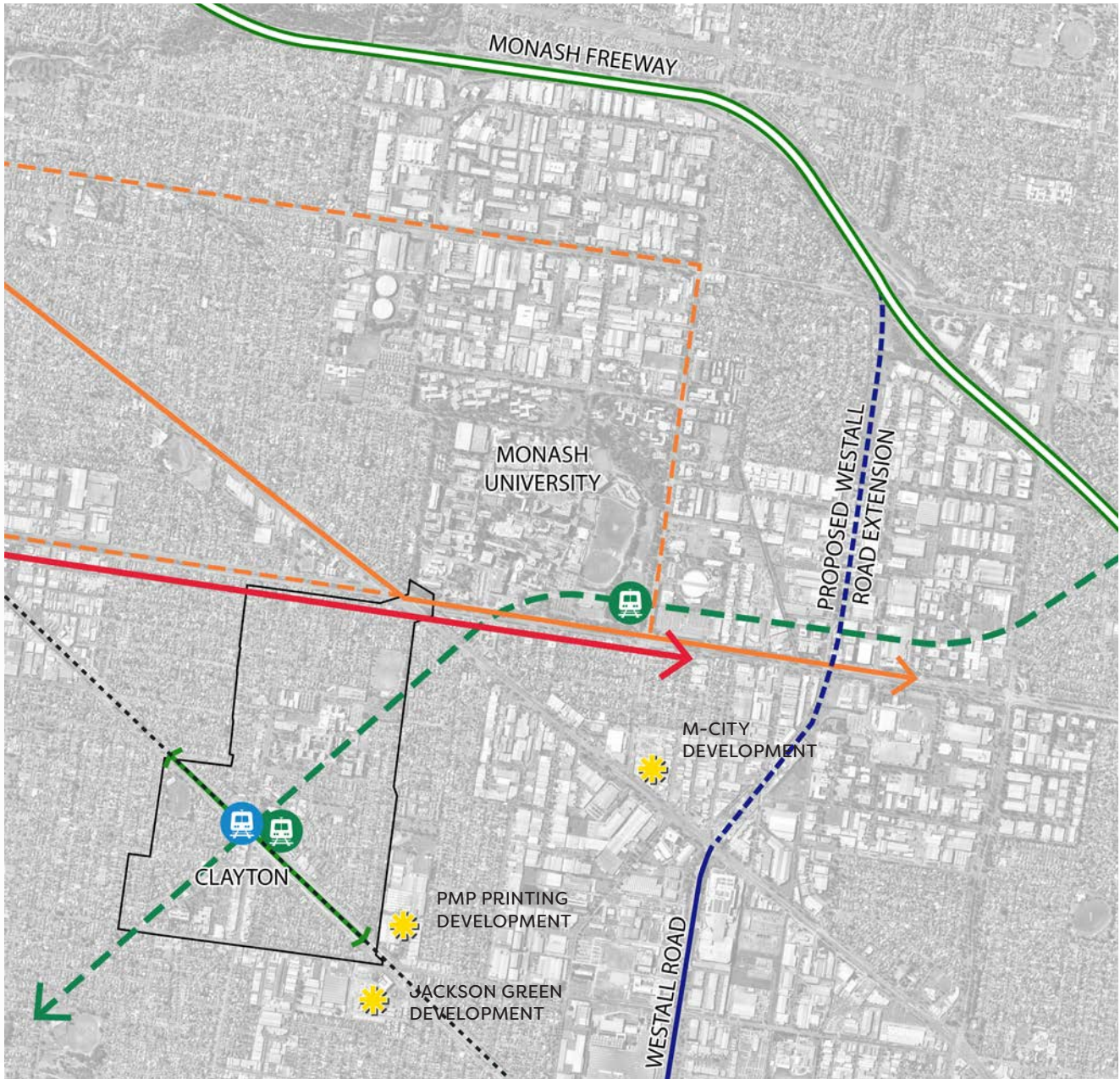
These projects include private developments, along with state infrastructure projects. They include:

- **Caulfield to Dandenong Level Crossing Removal** - New station, elevated rail, shared path, landscape and public realm upgrades in Clayton (completed)
- **Suburban Rail Loop** - The proposal will create an underground passenger railway route traversing through middle and outer suburbs of Melbourne connecting to many of the existing radial above-ground railway lines. Clayton is included in the first stage of the project and is identified as a potential interchange superhub, linking the Pakenham and Cranbourne lines (work on the first sections due to commence in 2022)
- **Rowville Heavy Rail** - A rail spur that extends from Huntingdale to Monash University and through to Rowville (currently in planning phase)
- **Caulfield to Rowville Light Rail** - Connects the Caulfield Monash University Campus to Clayton Campus and through to Rowville. A number of route options are being considered (currently in planning phase)
- **Westall Road Extension** - A proposed extension of Westall Road from the Princes Highway up to the Monash Freeway. This is expected to reduce traffic volumes along Clayton Road if implemented. (planned, awaiting government funding)
- **M-City Mixed Use Development** - Comprises 642 apartments, a hotel, KMart, Woolworths and Village Cinema (under construction)
- **PMP Printing Strategic Site** - A 10 hectare redevelopment site adjacent to the Clayton Activity Centre which will provide a mix of commercial development, housing and open space (currently in planning phase)
- **Jackson Green** - An Infill development providing up to 350 dwellings (under construction)

Each of these projects have had, or will have future implications for Clayton Activity Centre and should be considered in future planning.

Specifically, the Suburban Rail Loop would represent a major infrastructure 'game-changer' for the Clayton Activity Centre. The improved public transport accessibility will present a range of economic benefits to the centre, particularly due to improved access to jobs in Clayton. Importantly, access to the Monash Medical Centre and allied health facilities will be drastically improved for patients and staff as a direct result of the Suburban Rail Loop, allowing better health care access and opportunities.

Subsequently, the Clayton Activity Centre would be likely to experience greater demand for residential and commercial development. There is likely to be associated growth in land value once funding has been committed and after construction is completed. This increase in land value should be captured in a way that enables local government services to operate in an increased capacity and meet the demand of the future residents and workforce planned to utilise Clayton.



LEGEND

- | | | | | | |
|---|---|---|--|---|---|
|  | PRECINCT PLAN AREA |  | PROPOSED SUBURBAN RAIL LOOP ALIGNMENT |  | ELEVATED RAIL PUBLIC REALM IMPROVEMENTS |
|  | EXISTING TRAIN STATION |  | EXISTING WESTALL ROAD |  | MONASH M1 FREEWAY |
|  | PROPOSED SUBURBAN RAIL LOOP TRAIN STATION |  | POTENTIAL WESTALL ROAD EXTENSION |  | KEY REDEVELOPMENT SITES |
|  | TRAIN LINE |  | POTENTIAL ROWVILLE HEAVY RAIL ALIGNMENT | | |
| | |  | POTENTIAL LIGHT RAIL ROUTE ALIGNMENT | | |
| | |  | POTENTIAL LIGHT RAIL ROUTE ALTERNATIVE ALIGNMENT | | |

Figure 6. Major projects and infrastructure relevant to the Clayton Activity Centre

2.6 Key site opportunities

The Clayton Activity Centre includes a number of large, underutilised sites with potential to deliver high quality developments that diversify the mix of uses in the centre.

Many of these sites are identified as Council Owned car parks, the biggest site being the Cooke Street car park at 6,717sqm. This presents a significant opportunity because of its location between the retail strip and the Clayton Community Centre.

As part of the level crossing removal of Clayton Station, a redevelopment opportunity exists immediately south of the train station. This site is strategically located with immediate connections to public transport, and has limited interface issues

with adjacent land holders. This site could facilitate a mix of residential, retail and commercial development.

There are also a large number of redevelopment opportunities on privately owned commercial properties. Sites at the corner of Clayton Road and Centre Road are also strategic sites, they provide an opportunity for taller built form to create a strong entrance into the Clayton Road shopping strip.

Strategic sites within the study area are shown in Figure 8, and outlined further in Table 1. These sites are a mix between public and private ownership, and benefit from their strategic positions proximate to key services, retail, commercial, public transport and community services.

Site	Address	Current Use	Area (m ²)
a	171 CARINISH ROAD	Clayton RSL	4,736
b	1389-1391 CENTRE ROAD	Coles Supermarket	2,875
c	1399-1401 CENTRE ROAD	Car Park	3,212
d	1-3/212-220 CLAYTON ROAD	Monash Specialist Centre	4,571
e	264-268 CLAYTON ROAD	Clayton Hall and Gardens	5,602
f	359-365 CLAYTON ROAD	Asian Supermarket	1,399
g	367-369 CLAYTON ROAD	Fresh Fruit Market	1,731
h	409 CLAYTON ROAD	Caltex Service Station	2,100
i	2 COOKE STREET	Coles Supermarket	2,925
j	20-22 COOKE STREET	Car Park	1,890
k	6-18 COOKE STREET	Car Park	6,717
l	9-15 COOKE STREET	Clayton Community Centre	15,528
m	392-394 HAUGHTON ROAD	Car Park	1,378
n	13-15 MARY STREET	St Peter's Car Park	3,284
o	9 THOMAS STREET	Car Park	1,402
p	274-284 CLAYTON ROAD	Vacant Land	1,200 (approx)
q	MONASH MEDICAL CENTRE	Monash Medical Centre	157,500

Table 1. Key Renewal Sites

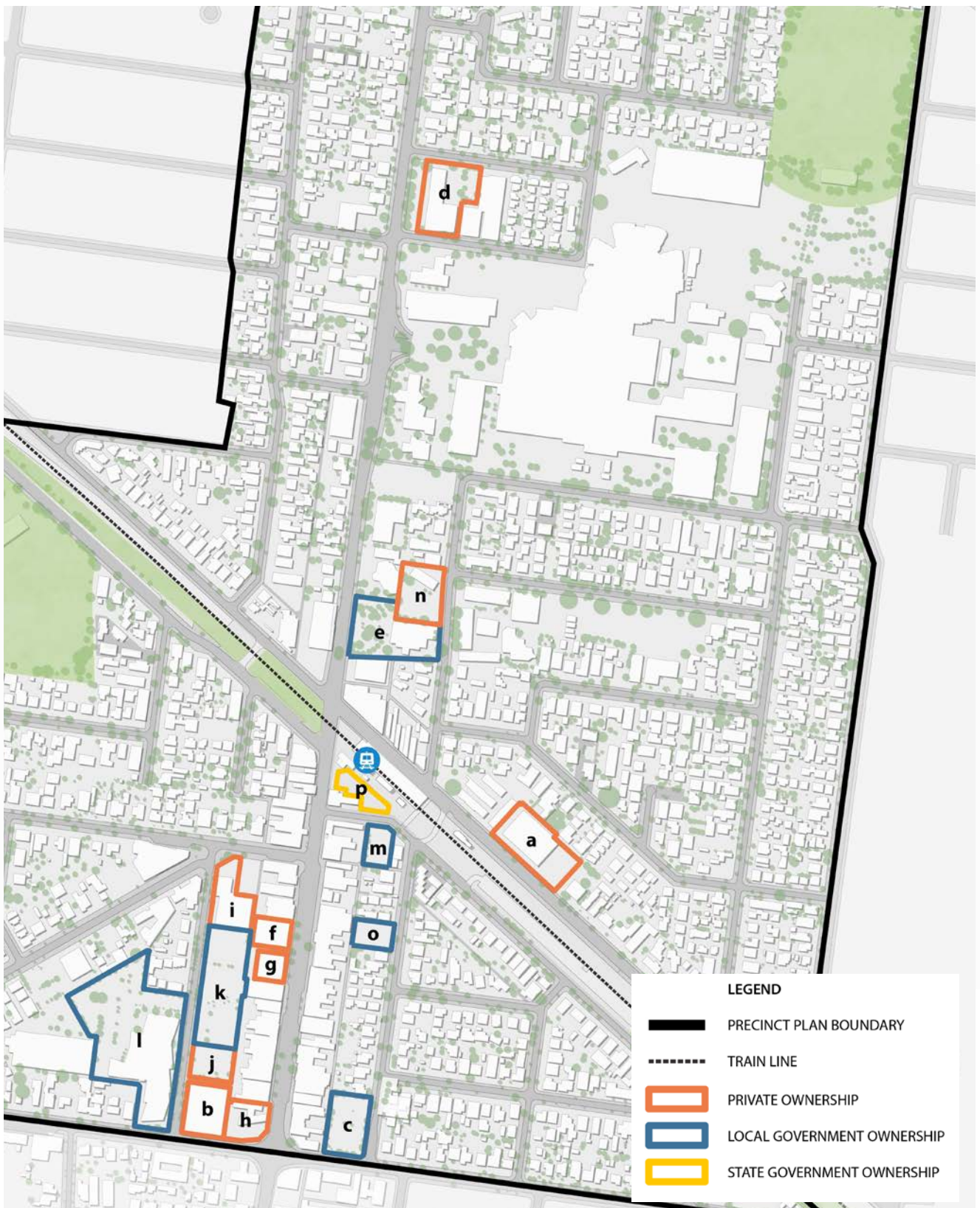
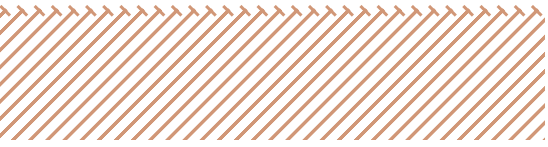


Figure 7. Key renewal sites within the Clayton Activity centre



03 VISION

3.1 Clayton Activity Centre Vision

The Vision provides a statement of the preferred future for the Clayton Activity Centre up to 2036.

Future initiatives and projects that follow throughout the document aim to achieve this vision.

Centrally located within the Monash NEIC, Clayton is a vibrant and diverse centre offering a range of learning, employment and lifestyle opportunities. It is a convenient centre where people can access major health, education and transport destinations along people focused streets.

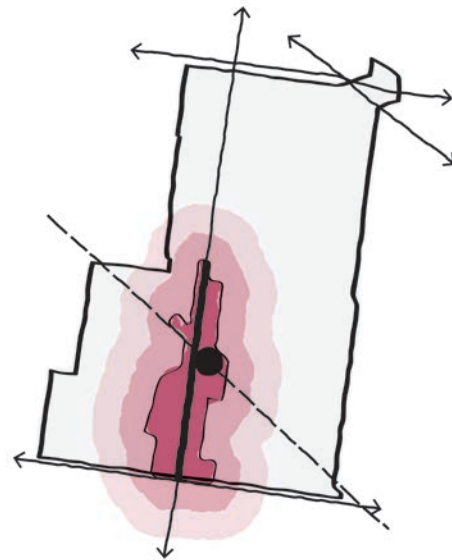
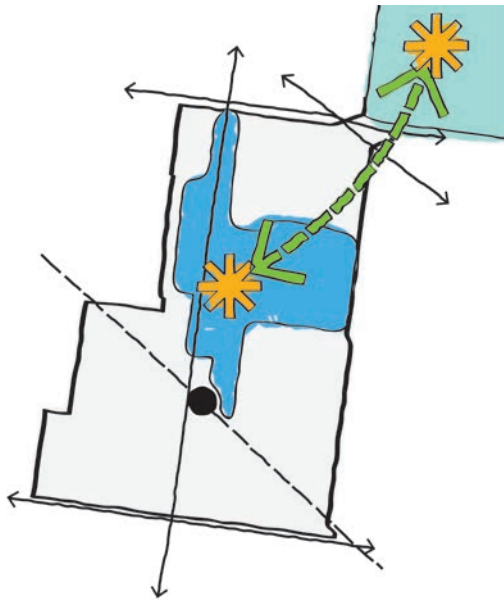


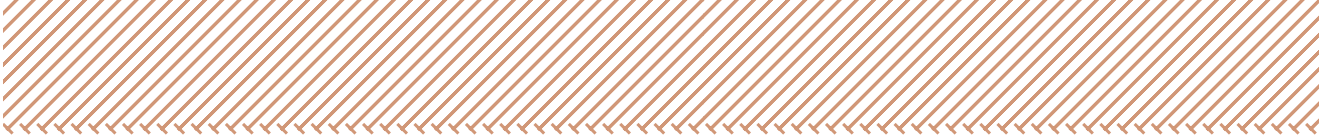
A Major Health, Employment and Innovation Hub

Clayton is the centre for innovative thinking and world leading business and commercial ventures and activities. The extensive health precinct is anchored by the Monash Medical Centre and Children's Hospital, with strong connections to the Monash University and Victorian Heart Hospital, providing opportunities to share ideas and knowledge.

A Thriving Retail & Commercial Heart

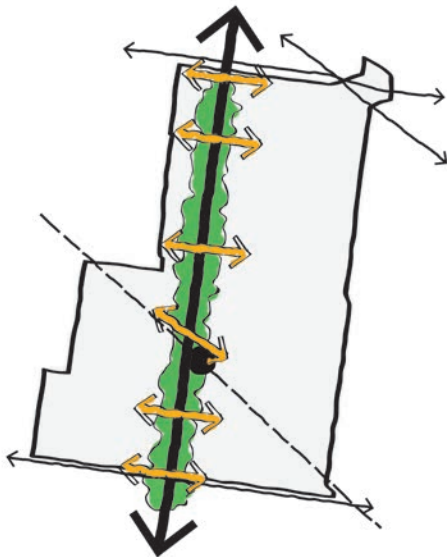
At its heart Clayton offers a local shopping experience, with a diversity of small scale shops and larger retail stores. Local traders are the heart beat of the Centre, its a place where the store owners know your name.





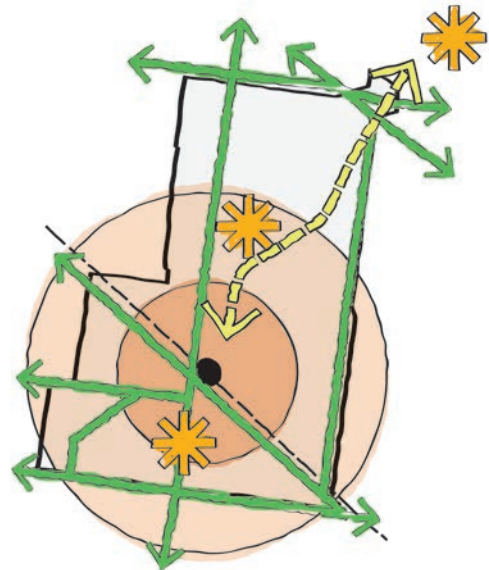
The Clayton Road Spine

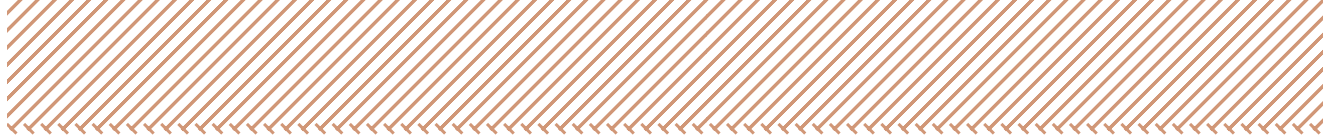
Clayton Road is the spine of the Centre, connecting people to places. Its a vibrant destination, with a comfortable mix of pedestrians, cyclists and vehicles, a central place that connects people, cultures and business.



Regional Transport Hub

Clayton is well connected centre, a place that is easy for everyone to move around, connect to Melbourne and the wider regions. Commuters seamlessly transit between bus and train services and integrated bike and walking infrastructure encourages active transport.



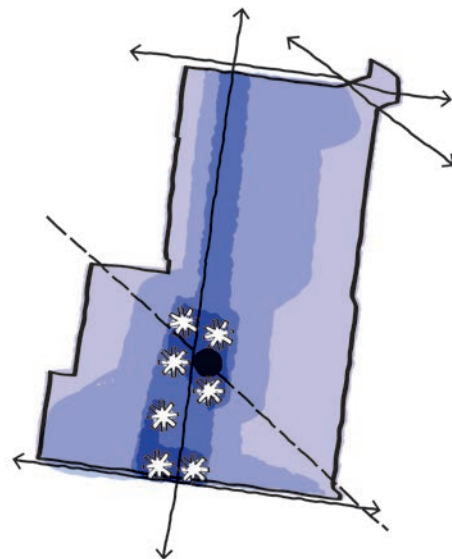
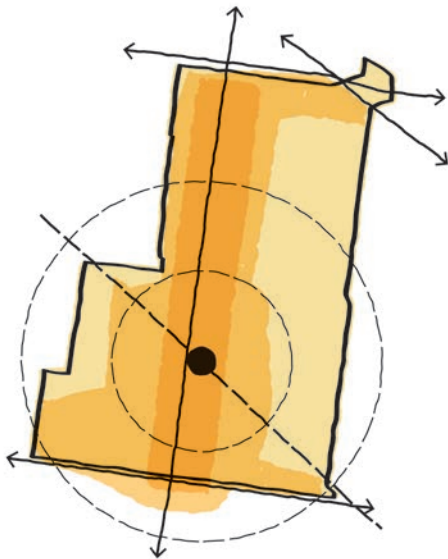


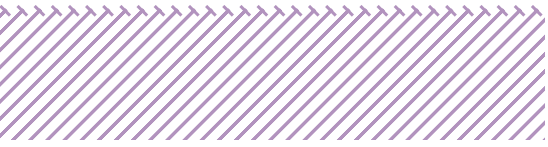
Housing for All

Clayton will provide housing options to suit the various needs of the community. Housing choices above shops or offices, or in residential streets where people can walk to access their daily needs.

A New Urban Character

New development throughout Clayton will set the tone for high quality architecture and urban design. New built form will be contemporary and site responsive. Landmark buildings and quality public spaces will define key street corners and community places.





04 THE STRATEGIC RESPONSE

The Strategic Response for the Clayton Activity Centre outlines a range of Objectives, Strategies and Actions to plant for the growth and development of the Activity Centre in a holistic way.

It is arranged under the following four themes:

- 4.1 Activities and Land Use
- 4.2 Built Form
- 4.3 Public Realm
- 4.4 Movement and Transport

4.1 Activities and land use

The Vision seeks to provide for employment opportunities and a thriving retail and hospitality heart.

This theme provides centre-wide initiatives for how this will be achieved through land use and investment. Figure 8 reflects the proposed future land uses across the Clayton Activity Centre.

A. Retail & Hospitality

Clayton has a strong retail offering with a significant supply of groceries, restaurants, take away food and discount shops. Larger format retail uses including the two Coles Supermarkets, the Hong Kong Supermarket and the Clayton Fresh Fruit Market, all of which are located on the western side of Clayton Road. This results in a greater amount of retail and pedestrian activity along the western side of Clayton Road.

Economic analysis has forecast demand for an additional 8,600m² - 13,000m² of retail floorspace by 2036. Because the majority of suitably zoned land is built on, additional under-utilised lands may need to be rezoned to meet the forecast demand.

The Precinct Plan proposes to incrementally extend retail uses across the Centre south of the rail line, to accommodate some of this additional growth. The sites for potential rezonings include large at-grade car parks and a number of sites in Cooke Street and Dunstan Street, which will allow for retail uses on both sides of the street.

The Plan also proposes extending the retail uses along Clayton Road north of the rail line. This will allow for additional retail and hospitality uses to support the significant increase in employment proposed as part of the Health precinct.

The economic analysis forecasts demand for an additional supermarket in the Centre. A future supermarket could be supported south of the railway line given the anticipated growth and change in the area. Alternatively, a location north of the railway line would be beneficial in catering to the additional workers and population in this area.

The economic analysis also identifies an opportunity for a Discount Department Store (DDS) in the Centre. Because of the large floor area required, a suitable location could be within the Cooke Street car park if it is redeveloped.

A. Retail & Hospitality continued

Objective

To support future retail and hospitality growth within the Clayton Activity Centre

Strategies

- Provide for the expansion of retail land uses across the Activity Centre to support the future residential and working population
- Provide additional retail and hospitality uses north of the railway line to support the future health and medical precinct
- Encourage the provision of an additional supermarket within the Activity Centre
- Enhance Clayton Road as a place for people to strengthen it as a retail and hospitality destination
- Provide additional cafes and restaurants across the Activity Centre focused along Clayton Road and adjoining retail areas

Actions

- A.1 Facilitate the rezoning of land south of the railway line along Cooke Street and Dunstan Street, Haughton Road and Thomas Street to support retail and mixed use development
- A.2 Facilitate the rezoning of land along Clayton Road north of the railway line and south of Colonel Street to support retail, health, medical and residential uses
- A.3 Undertake streetscape improvements along Clayton Road to create a high quality pedestrian environment and suitable spaces for outdoor dining
- A.4 Encourage the provision of an additional supermarket within the Activity Centre
- A.5 Encourage the provision of a Discount Department Store (DDS) within the Activity Centre

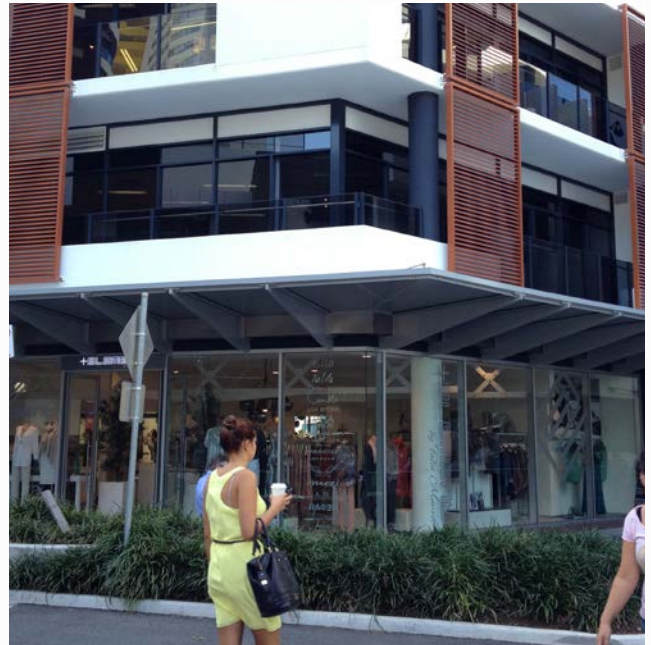


Image 7. Future retail diversity in Clayton



Image 8. Future hospitality venues can complement the growing student population, health worker group, and other residents.

B. Health and Medical

Health is the dominant employment sector for the Clayton Activity Centre, currently providing approximately 6,538 jobs. The health sector is projected to grow significantly with an estimated additional 3,000 - 5,000 jobs to be provided within the Activity Centre by 2036. This equates to an approximate floorspace increase of between 95,000 and 158,000sqm. The Precinct Plan aims to support this growth and reinforce Clayton's health focus within the Monash National Employment and Innovation Cluster.

Many of these additional jobs could be provided within Monash Medical Centre, which could expand and grow within its current boundaries. Opportunities also exist to make the Medical Centre more accessible for employees, visitors and students by improving the walking and cycling network both within the Medical Centre and surrounding streets.

In addition to the growth of the Medical Centre, a specialised medical precinct is proposed to ensure competing land uses do not diminish the opportunity to strengthen Clayton's Health focus. This precinct focuses on Clayton Road north of the railway line and extends into a number of surrounding streets including Madeleine Road, Mary Street, Kanooka Grove and Browns Road. Changes to land zoning will be required to support future medical uses in these locations.

***'The health sector is projected to grow significantly with an estimated additional 3,000 - 5,000 jobs to be provided within the Activity Centre by 2036.'* (UE, 2019)**

Objective

To strengthen Clayton as the focus for health and medical uses within the Monash National Employment and Innovation Cluster

Strategies

- *Support the development of a specialised medical precinct within the Clayton Activity Centre*
- *Support the future growth and improvement of the Monash Medical Centre*
- *Strengthen the connection between the railway station, Monash Medical Centre and the Monash University*

Actions

- B.1 Facilitate the rezoning of land within the Health and Medical Precinct to support these uses whilst allowing for mixed use development including residential
- B.2 Undertake streetscape improvements to Clayton Road to enhance it as a high quality destination for future medical and health businesses
- B.3 Encourage the Monash Medical Centre to undertake Master Planning of the site to create a clear network of pedestrian, cycle and vehicle within and into the site, improve wayfinding, provide a higher level of amenity and safety for workers and visitors and provide a better address to surrounding streets
- B.4 Improve walking and cycling connections between the railway station, Monash Medical centre and the Monash University. Refer to Chapter 5 for details



Image 9. Monash Medical Centre



Image 10. Better public realm connections to health precincts will contribute to the amenity for the additional health workers in the future

C. Commercial and office

Commercial and office uses across the Clayton Activity Centre are currently limited and focused on upper levels of buildings along Clayton Road. These uses are important to Activity Centres as they provide local employment opportunities, provide additional customers for the hospitality industry and also provide services to residents.

Projections for the Activity Centre indicate that there will be commercial employment growth in the order of 500 jobs and 9,600 m² of floorspace by 2036. It is expected that some of this floor space will be accommodated in the upper levels of buildings when they are redeveloped across the Centre.

It would be advantageous for the future commercial and office space to be located within the existing Commercial 1 Zoned areas to provide greater support to retail and hospitality uses and allow properties north of the railway line to provide a stronger health focus.

The Precinct Plan proposes a new area for mixed use commercial, office and residential uses along the western side of Thomas Street. This area is well located nearby the railway station and abuts the existing commercial properties along Clayton Road. It also has good access via a rear laneway and Thomas Street.

Objective

To provide a diverse range of business services and employment opportunities within the Activity Centre

Strategies

- *Strengthen commercial and office uses within the core of the Activity Centre above existing and future shops*
- *Encourage major office tenants to locate in the Activity Centre*
- *Support the development of office uses at ground and podium levels in the Thomas Street Precinct*

Actions

- C.1 Facilitate the rezoning of properties along the west side of Thomas Street to support office and residential uses
- C.2 Develop and implement strategies for actively seeking government agencies, head offices and large businesses to locate within the commercial area
- C.3 Develop and implement policies to encourage office and commercial uses above shops and other ground floor uses

'There will be commercial employment growth in the order of 500 jobs and 9,600 m² of floorspace by 2036.' (UE, 2019)



Figure 10. An artist's impression of Thomas Street future development outcomes

D. Community and Social Infrastructure

Clayton is currently well served by the Clayton Community Centre located in the heart of the Activity Centre. This facility is co-located with an Aquatics and Health Club providing a major destination for the residents and visitors.

By 2036, additional services and facilities will be required to meet the needs of the Clayton Activity Centre resident population, workers and students. This is based on industry benchmarks for community services and facilities applied to the projected future population numbers.

Best practice approaches to building community infrastructure show that this anticipated additional floor space would be most suitably be developed through one or more one of the following community facility options:

- **Integrated Early Years Facility** – providing a range of services and programs for families with young children in one location and close to other support services such as primary schools
- **Intergenerational Community Facility** – providing a range of community spaces that support community needs and promote intergenerational contact and connection, contributing to increased social participation and community wellbeing
- **Health and Wellbeing Facility** – providing opportunities for active ageing, community health, social connection and increased community wellbeing

There are a number of sites within the Centre that could incorporate the new community infrastructure, into a redevelopment or expansion of existing facilities.

Co-locating these new facilities with the existing Clayton Community Centre, and Aquatics and Health Club on Cooke Street may be a suitable option. The new facilities could compliment the existing community infrastructure, in a central location. Potentially, the new facilities could be located within a redeveloped Community Centre on land owned by Council, or included as part of a redevelopment of the Cooke Street car park.

Another suitable location for such a facility would be the Clayton Hall Site located north of the railway line. This site is owned by Council, located close to the primary school and church and will serve the existing and future residents and workers in the northern sections of the Activity Centre. Any redevelopment of the site must ensure that existing and future community facilities are included in the proposal.

Objective

To provide community, civic and cultural facilities and services that cater to the needs of existing and future populations

Strategies

- *Establish a new multi-purpose community facility in Clayton Activity Centre*
- *Promote additional community events and festivals throughout the year*
- *Strengthen the role of the Clayton Community Centre by improving its connection with the shopping precinct*

Actions

- D.1 Undertake detailed planning of future community facilities including the composition of facilities, preferred site(s) and timing for delivery of the future infrastructure
- D.2 Develop a program of additional community events and festivals to occur in the existing streets and public spaces of the Clayton Activity Centre
- D.3 Utilise future public spaces such as the proposed Cooke Street shared space and plaza for public events and markets
- D.4 Plan for a strong pedestrian connection between Clayton Road and the Clayton Community Centre if the Cooke Street car park is redeveloped



Image 11. Clayton Community Centre

E. Housing

At the 2016 Census, there were approximately 10,463 people living within the Clayton Activity Centre across 3,257 dwellings. Of this count, only 25 people were living within the commercial core of Clayton.

Population projections prepared as part of the precinct plan forecast that there will be an additional 3,125 people will be living in Clayton Activity Centre by 2036 across 1,157 new dwellings.

The existing residential areas will have a significant role to play in delivering this future housing through the redevelopment of single dwellings for townhouses and low scale apartment buildings. These areas are well located close to the railway station, shops, community facilities, medical precinct and Monash University.

The Residential Growth Zone is currently applied to a substantial amount of the surrounding residential areas. This zone aims to provide for higher density housing and allows for development of up to four storeys with front, side and rear setback requirements. The Precinct Plan recommends further expansion of the zone to incorporate areas that are very close to transport, shops and services, and are located nearby strategic redevelopment sites.

The commercial areas of Clayton also provide good opportunities for mixed-use apartment developments that can meet some of the housing needs. Providing housing within the commercial core of Clayton will contribute to active and vibrant streets, and provide greater demand for additional shops, cafés, restaurants and services.

Further to the above, by 2036 there will be a shortfall in social and affordable housing within

the Eastern Metropolitan Region. In particular, it has been noted that the social housing shortfall in the City of Monash will be 2,860 dwellings by the year 2036.

Clayton presents an ideal location for social and affordable housing and is included on the Department of Human Service's list of preferred locations.

***'there will be an additional 3,125 people living in Clayton Activity Centre by 2036 across 1,157 dwellings.'** (UE, 2019)*



Image 13. Existing apartment housing within Clayton

Objective

To provide a diverse range of housing types within the Activity Centre that caters to the needs of existing and future residents and meets expected population growth

Strategies

- *Provide broader housing choices within the Activity Centre to support the needs of families, couples, singles, older people, students and multi-generational families*
- *Support higher density development on key redevelopment sites within the Commercial Areas of the Activity Centre*
- *Promote residential uses above retail and office premises to provide for additional people living within the commercial area, and provide greater surveillance of streets and public spaces*
- *Provide for a greater diversity of housing types within the residential areas of the Activity Centre*
- *Support the development of adaptable housing that can meet the needs of all users*
- *Encourage opportunities for affordable housing across the Activity Centre*
- *Encourage the consolidation of allotments within the Commercial and Residential Areas of the Activity Centre to provide for greater efficiency and higher amenity in new housing developments.*

Actions

- E.1 Prepare and implement strategies that encourage the re-use of existing multi-level buildings and additions to existing shops for housing
- E.2 Prepare amendments to the planning scheme to encourage higher density housing on key strategic redevelopment sites within the Activity Centre
- E.3 Prepare amendments to the planning scheme to expand the Residential Growth Zone 3 to enable a greater diversity of housing types in residential areas
- E.4 Implement public realm, land use and access improvements identified in the Precinct Plan to attract housing to the Activity Centre



Image 14. Examples of other high quality, medium density built form residential outcomes

4.2 Built form

The Vision promotes a new urban character with excellence in the quality of the built form. This theme outlines initiatives to deliver high quality built form. Figure 11 identifies the preferred building heights and setbacks across the Activity Centre. Refer to Chapter 5 for more detailed built form recommendations.

F. Built Form and Design

Clayton's predominant one and two storey, interwar and post-war development means that there are limited examples of high quality architecture. This provides an opportunity for new buildings to define a contemporary and exciting character for the Centre.

One element of a strong built form character is taller buildings, which punctuate the skyline and create visual interest. Currently, the only buildings of significant scale (5-6 storeys) are located within the Monash Medical Centre. There is however an approval for a seven storey commercial building on Carinish Road.

The Precinct Plan aims to locate taller buildings to locations within the commercial core of Clayton, on larger redevelopment sites and in prominent locations to contribute to creating a stronger entrance into Clayton. The building heights identified in Figure 11 demonstrate how this has been applied.

Figure 11 also identifies key areas within the Activity Centre where sunlight should be provided to footpaths and public spaces at key times of the year. In these areas, building heights may need to be limited and upper level setbacks provided to allow sunlight to the key public space.

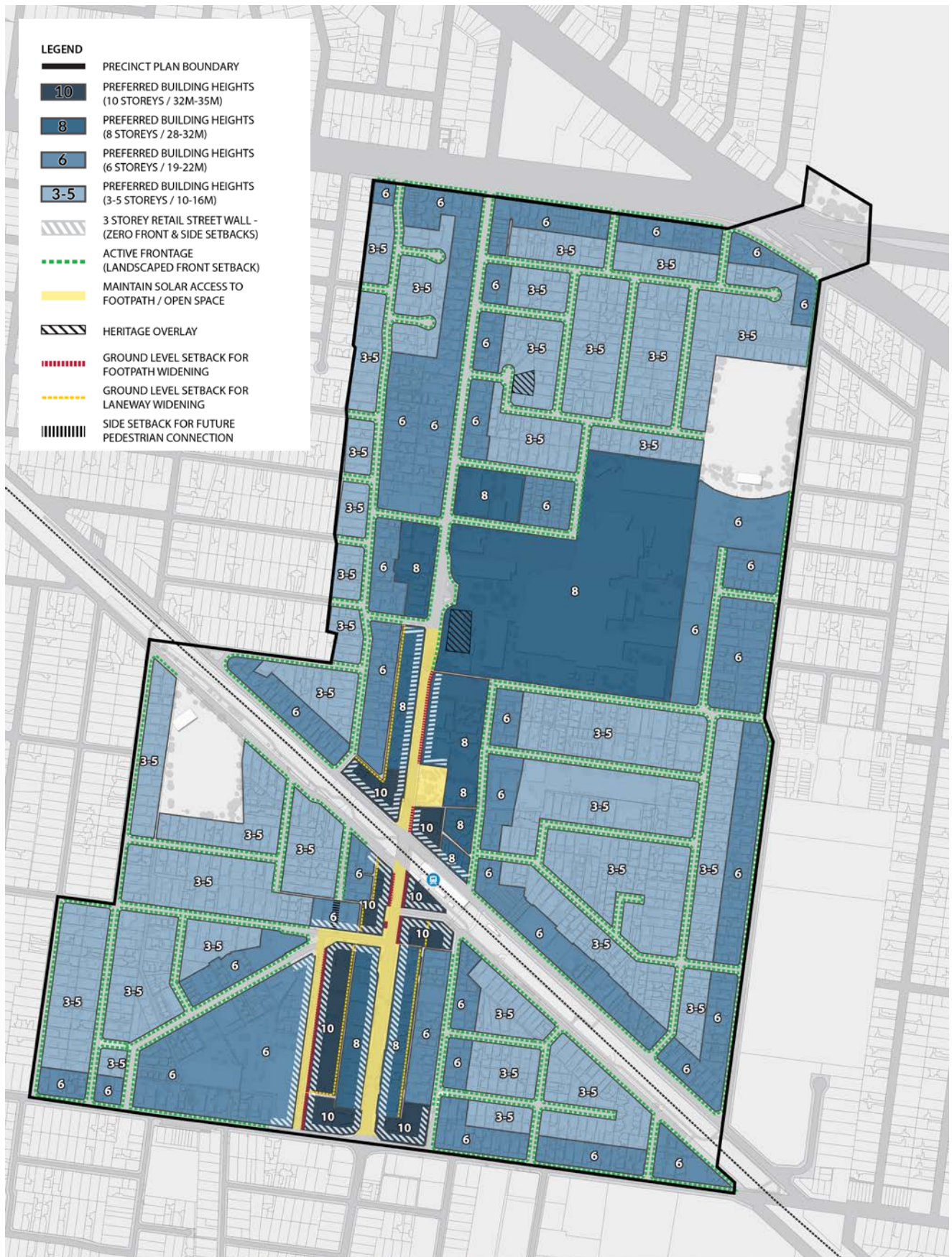


Figure 11. Clayton Activity Centre Built Form Heights Plan

The character of built form will change across the Activity Centre to accommodate the future land uses proposed. In particular, the section of Clayton Road north of the Railway Line and south of Colonel Street will transform from detached dwellings and medical suites set in gardens into attached shopfronts and mixed use buildings with no setback to the street. This will support retail and hospitality uses in this area.

The Precinct plan also promotes high quality building design that enhances the streets and public spaces. This not only includes how buildings are designed to integrate with the streetscape but also how the building addresses the adjoining footpaths and adjoining properties. The Strategies outlined opposite and detailed built form recommendations in Chapter 5 provide details on how this will be achieved.



Image 16. Opportunities for high quality housing

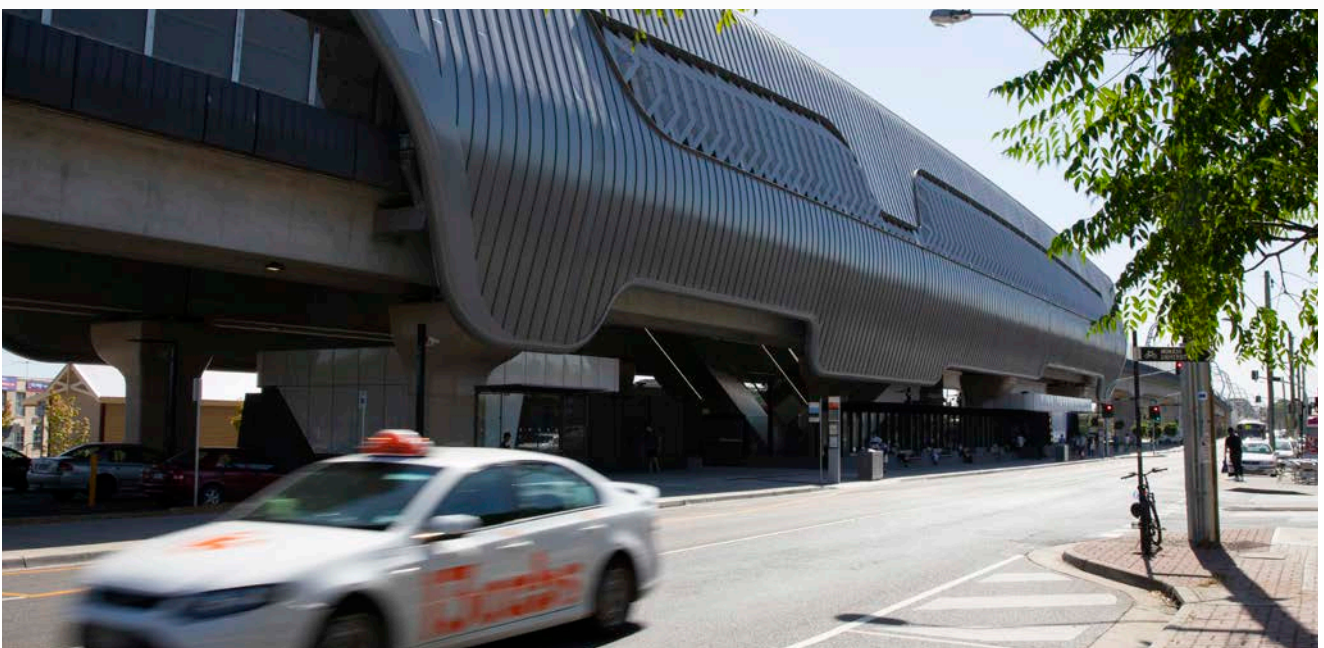


Image 15. High quality built form improves the aesthetic and sense of place of the area

Objective

To create a contemporary and exciting identity for the Clayton Activity Centre through high quality design and architectural excellence

Strategies

- Encourage excellence in building design within both commercial and residential areas so that development responds to the characteristics of the site and its context
- Maximise building height on larger sites where interface issues can be minimised
- Strengthen sense of place and arrival in the Clayton Activity Centre by providing buildings of exemplary architectural quality on prominent sites and in locations adjacent to existing and proposed public spaces
- Ensure buildings do not overshadow key public spaces such as key commercial areas streets, parks and plazas
- Ensure buildings do not overwhelm the footpaths of retail areas with taller element recessed from the street.
- Ensure buildings integrate with and contribute positively to streetscapes and public spaces by providing a continuous network of fine-grain active frontages with clear glazed ground floors
- Encourage consolidation of sites through greater height on longer sites.

- Strengthen the presence of buildings along the entire length of Clayton Road to promote increased interaction between the street and future retail, health and office uses.
- Ensure a transition in building height between commercial and residential areas
- Ensure that all development supports a sustainable activity centre through high ESD standards, including energy efficiency, water management and use of low embodied energy materials

Actions

- F.1 Prepare and implement planning scheme policies and controls that reinforce the strategies outlined above and the detailed built form recommendations for each precinct outlined in Chapter 4.



Image 17. Taller building forms

Table 2. Built form strategies xxxx

4.3 Public realm

The Vision aims to provide streets and public spaces that are inspiring and people oriented. This theme provides initiatives on how the public realm should respond. Figure 12 reflects the future public realm recommendations across the Clayton Activity Centre



Image 18. Existing high quality public realm

G. Streetscapes

The Streets of the Clayton Activity Centre not only play an important role in moving people around but also as spaces that provide places for shopping, social interaction and recreation. Clayton Road is perhaps the most important street in the Activity Centre because of its retail and hospitality role.

Successful retail strips such as Clayton Road are characterised by a number of elements that make people want to stay and enjoy the area. These include generous footpath widths, outdoor dining, tree canopy cover, and slow vehicle speeds making people feel comfortable and safe. Clayton Road is currently a car dominated environment and lacks a number of these elements. The significant traffic volumes and four vehicle lanes diminishes the pedestrian experience and provides a barrier between the east and west sides of the street.

Since the level crossing was removed, vehicle traffic has increased, indicating that more vehicle users are utilising this street as a north-south access route. This is not considered a desirable outcome for Clayton Road given its role as the retail and hospitality spine for the Activity Centre.

The Precinct Plan recommends revisiting the role and reconfiguration of Clayton Road to make it a better place for people. The preferred configuration includes the removal of a traffic lane in each direction (where traffic management can support it). This will allow for wider footpaths and a planted central median, which will create a better place for people to gather and a less car dominated environment.

As Clayton Road is designated as an arterial road, further discussions with VicRoads will be required to better understand improvement scenarios. This will include understanding the impact of the Westall Road extension in reducing traffic along Clayton Road.



Figure 12. Clayton Activity Centre public realm projects

In addition to Clayton Road, the Precinct Plan recommends the establishment of a consistent high quality streetscapes within the commercial areas of the Activity Centre. This will define the retail and commercial core of Clayton and provide a high quality destination for businesses.

The Precinct Plan also recommends a number of other residential streets across the centre for improvement. These streets help people move between key destinations across the centre and could provide better experience for pedestrians with additional street tree planting, consistent footpaths and priority over vehicles.

Objective

To provide streets across the Clayton Activity Centre that are people focused, green and strengthen the economic and social vitality of the centre

Strategies

- *Enhance Clayton Road as the primary street within the Activity Centre by providing a high level of pedestrian priority and amenity to support retail and hospitality uses*
- *Clearly delineate the commercial streets within the Activity Centre by providing consistent landscaping and paving and a high level of pedestrian and cyclist priority*
- *Improve entries into the Activity Centre by providing additional planting, landscaping and public art along main roads*
- *Enhance key residential streets that connect into the commercial areas to encourage walking for short trips within the Centre.*
- *Provide for continual weather protection along key streets within the commercial area of the Activity Centre*
- *Ensure streetscape works utilise best practice water conservation, treatment and re-use in their design*

Actions

G.1 Work closely with the community, traders and other stakeholders to prepare a streetscape master plan for Clayton Road. Refer to Chapter 5 for details

G.2 Commence discussions with VicRoads regarding the application of their Movement and Place program to Clayton Road, with the aim to downgrade the traffic role of the road and provide a higher level of pedestrian amenity

G.3 Develop a Streetscape Master Plan for all streets within Clayton Activity Centre that:

- Identifies a hierarchy of streets within the commercial area and applies a suite of materials, planting and furniture for each street
- Identifies a landscape response to key arterial roads and entries that emphasises street tree planting, landscaping and provision of consistent footpath treatments and width
- Identifies improvements to key residential streets providing consistent street tree planting, lower level landscaping and provision of consistent footpath treatments and width
- Integrates water sensitive urban design techniques to provide for water treatment and passive irrigation of trees and planting

G.4 Develop a co-ordinated lighting strategy which focuses on illumination of streets and other public spaces and the use of feature lighting to emphasise key features

G.5 Develop planning measures that ensure the provision of building awnings in the locations identified as Weather Protection Areas in Figure 12



Figure 13. Artist's impression of Clayton Road streetscape upgrades, including a reduced traffic role for Clayton Road, planted central median, wider footpaths and redevelopment of some existing buildings.

H. Parks and Public Plazas

The Clayton Activity Centre is projected to expand significantly in regards to residents, workers, students and visitors. It is important that the provision of public infrastructure such as parks, plazas and civic squares also expands and improves.

There is currently a lack of high quality 'green' public open space within the Clayton Activity Centre. Fregon Reserve and Meade Reserve provide important sporting roles however have limited passive recreation facilities.

The recently completed linear parklands provided as part of the Level Crossing Removal Project have provided a marked increase in the quality and quantity of public open space within the Activity Centre. However, the linear parklands are primarily hardstand surfaces.

The Remembrance Gardens in front of Clayton Hall is one of the only 'green' parks provided within the Centre. There is an opportunity to enhance the Remembrance Gardens as a key park for the Centre, if the Clayton hall is redeveloped (with community facilities included within the new development), as this space will provide an increasingly important role as the health employment increases in the nearby Monash Medical Centre.

The Precinct Plan proposes a potential 'pocket park' adjacent to the Thomas Street car park to address the current undersupply of open space in the south east section of the Activity Centre. This park will not only serve residents in the area but also provide a destination for shoppers and workers on the east side of Clayton Road.

In addition a number of new, hardscaped plaza spaces, are also proposed through the redevelopment of Council land or re-configuration of road space. The Cooke Street car park is a major opportunity and could be provided as part of any future redevelopment of the car park. This plaza is located in the heart of the retail core and

could enhance the connection between Clayton Road and the Community Centre.

Another identified opportunity is through the re-configuration of Haughton Road, adjacent to the new hard court spaces under the railway line. This will build on the popular hard courts and provide some greenery and expanded outdoor dining areas for adjacent businesses.

In addition to new public space there is also an opportunity to deliver water sensitive urban design in existing streets and spaces, to provide for treatment of stormwater and re-use of water. The linear green space adjacent to the railway line is identified as a key opportunity.

Objective

To provide a range of public and civic spaces that support community gathering, social interaction and passive and active recreation

Strategies

- *Provide new public gathering and event spaces within the commercial core of the Centre including plazas and squares*
- *Provide new 'green' public open space within the commercial core of the Centre that can be conveniently accessed by residents, shoppers, workers and visitors*
- *Enhance Memorial Gardens as a key open space destination for the Health and Medical precinct and nearby residents*
- *Pursue opportunities for public art and cultural interpretation within existing and new public spaces*
- *Ensure public spaces for both passive and active uses employ sustainable practices such as innovative water conservation, harvest and reuse strategies in their design*



Figure 14. Example of a high quality plaza space

Actions

- H.1 *Develop a new public square / plaza within the Cooke Street car park site as part of its redevelopment. The plaza should connect Clayton Road through to the Community Centre and provide a space suitable for events. Refer to Chapter 5 for details*
- H.2 *Provide for a new public plaza on Haughton Road that integrates with the newly developed Level Crossing Removal hard court spaces. Refer to Chapter 5 for details*
- H.3 *Develop a new pocket park as part of any redevelopment of the Car park at the corner of Centre Road and Thomas Street*

4.4 Movement and transport

The Vision aims to transform Clayton into a regional transport hub with excellent public transport access to Melbourne and the wider regions, which connects seamlessly into a comprehensive walking and cycling network. Figure 15 and 16 reflects the future movement and transport recommendations across the Clayton Activity Centre.



Image 19. Upgraded shared path infrastructure

I. Walking & Cycling

Walking and cycling are enjoyable and easy forms of transport that bring significant health, economic and environmental benefits. The Precinct Plan proposes a number of walking and cycling improvements which will create clear priority and make walking and cycling the preferred mode of transport for short trips within the Centre.

The Plan delineates a network of streets within the commercial core where the highest level of pedestrian priority should be provided. A number of projects are proposed in this area including streetscape upgrades and measures to make the streets less car-dominated and more pedestrian friendly.

The surrounding residential streets of Clayton also play an important role in getting people to and from the shops, station, hospital and education facilities. These streets provide good opportunities for improvement to promote walking as a logical choice for short transport trips. A Principal Pedestrian Network is delineated along key streets that link residents to key destinations across the Centre.

A particularly important link is the connection from the Clayton Railway Station to the Monash Medical Centre and up to the Monash University. Establishing a clear and logical path network between these destinations will further strengthen the synergies between the Medical Centre and the University, and encourage more student housing within the Activity Centre.

A major barrier for this link is the Princes Highway and Wellington Road. This requires significant waiting times at signals and the crossing of 17 lanes of traffic for pedestrians and cyclists. Some improvement has been made with the recent installation of pedestrian signals at Cobain Street, however the time taken to cross is still significant.

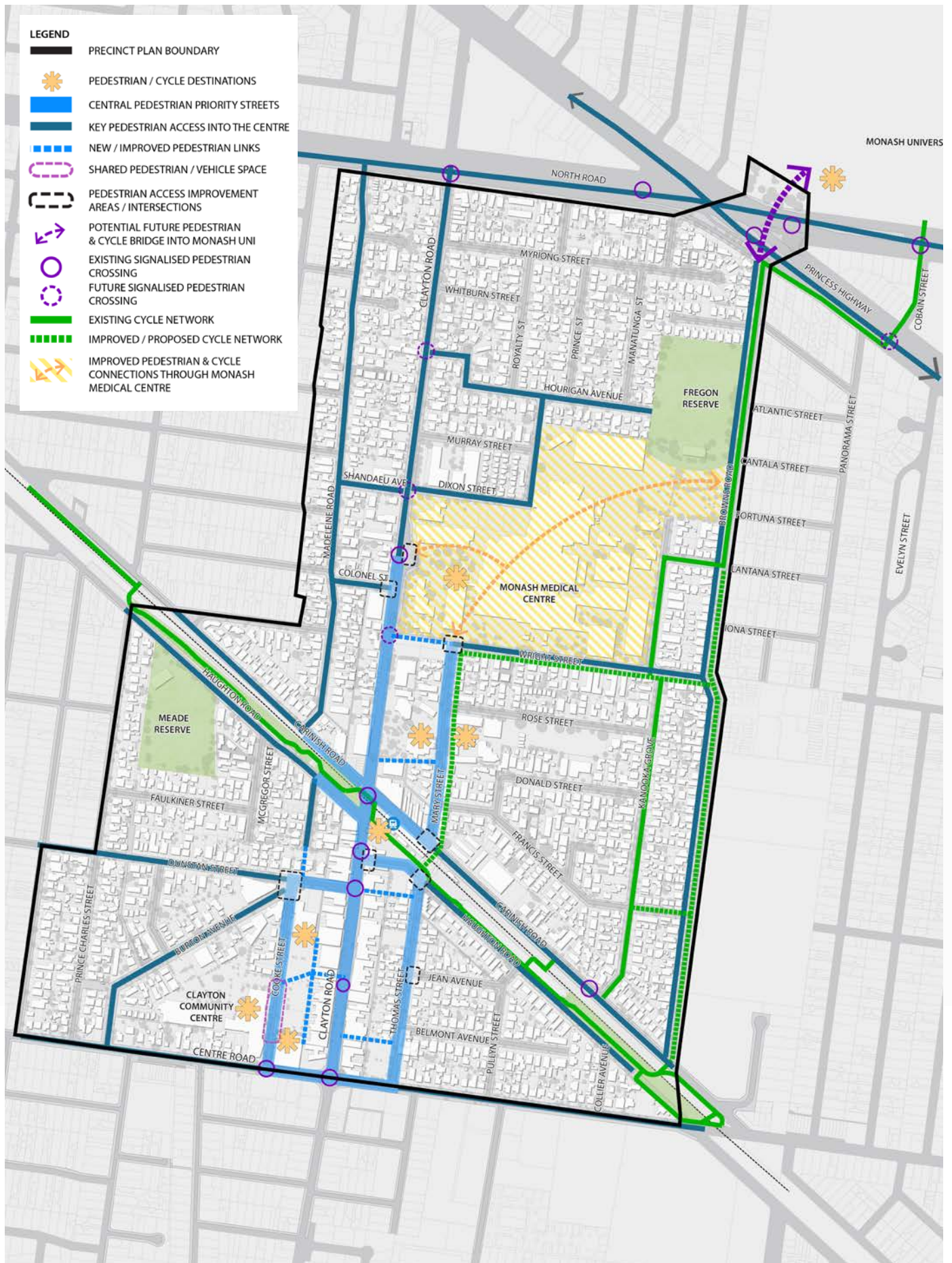


Figure 15. Pedestrian and cyclist movement plan

The Precinct Plan supports the provision of a large footbridge over the Princes Highway connecting from Browns Road into northern footpath of Wellington Road and the University. This project would require significant external funding, but provides for the most direct and safest solution without limiting vehicle movements along the Princes Highway and Wellington Road.

Key cycling routes within the Centre are located along the railway line providing connection between Caulfield and Dandenong. This route is provided along a newly constructed shared path

Objective

To prioritise walking and cycling across the Clayton Activity Centre through a connected, convenient, comfortable and safe network

Strategies

- *Provide a high level of pedestrian priority along all streets within the Commercial Area of the Activity Centre*
- *Reduce vehicle speeds and provide traffic calming along Clayton Road and other streets within the Commercial Core and Health Precinct*
- *Strengthen the pedestrian and cycle links between the Commercial Core, Clayton Railway Station, Monash Medical Centre and Monash University*
- *Enhance existing laneways within the Activity Centre to provide a higher level of amenity and safety for pedestrians*
- *Improve pedestrian priority and infrastructure along key residential streets that connect into the Commercial Core*
- *Ensure new public realm works provide for a high level of DDA compliance*
- *Provide bicycle parking within the Commercial Core and Health Precinct to encourage people to cycle to the centre*
- *Encourage the provision of trip end facilities for cyclists and walkers within all new developments including, lockers, showers, change rooms and clothes drying rooms*

Actions

- I.1 Develop a Principal Pedestrian Network for the Activity Centre to reinforce pedestrian priority along key streets both within the Commercial Core, Health Precinct and surrounding residential areas
- I.2 Provide pedestrian improvements along key commercial streets including the widening of footpaths in a number of locations, raised paving at intersecting side streets, improved lighting and street tree planting. Consider speed reductions and traffic calming measures for vehicles
- I.3 Widen footpaths along both side of Cooke Street to support a future retail and hospitality role for the street
- I.4 Create a shared pedestrian and vehicle zone along Cooke Street to better integrate the Community Centre with Clayton Road. This would be undertaken as part of a redevelopment of the Cooke Street Car Park
- I.5 Upgrade key laneways for lighting and surfacing where pedestrian activity and movement is desired
- I.6 Undertake a Disability Discrimination Act (DDA) audit of the key commercial to assess the access conditions for people with limited mobility
- I.7 Work with key stakeholders to understand the design, cost, benefits and funding opportunities for pedestrian and cycle footbridge connecting across the Princes Highway and Wellington Road from Browns Road to the Monash University
- I.8 Work with VicRoads to minimise pedestrian waiting times within and abutting the Activity Centre, along Clayton Road, Centre Road, Princes Highway and Wellington Road
- I.9 Work with VicRoads to implement additional signalised pedestrian crossings along Clayton Road
- I.10 Complete the implementation of a shared pedestrian and cycle path that extends along Kanooka Grove, Lantana Street, Browns Road and Princes Highway to better connect the railway station up to the Monash Medical Centre and Monash University
- I.11 Undertake an audit of bicycle infrastructure across the Activity Centre to determine if additional facilities are required
- I.12 Develop Planning Scheme measures to ensure end of trip facilities are provided in new developments for walkers, runners and cyclists
- I.13 Install co-ordinated wayfinding signage in key locations to direct pedestrians to key precincts, shops, transport, community facilities and open space

J. Public Transport

Clayton has been experiencing significant public transport change over a number of years through the removal of the level crossing on Clayton Road and the construction of the new elevated rail line and station. These changes have improved the frequency and reliability of the bus network by providing less disruption to services along Clayton Road and additional bus stops.

A number of transformative public transport proposals have been mooted for the Clayton Activity Centre and surrounding area, which will impact significantly on the Clayton's accessibility and economic role. Due to election commitments by the re-elected Andrews Government, the Suburban Rail Loop is expected to become the next public transport infrastructure project to impact the Clayton Activity Centre.

The Suburban Rail Loop will connect outer Metropolitan Melbourne together through a single rail line and also provide the interchange of V/Line and Metro services. Stage one is to be delivered by 2032 and includes the southeast section between Cheltenham and Box Hill, via Clayton Station and Monash University.

Clayton is identified as a regional 'super-hub' interchange station for metro and V/Line services. This will provide a significant increase in the volume of regional passengers accessing the Centre for its health, education and commercial opportunities, specifically those on the Bairnsdale regional line. It will also provide regional Victorians with radial access from Clayton to other metropolitan centres, including Glen Waverley, Box Hill, Heidelberg and Reservoir.

Another important public transport project is the Caulfield to Rowville Light Rail which is proposed to connect Caulfield Station to Monash University in Clayton in its first stage, and then connection to Rowville along Wellington Road as part of

stage 2. The light rail will provide a local level of connection as travel times will be similar or even slower than private car. However, it will enable land use change and development along the route, especially proximate to the stops.

The third public transport project that has identified is the first stage of the Rowville Rail project, which provides a spur line under North Road / Wellington Road between Huntingdale Station and the University Campus. This proposal would support increased access to the MNEIC, mainly the University Campus, along with additional activity along Wellington Road. There is seen to be limited benefit for the Clayton Activity Centre as the spur line comes off the same line as the Clayton Station is on.

These public transport proposals will increase the value of the land within Clayton once funding has been committed, and once construction is complete. This increase in land value should be captured and invested into improving infrastructure, services and the public realm across the Centre. This will ensure the Centre can best cater to the needs of the significant increase in residents, workers and visitors.

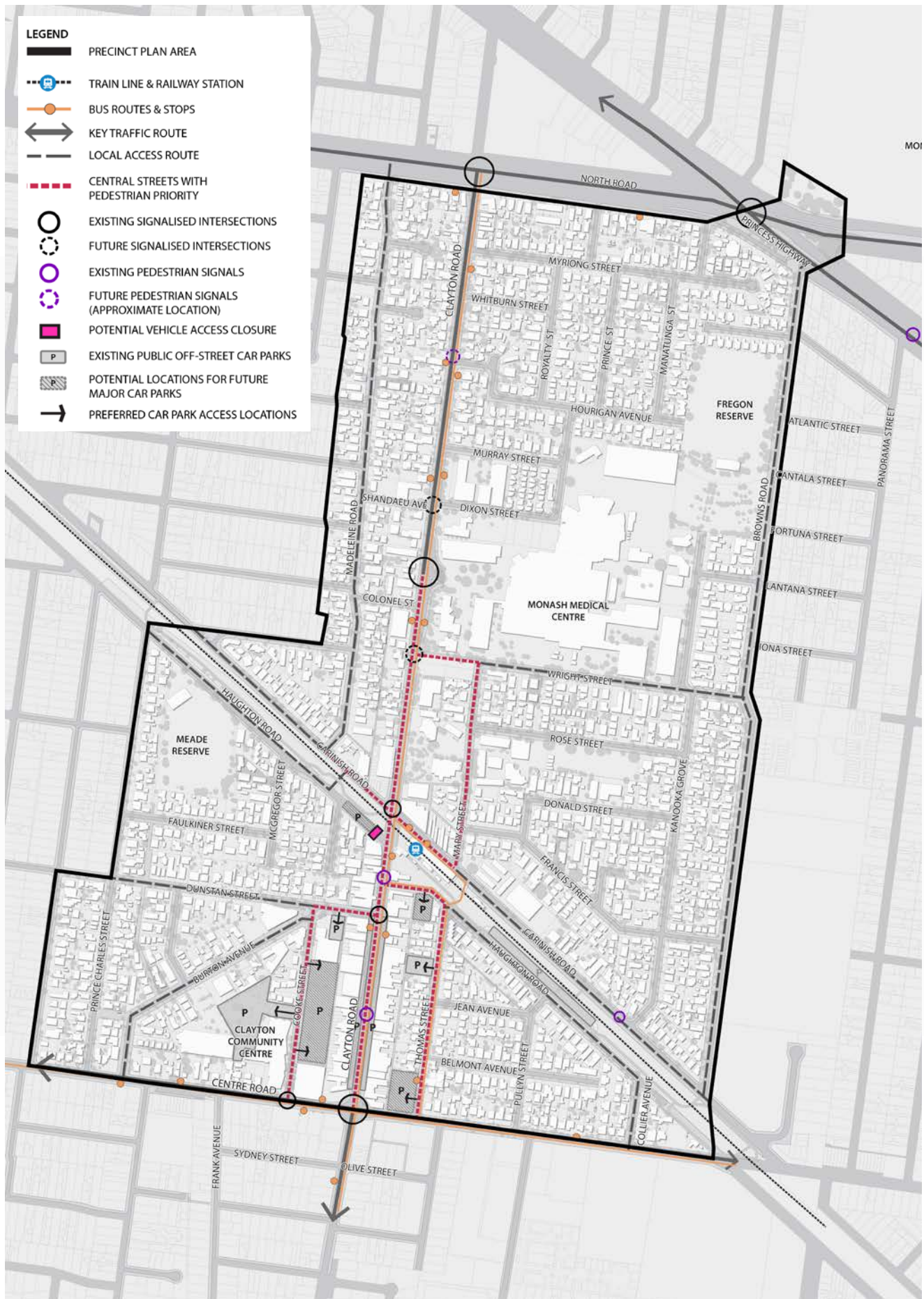


Figure 16. Proposed movement and access arrangements

Objective

To support public transport as an attractive and logical choice to access the Activity Centre both locally and regionally

Strategies

- *Improve the frequency, integration, comfort and amenity of public transport*
- *Advocate for the implementation of the Suburban Rail Loop project and the development of Clayton as a regional public transport interchange*
- *Advocate for the Caulfield to Rowville Light Rail project and the Rowville Rail project*
- *Advocate for the improvement of bus services within the Activity Centre*
- *Capture value uplift created through major public transport infrastructure projects, to fund community infrastructure and public realm projects*

Actions

- J.1 Work with the State Government and other stakeholders to better understand the potential design, economic and movement impacts of the Suburban Rail Project on the Clayton Activity Centre
- J.2 Work with the State Government to better understand the likelihood of the Rowville Light Rail project and the Rowville Rail project and potential impacts on the Clayton Activity Centre
- J.3 Improve the location and design of existing bus stops along Clayton Road as part of any streetscape upgrade
- J.4 Enhance pedestrian links and signage between the Railway Station and bus stops along Clayton Road south of Dunstan Street
- J.5 Improve bus movements along Clayton Road by reducing vehicle traffic along the road through various proposals outlined in the Precinct Plan
- J.6 Advocate for improved bus services including higher frequencies, additional services, user focused timetabling and higher reliability through dedicated bus lanes to access local areas that rail does not service
- J.7 Investigate appropriate mechanisms for value capture to fund improvements to infrastructure, services and public realm projects

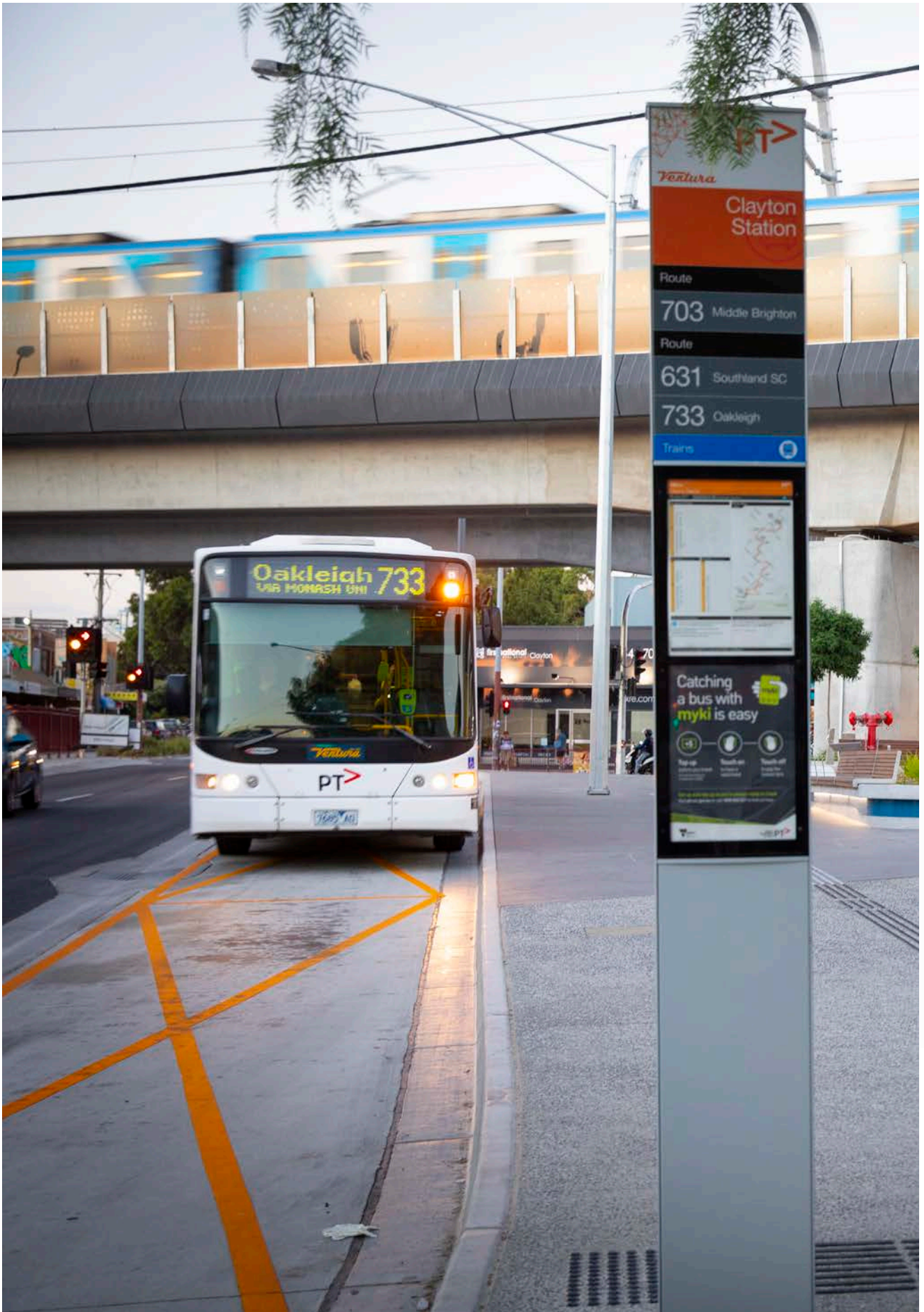


Image 20. Existing bus stop in proximity to the Clayton train station

K. Car Parking

There are approximately 900 off-street publicly available car parking within the centre, which are primarily located in six at-grade car parks. These spaces cater for those accessing the shops and services within the retail core. All of these spaces are well utilised and approaching capacity in some locations during peak times.

Clayton Road provides a total of 68 on-street parking spaces between Centre Road and Dunstan Street. They have an average occupancy of approximately 95% during the day, with this demand remaining constant on both weekdays and weekends.

The Precinct Plan aims to reduce the demand on parking in the longer term through a range of walking, cycling and public transport improvements. These improvements will encourage a mode-shift from vehicle access to the Centre to more sustainable forms of transport.

In the shorter term, there are opportunities to provide better access to the existing car parks to encourage better utilisation and reduce vehicle congestion along Clayton Road. Defining clear vehicle access to the car parks from Centre Road, Dunstan Street and Haughton Road will remove unnecessary movements along Clayton Road. Dynamic signage will allow people to quickly identify if any spaces are available in each facility.

Another improvement will be providing better pedestrian access between the car parks and shops along Clayton Road. A number of new mid-block links are proposed to allow easy walking access once parked.

The Precinct Plan also recommends the preparation of a Car Parking Strategy. This will enable the current and future demands for parking to be assessed in detail to better understand if additional parking facilities are required and mechanisms for funding future parking infrastructure.

Objective

To provide car parking easy to access with minimal disruption on the public realm whilst decreasing demand for parking in the longer term

Strategies

- *Locate car parking facilities in strategic locations around the Activity Centre that can be accessed with minimal disruption to Clayton Road*
- *If existing at-grade car parks are redeveloped, ensure adequate parking is provided to replace existing parking and meet the demand generated by new land uses*
- *Provide for greater use of off-street car parks to reduce parking demand along Clayton Road*
- *Provide for safe and convenient pedestrian access to existing and future off-street car parks*
- *Encourage car-share services and parking within the Activity Centre*

Actions

- K.1 Develop a Car Parking Strategy for the Clayton Activity Centre to assess the future provision of parking based on current and future demand, management of parking and access to parking
- K.2 Implement additional pedestrian links from existing at-grade car parks through to Clayton Road as shown in Figure 16
- K.3 Develop signage to direct vehicles to car parking locations along preferred routes as shown in Figure 16
- K.4 Undertake an audit of existing Council car parks to assess their accessibility and safety for pedestrians
- K.5 Implement pedestrian, cycle and public transport improvements outlined in the Precinct Plan plan to encourage people to use other modes of transport to access the Centre and reduce demand for parking



Image 21. Multi-level car park appropriately screened



Image 22. Multi-level car parking sleeved with residential uses above ground

L. Vehicle Access

The arterial road network within and around Clayton is quite uniform, allowing for good accessibility in all directions, but ultimately it funnels traffic down into Clayton Road, causing significant congestion. The local road network also funnels traffic onto Clayton Road through a number of traffic management measures aimed to reduce 'rat-running'.

The planned extension of Westall Road, which runs parallel Clayton Road is expected to divert traffic and freight from Clayton Road and improve traffic movement from the Princes Highway to the Monash Freeway. This will support a reduced traffic role for Clayton Road, allowing for an enhanced streetscape and improved pedestrian environment along the road.

The Movement and Place framework has been developed by Transport for Victoria and VicRoads. It allows for roads such as Clayton Road to be re-assessed with the understanding that all transport corridors perform two functions; the movement of people and goods and serving as a place (a destination in its own right). Clayton Road is a good candidate for this program given its importance as a retail strip and the potential reduction in traffic from the Westall Road extension.

Another opportunity for reducing traffic congestion within the Clayton Activity Centre is through encouraging visitors, residents and workers to walk, cycle or use public transport to access the centre instead of their motor vehicles.

The forecast employment and residential growth, the proposed pedestrian and cycle improvements and the major public transport infrastructure investment in the Clayton Activity Centre provide good incentives for encourage a shift to sustainable modes of transport.

Objective

To provide clear and logical vehicle access into the Centre whilst decreasing demand for vehicle access into the Centre in the longer term

Strategies

- *Reduce vehicle movements and congestion along Clayton Road to enhance its role as a retail and hospitality street*
- *Support the implementation of the Westall Road extension to reduce traffic volumes along Clayton Road*
- *Support enhanced vehicle access into the Monash Medical Centre to improve its function and enhance pedestrian access and safety*

Actions

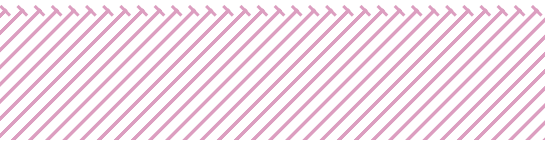
- L.1 Work with VicRoads to have Clayton Road assessed under the Movement and Place framework with the aim of increasing the role of Clayton Road as place for people and reduce its traffic role
- L.2 Lobby the State Government to fund the Westall Road extension
- L.3 Work with the Monash Medical Centre to determine the preferred vehicle access arrangements including the provision of additional signals as shown in Figure 16. Ensure pedestrian access into the Medical Centre and along Clayton Road and side streets is improved
- L.4 With the support of relevant authorities, narrow portions of Clayton Road to achieve a better pedestrian and public realm outcome



Image 23. Encourage public transport and active modes of transport to reduce vehicle use



Image 24. Opportunity for a reduced traffic role along Clayton Road to improve the amenity of the street



05

THE PRECINCTS

The Precincts section provides details on key projects and built form requirements that help achieve the Vision for the Activity Centre

Three precincts have been delineated and are described in the following pages

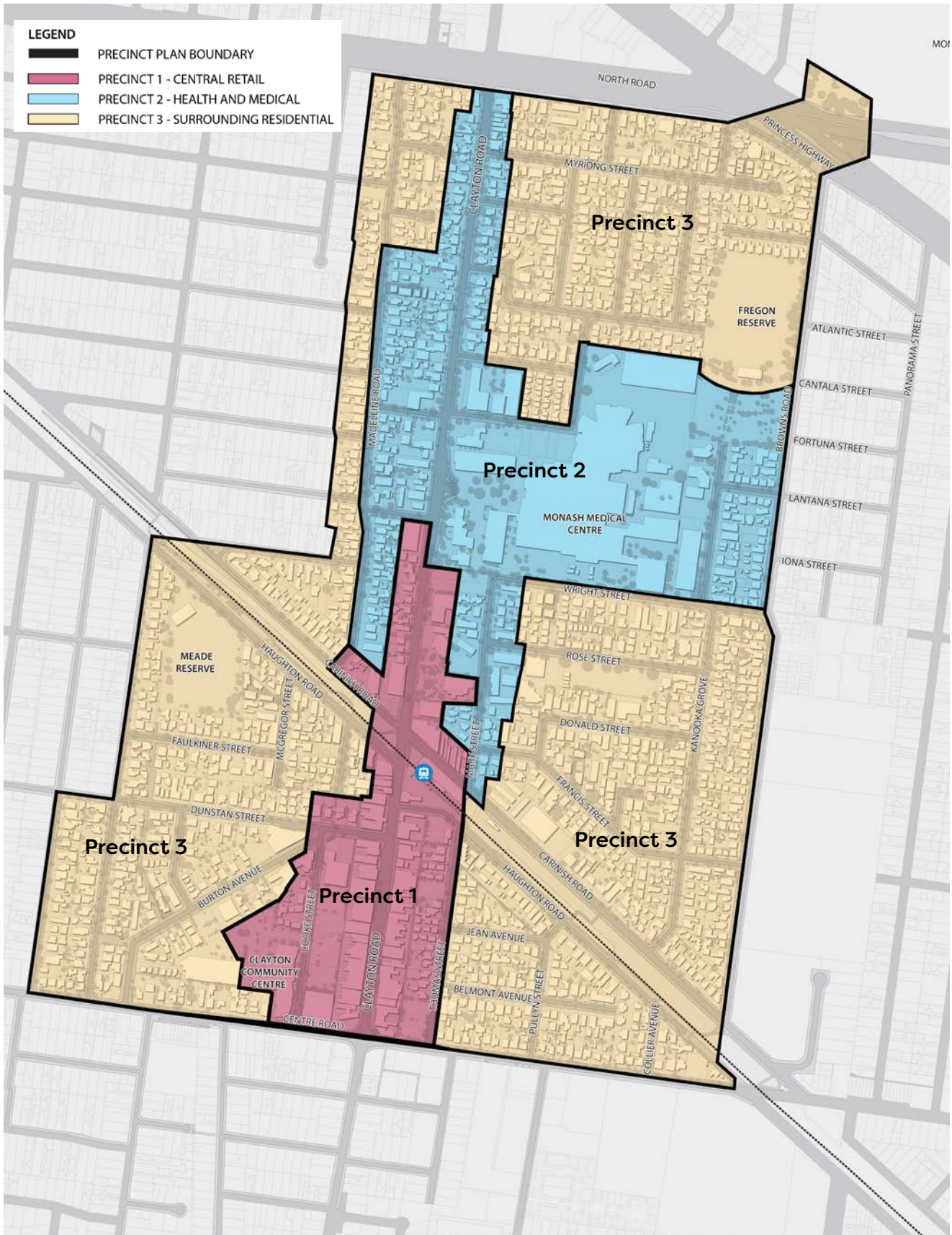


Figure 17. Clayton Activity Centre Precinct Boundaries



5.1 Precinct 1: Central retail

Precinct Overview

The Central Retail Precinct is the major focus for hospitality and retail uses for the Activity Centre providing interesting and vibrant streetscapes.

Clayton Road is transformed not only from streetscape upgrades but also its high quality, contemporary buildings, which provide for a greater sense of enclosure within the street. Offices and housing are provided in upper levels of development and help to activate the street across the day and night.

The Cooke Street car park presents a major renewal opportunity providing a new public plaza, multi-level car park, and a major mixed use development that connects Clayton Road through to the Community Centre.

A number of other renewal sites will deliver additional life into the Centre and provide opportunities throughout for living in the heart of the Activity Centre.

Higher scale development is visible around the railway station, at the southern entry to the Activity Centre and along Cooke Street creating variation in the skyline and marking these important locations.

Precinct 1: Key Projects

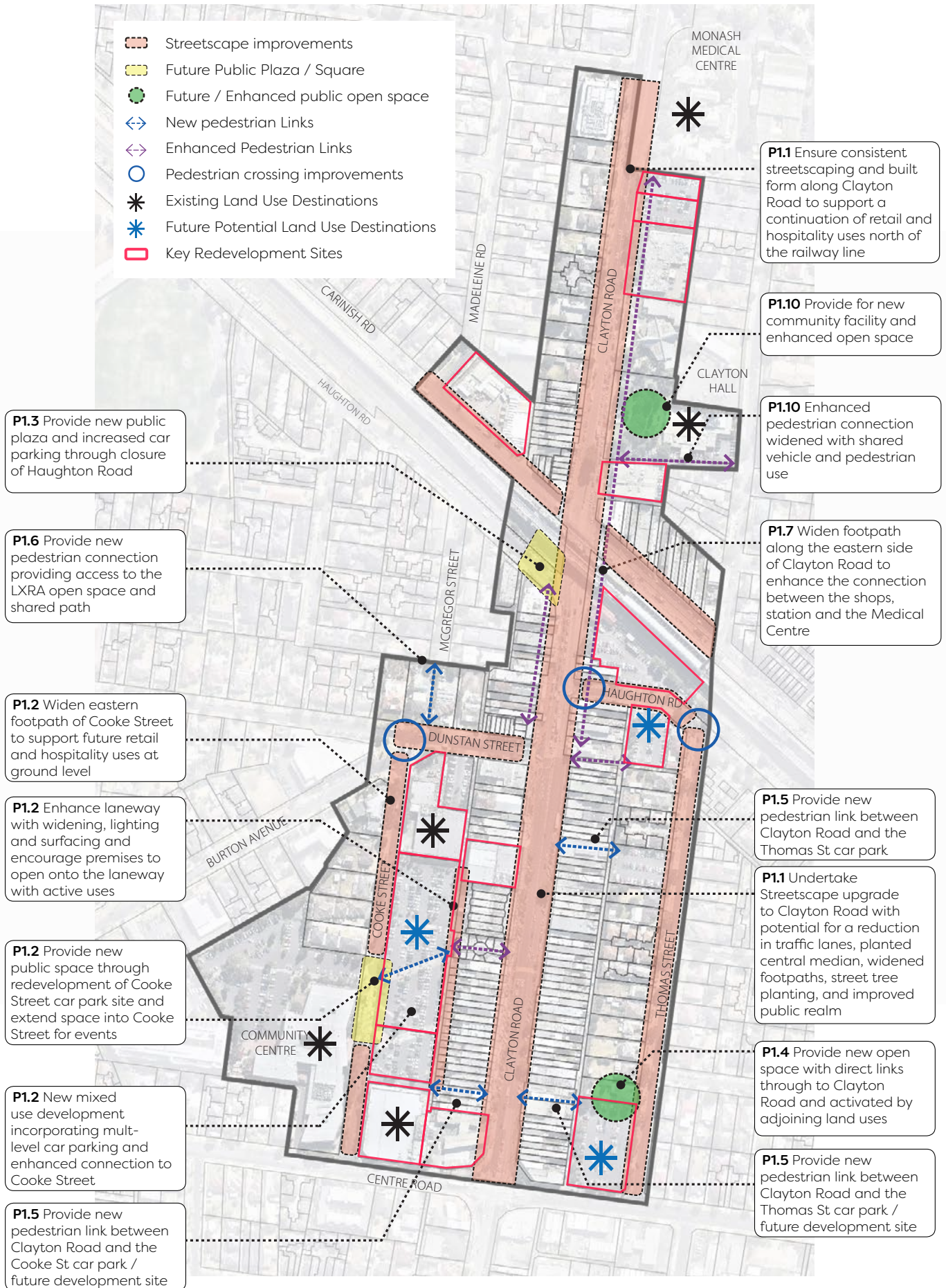


Figure 18. Precinct 1 - Key Projects

Precinct 1: Key Projects

Key Projects

P1.1 Clayton Road Streetscape Upgrade

- Work closely with the community, traders and other stakeholders to prepare a streetscape master plan for Clayton Road. The Master Plan will consider:
 - Potential removal of a traffic lane in some sections where effective traffic movements can be maintained.
 - Inclusion of a planted median to provide additional greening and allow for informal crossing opportunities along the road.
 - Consistent street tree planting and landscaping.

- Relocation and retention of limited convenience parking.
- Widening of footpaths to support outdoor dining and comfortable pedestrian movements.
- Providing high quality and consistent paving materials, street furniture and lighting.
- Configuration of car parking to provide consistent treatment
- Utilisation of water sensitive urban design techniques
- Undergrounding of powerlines

Figure 19. An artist's impression of Clayton Road Upgrade Scenario 3. Refer to Figure 21 for details



Clayton Road Existing Configuration

The existing cross section of Clayton Road shows a streetscape that heavily dominated by road space. There is a lack of impactful street tree planting and overhead powerlines are dominant elements



Figure 20. Clayton Road Existing Cross Section

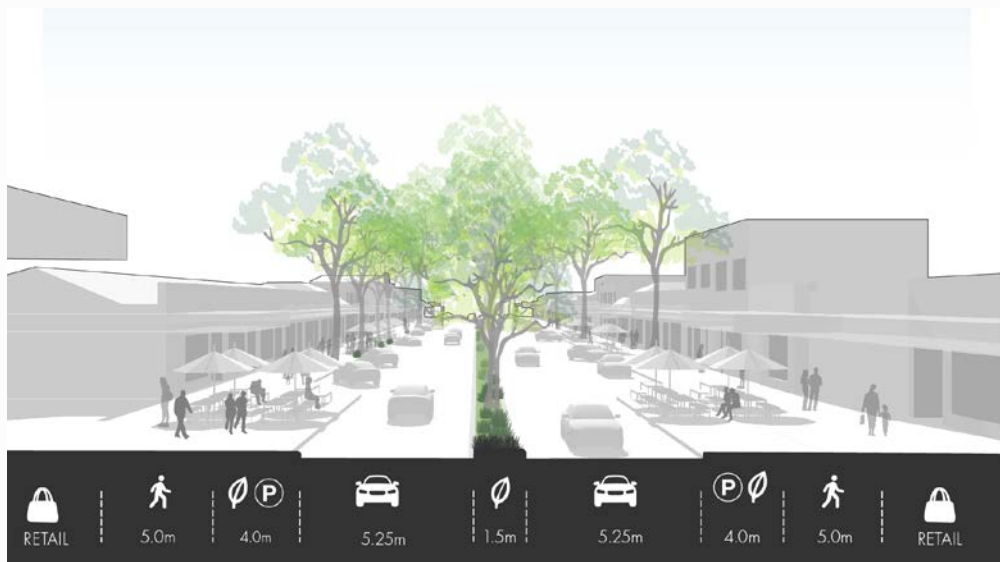
Clayton Road Upgrade Scenario 1



This scenario retains two traffic lanes in each direction however it provides a planted median. This is achieved by changing parking from angled to parallel.

This scenario provides a minimal change to existing pedestrian environment however it maintains the traffic role of Clayton Road. It also reduces the existing provision of parking.

Clayton Road Upgrade Scenario 2



This scenario removes a traffic lane in each direction, provides a planted median and retains angled parking. Footpaths will be similar in width to existing footpaths.

This scenario provide parking numbers similar to existing and will allow for easier informal crossing of the street because of a reduced traffic role and planted median.

Clayton Road Upgrade Scenario 3



This scenario removes a traffic lane in each direction, provides a planted median and parallel parking on both sides of the road. Footpaths will be expanded to provide for outdoor dining opportunities.

This scenario provides for the greatest improvement to the pedestrian environment through widened footpaths and reduced traffic lanes.

Figure 21. Clayton Road Upgrade Scenarios

Precinct 1: Key Projects continued

Key Projects

P1.2 Cooke Street Car Park

- Plan for the redevelopment of the Cooke Street car park. The redevelopment should:
 - Preferably be undertaken in conjunction with adjoining property at 20-22 Cooke Street
 - Support a large format retail use such as a supermarket or a discount department store
 - Provide for retail and hospitality uses at ground level and a mix of residential, office and commercial on upper levels
 - Replace existing car parking within a basement or elevated deck and provide sufficient parking to meet the demand of new uses provided on the site
 - Provide activation to Cooke Street, the future pedestrian plaza and the rear laneway
 - Deliver a new public plaza / square of sufficient size and suitable design to hold community events
 - Upgrade the existing walk through to 371-373 Clayton Road to better integrate Clayton Road to the new public plaza/square and provide better pedestrian amenity
 - Ensure buildings do not unreasonably over shadow the new plaza space
 - Allow for the widening of the footpath on the east side of Cooke Street to support the retail role of the street



Image 25. Opportunity for additional gathering spaces within Cooke Street



Image 26. Opportunity to provide active uses on the east side of Cooke Street

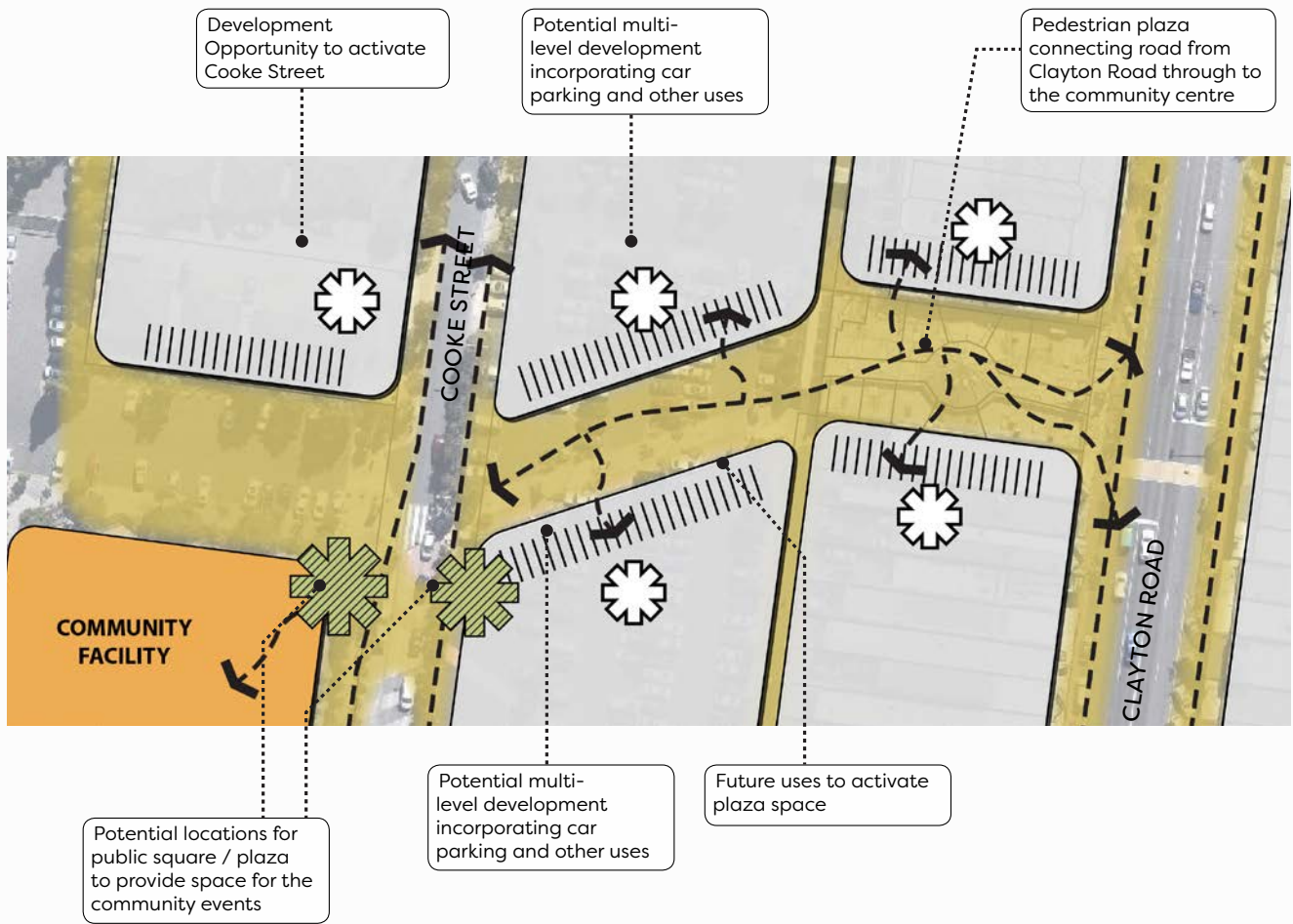


Figure 22. Potential Cooke Street Plaza Concept

Precinct 1: Key Projects continued

Key Projects

P1.3 Haughton Road Plaza

- Work with the community, traders and other stakeholder to develop a master plan for the Haughton Road Plaza. The plaza should:
 - Integrate with the adjoining hard surface play area under the railway line
 - Provide tree planting and soft landscaping
 - Maintain service access to existing properties
 - Consider opportunities to provide additional car parking through the reconfiguration of the road space

P1.4 South East Pocket Park

- Develop options for providing a new park in proximity of the Thomas Street / Centre Road car park. This could be provided within Council's current land holdings and be integrated into a future redevelopment of the car park

P1.5 Car Park Pedestrian Links

- Investigate potential locations and mechanisms for achieving additional mid-block links between Clayton Road and the existing at-grade car parks on Thomas Street and Cooke Street (where these sites may be subject to future development, they must retain some form of public parking in a multi-level parking facility). Ensure there are opportunities for surveillance of the links by adjoining buildings and clear sightlines between the car park and Clayton Road.

P1.6 McGregor Street Link

- Develop Planning Scheme measures to provide for a pedestrian link between Dunstan Street and McGregor Street

P1.7 Clayton Road Footpath Widening

- Implement planning scheme measures requiring building setbacks to achieve footpath widening along the eastern side of Clayton Road between Haughton Road the Clayton Town Hall and on the western side of Clayton Road between Dunstan Street and Haughton Road

P1.8 Cooke Street Footpath Widening

- Provide for a widened footpath along Cooke Street to support a future retail role of the street. Refer to Built Form Recommendations for more information.

P1.9 Pedestrian Priority

- Implement raised threshold paving in locations identified in Figure 18 to ensure pedestrian priority is maintained across key intersections

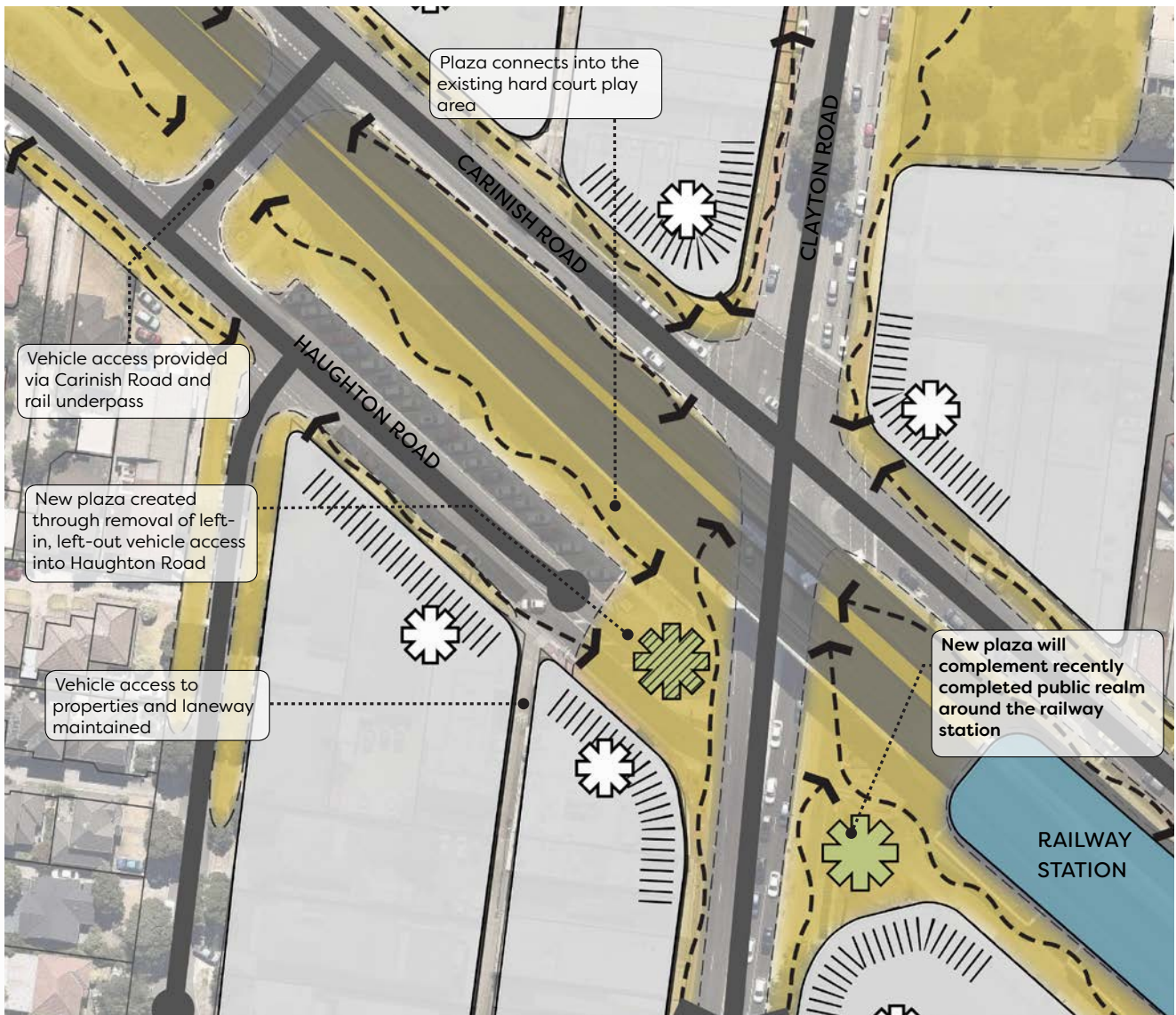


Figure 24. Potential Haughton Road Plaza Concept



Figure 23. A cross section through the potential Haughton Road Plaza

Precinct 1: Built Form and Development Requirements

The table below outlines specific development requirements for new buildings within Precinct 1. It should be read in conjunction with Figure 26 - Precinct 1 Built Form Plan, Figure 27 - Precinct 1 Cross Sections and Section 5.4 which outlines Activity Centre-wide built form requirements.

Development Requirements	Development Outcomes
<p>Building Heights</p> <ul style="list-style-type: none"> Building heights should not exceed height specified in Figure 26 <p>Street Wall</p> <ul style="list-style-type: none"> Street wall should not exceed height identified in Figure 26 Zero front and side setbacks up to the street wall height <p>Landscaped front setbacks</p> <ul style="list-style-type: none"> Provide 4m landscaped front setbacks along Thomas Street and Mary Street as specified in Figure 26 <p>Ground Level Setbacks for footpath / Laneway widenings</p> <ul style="list-style-type: none"> Provide ground level setbacks to achieve footpath and laneway widening as identified in Figure 22 as follows: <ul style="list-style-type: none"> W1 - 2.0m to achieve footpath widening along eastern side of Clayton Road W2 - 2.0m to achieve widening along the western footpath of Clayton Road W3 - 6.0m to provide for a consistent footpath alignment W4 - 3.0m to achieve footpath widening along the eastern side of Cooke Street W5 - 2.5m along west and north side of laneway to achieve laneway widening to 5.5m W6 - 2.5m along west side of laneway to achieve laneway widening to 5.5m W7 - 2.5m along east side of laneway to achieve laneway widening to 5.5m W8 - 2.5m along east and southern side of laneway to achieve laneway widening to 5.5m W9 - 2.5m along eastern boundary of 29 Dunstan Street and 2.5m along western boundary of 31 Dunstan Street to achieve a pedestrian link of 5.0m width 	<p>Development that:</p> <ul style="list-style-type: none"> Provides for additional hospitality, retail and commercial uses within the lower levels of development Reinforces key activity nodes and entries into the Activity Centre with higher scale and exemplar built form Promotes visual interest and variation in the Activity Centre skyline Promotes a sense of enclosure and continuity in built form Establishes a pedestrian scale environment at street level Avoids unarticulated façades that give a bulky appearance, especially from oblique views Avoids excessive stepping and reads as a vertical element from neighbouring properties Does not overwhelm existing residential properties and is articulated to minimise visual bulk when viewed from residential properties Minimises overlooking onto adjoining private open space Provides an overall scale that is compatible with adjoining low-scale residential areas Maintains solar access to Clayton Road, Cooke Street, Dunstan Street and the future Cooke Street Plaza Provides shade and shelter to footpaths in key retail areas Supports adequate servicing of development via rear laneways

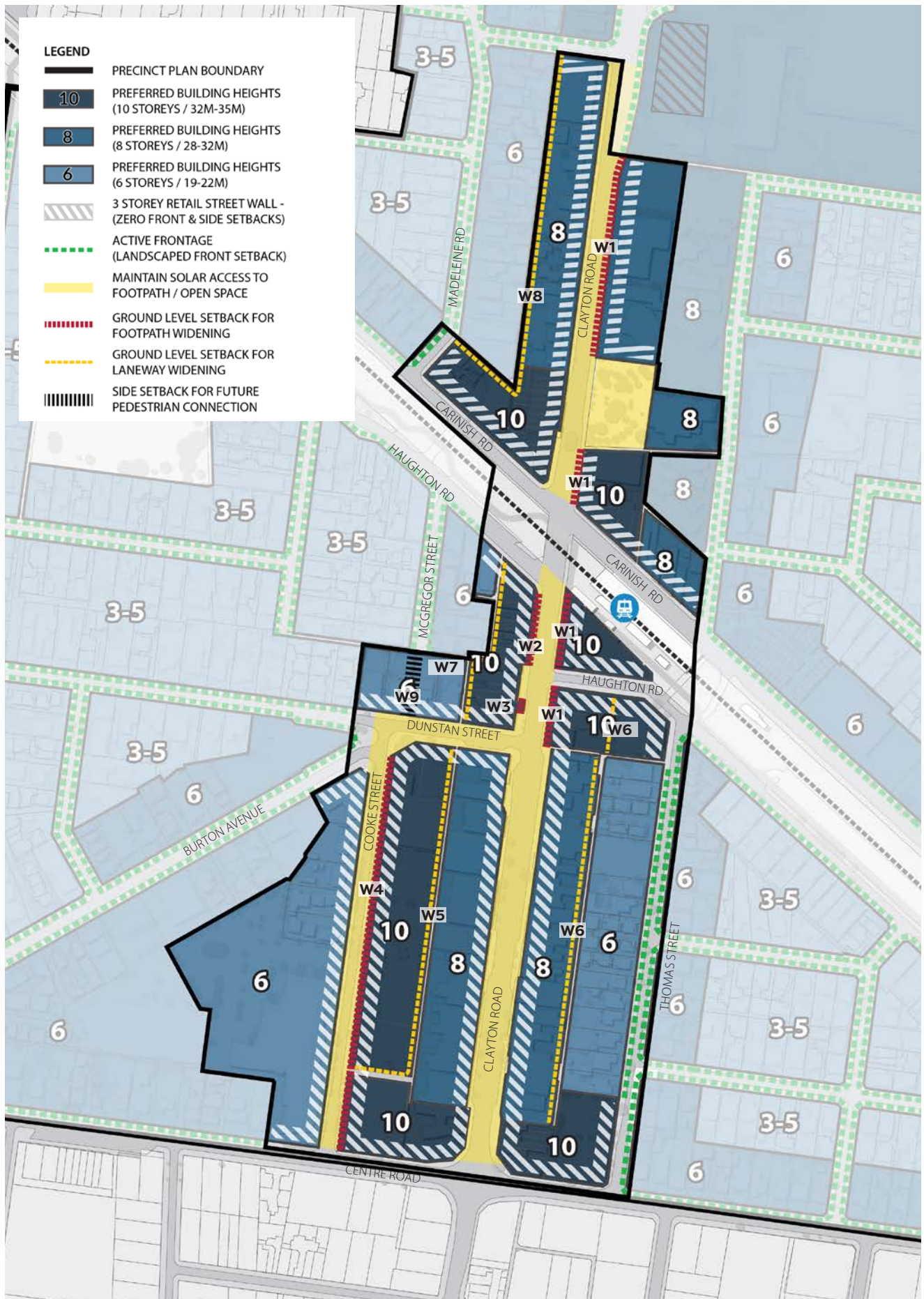
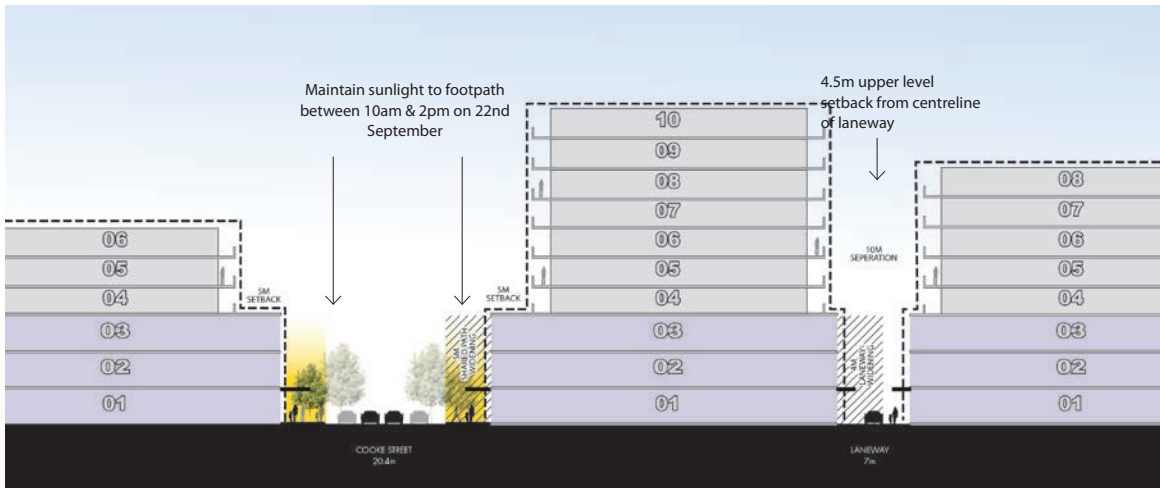


Figure 25. Precinct 1 - Built Form Plan

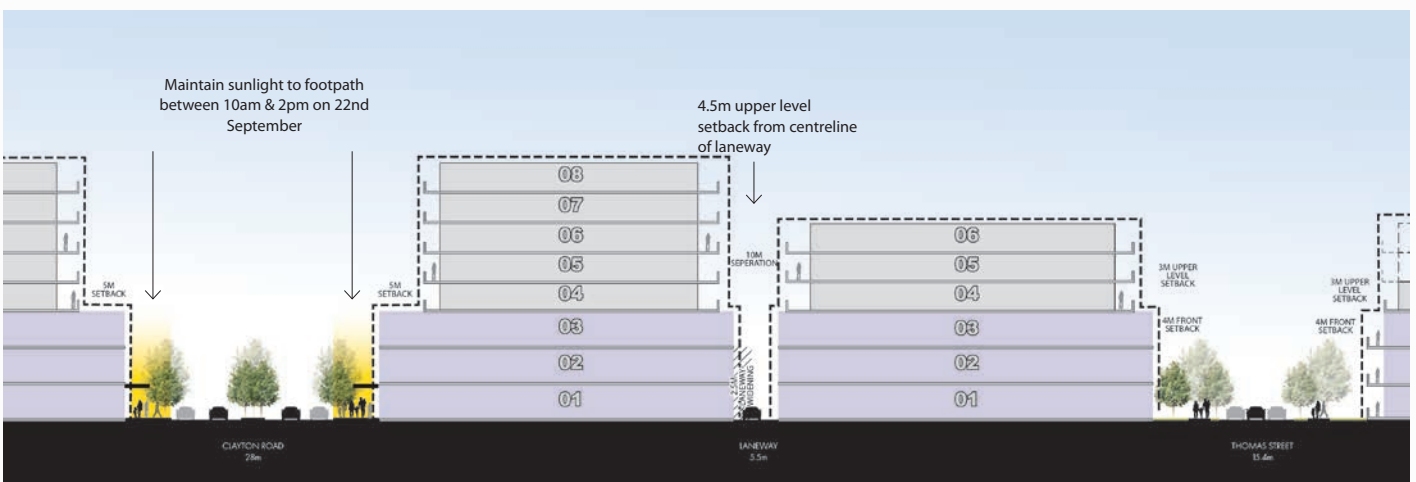
Precinct 1: Built Form and Development Requirements continued

Development Requirements	Development Outcomes
Upper Level Setbacks <ul style="list-style-type: none">The upper level setback from the front of the building should be a minimum of 5mDevelopment should adopt the same street setback for at least 65% of the height of the upper levels to avoid 'wedding cake' built form outcomes.	<i>See above</i>
Solar Access <ul style="list-style-type: none">New development should maintain solar access to footpaths and public spaces as follows:<ul style="list-style-type: none">Within 4.0 metres of the eastern and western boundary of Clayton Road between 10:00am and 2:00pm on 22 September (the equinox)Within 3.0 metres of the eastern and western boundary of Cooke Street between 10:00am and 2:00pm on 22 September (the equinox)Within 3.0 metres of the southern boundary of Dunstan Street and Haughton Road from 10.00am onwards on 22 September (the equinox)Beyond 15.0m of the northern boundary of the Clayton Remembrance Gardens at 10am on September 22 (the equinox)	
Residential Interface <ul style="list-style-type: none">Building height should not exceed 11.5m (3 storeys) at the residential interfaceNew development should minimise overshadowing to the private open space of existing residential dwellings located directly south, east or westMinimise stepping of upper level setbacks to provide a high quality outcome to the adjoining residential interfaceNew development should ensure that there is no view line from a habitable internal space to adjoining existing private open space to within 9m horizontal distance of the window	
Weather Protection <ul style="list-style-type: none">Incorporate verandahs into the façade design along all streets within Precinct 1 excluding Thomas StreetVerandahs should be at an appropriate height above the footpath to avoid damage whilst still providing effective weather protection, generally between 3 and 4.5m and consistent with adjoining sites	

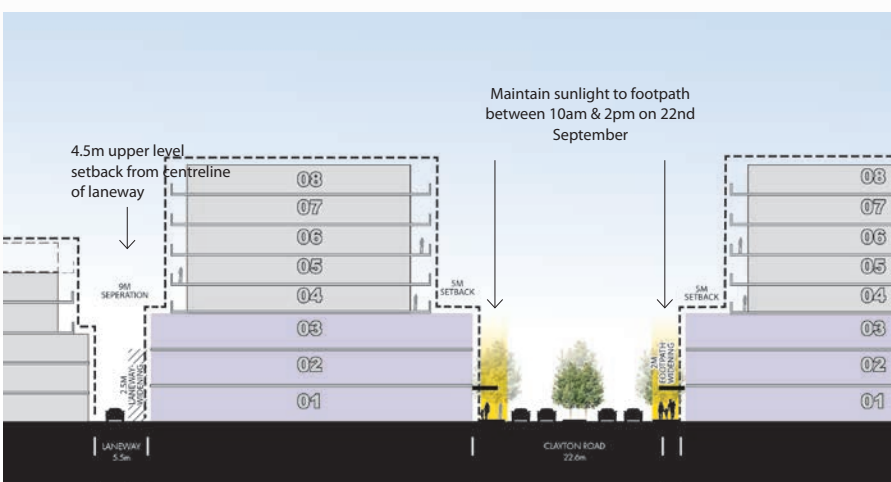
Cross Section A-A'



Cross Section B-B'



Cross Section C-C'



Cross Section Key Plan

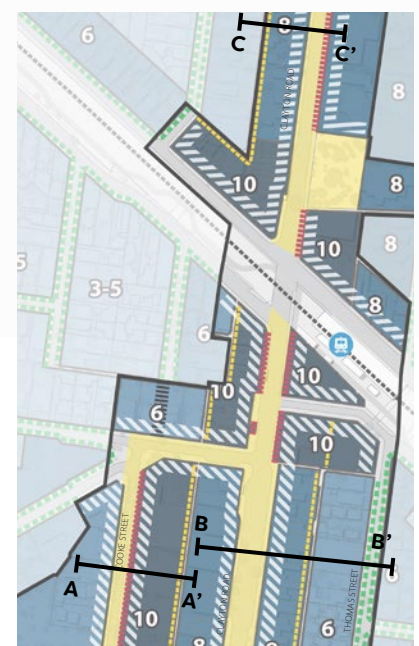


Figure 26. Precinct 1- Built Form Cross Sections

5.2 Precinct 2: Health and medical

Precinct Overview

The Health and Medical precinct forms a key gateway entry into the Clayton Activity Centre. New development activates the precinct through medical suites, offices, and housing, making the streets feel more lively and safer.

The Monash Medical Centre strengthens its role as land use and employment anchor for the precinct. It is better integrated with surrounding streets and clear and easy to navigate to and through.

The journey from the station along Clayton Road and Mary Street and up to Monash University is easy to take by foot or bike. This creates strong synergies between the Health and Medical Precinct and the University.

High quality, mixed use buildings integrate with the existing streetscape character by providing similar building proportions, articulated roof forms and landscaped front setbacks.

Precinct 2: Key Projects

Key Projects

P2.1 Clayton Road North Streetscape Upgrade

- Work closely with the community, traders and other stakeholders to prepare a streetscape master plan for northern section of Clayton Road. The Master Plan should consider:
 - Inclusion of a planted median to provide additional greening and allow for informal crossing opportunities
 - Consistent street tree planting and landscaping
 - Widening of footpaths with potential removal of nature strips
 - Utilisation of water sensitive urban design techniques
 - Consideration of a lane reduction and inclusion of on-street car parking if traffic role of Clayton Road is downgraded
 - Undergrounding of powerlines

P2.2 Mary Street Streetscape Upgrade

- Undertake streetscape improvements to Mary Street to create strong pedestrian connection from the Railway Station up to the Monash Medical Centre

P2.3 Railway to Medical Centre to Monash University Connection

- Finalise the Shared Path along Kanooka Grove, and the separated bike lane along Browns Road as part of the connection up to Monash University

P2.4 Monash Medical Centre

- Encourage the Monash Medical Centre to develop a master plan to improve pedestrian connections to and within the site, provide higher level of amenity for workers and visitors and enhance its interface to surrounding streets
- Support the installation of additional traffic signals as proposed by Monash Medical

Precinct 2: Key Projects

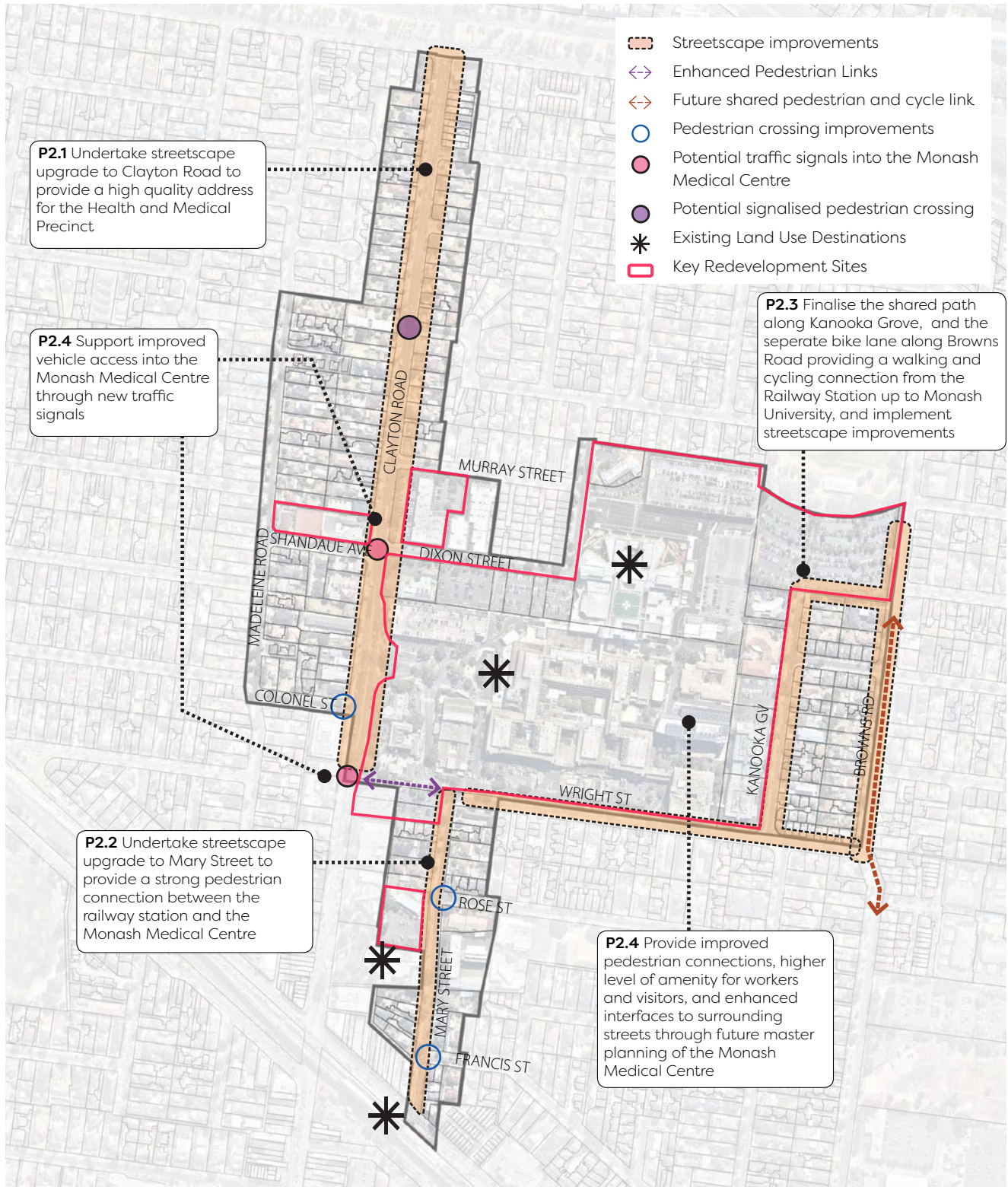
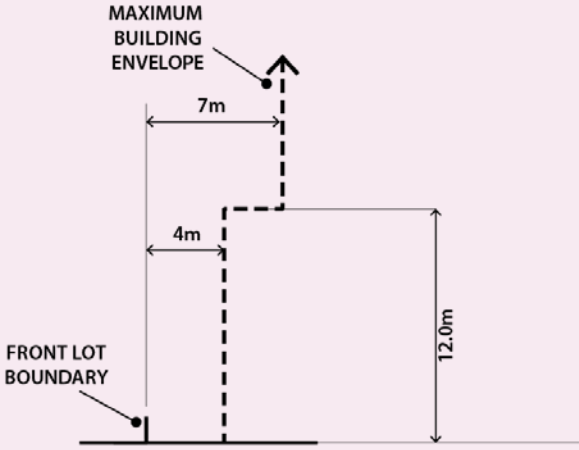


Figure 27. Precinct 2 - Key Projects

Precinct 2: Built Form and Development Requirements

The table below outlines specific development requirements for new buildings within Precinct 2. It should be read in conjunction with Figure 30 - Precinct 2 Built Form Plan, Figure 29 - Precinct 2 Cross Sections and Section 5.4 which outlines Activity Centre-wide built form requirements.

Development Requirements	Development Outcomes
<p>Front setbacks</p> <ul style="list-style-type: none"> • 4 metres landscape setback from the street for development up to 12.0m • 3 metres additional upper level setback for development above 12.0m  <p>The diagram illustrates the front setbacks for a building. A horizontal line represents the 'FRONT LOT BOUNDARY'. A vertical line indicates a height of '12.0m'. A dashed line shows the 'MAXIMUM BUILDING ENVELOPE'. The setback from the boundary to the start of the building is '4m'. The setback from the boundary to the start of the maximum building envelope is '7m'.</p>	<p>Development that:</p> <ul style="list-style-type: none"> • Provides for additional medical, office and residential mixed use developments • Creates a high quality address for the health and medical precinct • Promotes visual interest and variation in the Activity Centre skyline • Provides surveillance to footpaths through windows and habitable rooms at the street frontage • Provides a high quality landscaped setting for buildings that relates to the existing character • Establishes a pedestrian scale environment at street level • Avoids unarticulated façades that give a bulky appearance, especially from oblique views • Is articulated to reflect a mix of fine grain and wider frontages to support a diversity of uses • Avoids repetitive stepped/'wedding cake' profile

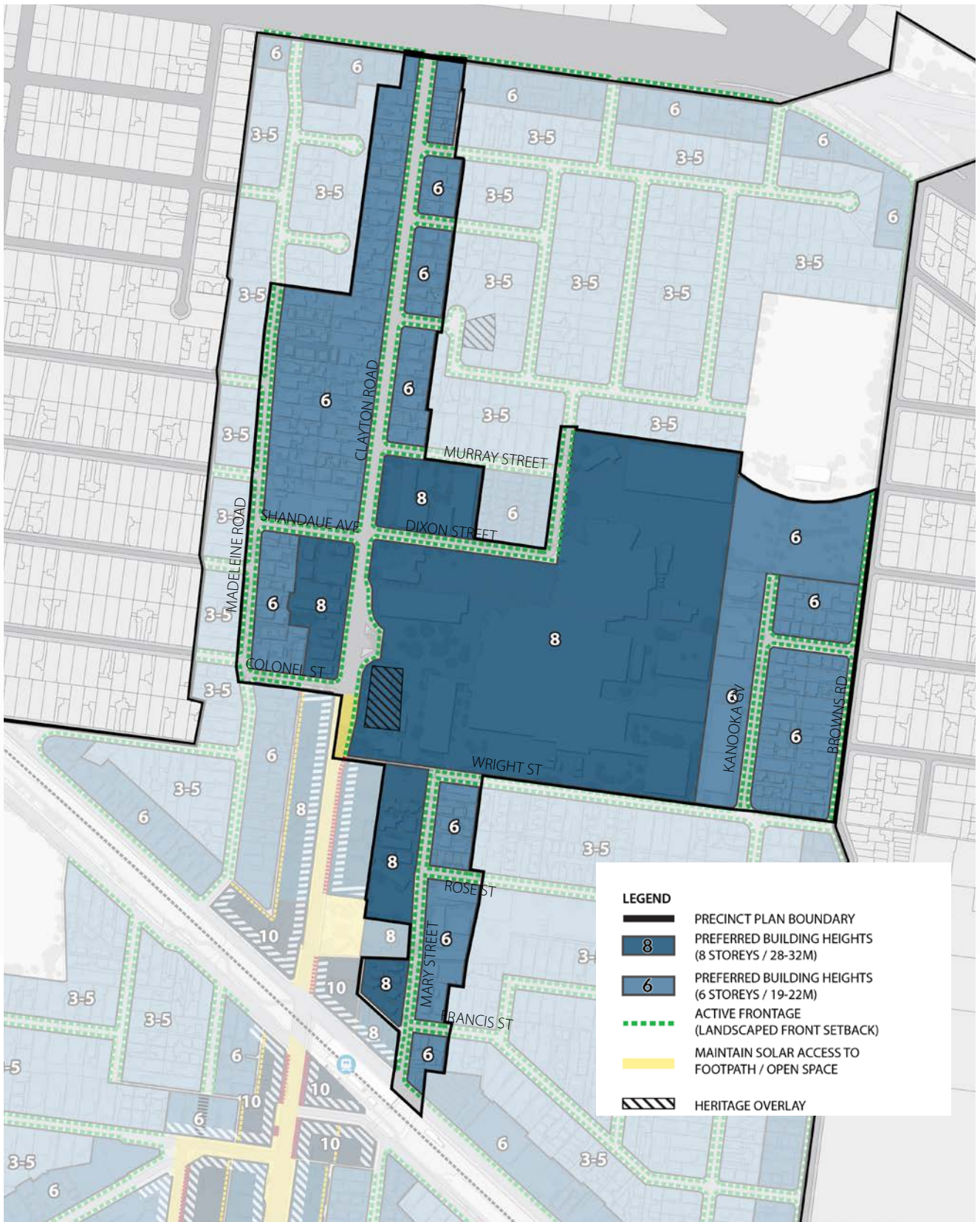
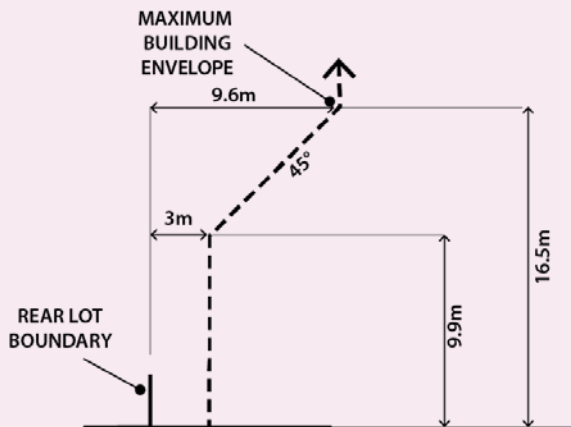


Figure 28. Precinct 2 - Built Form Plan

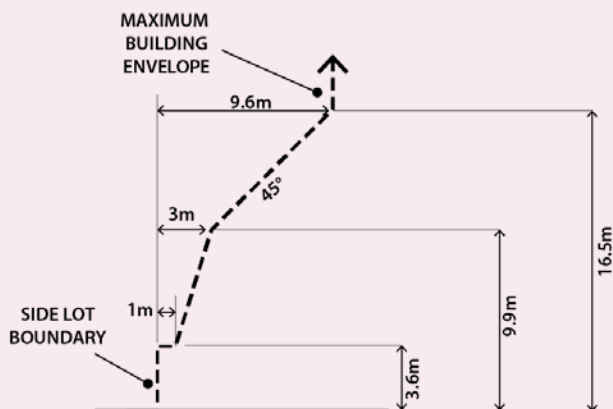
Rear setback

- 3 metre rear setback for development up to 9.9 metres, plus 1 metre additional setback for every metre of height over 9.9 metres up to a height of 16.5m.



Side setback

- 1 metre setback, plus 0.3 metres for every metre of height over 3.6 metres up to 9.9 metres, plus 1 metre for every metre of height over 9.9 metres, up to a height of 16.5m



Building Heights

- Building heights should not exceed height specified in Figure 30

Access

- Minimise disruption to the Clayton Road footpath by limiting driveway crossovers along Clayton Road. Utilise rear laneway access where it exists or the secondary road where properties have been consolidated
- Car parking, turning areas, loading areas or other hard stand areas should not be located in front setbacks

Cross Section A-A'

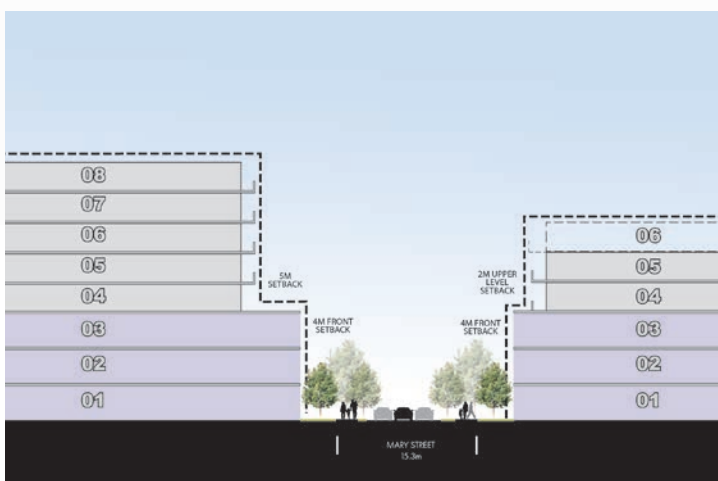
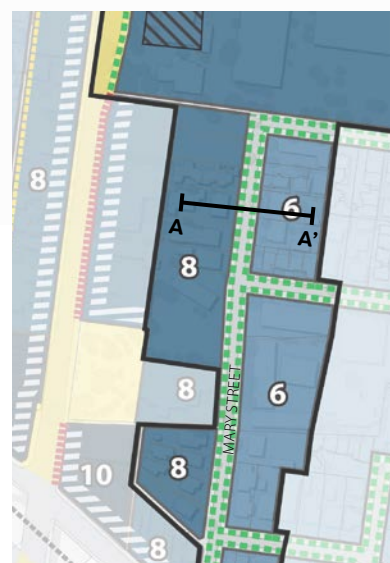


Figure 29. Precinct 3 - Built Form Cross Sections

Cross Section Key Plan



5.3 Precinct 3: Surrounding Residential

Precinct Overview

The Surrounding Residential Areas are located close to everything Clayton has to offer. The journey to the shops, station, work or University is an enjoyable experience along green and active streets.

There are a variety of housing types to choose from catering to range of people including looking to downsize, families, multi-generational families, couples and singles, and students.

Low scale apartment buildings and townhouses with landscaped front gardens sit comfortably next to detached dwellings and define a high quality and contemporary character for the precinct.

Precinct 3: Key Projects

Key Projects

P3.1 Pedestrian Priority

- Undertake and audit of key residential streets identified in Figure 14 to assess footpath width and condition, street tree cover, lighting, intersection delays for pedestrians and opportunities for passive surveillance improvements. Implement the recommended improvements following the audit to establish a high level of pedestrian priority

P3.2 Pedestrian Priority

- Implement raised threshold paving treatments along the key intersections identified opposite to provide safe and convenient pedestrian access into the centre.

P3.3 Railway to Medical Centre to Monash University Connection

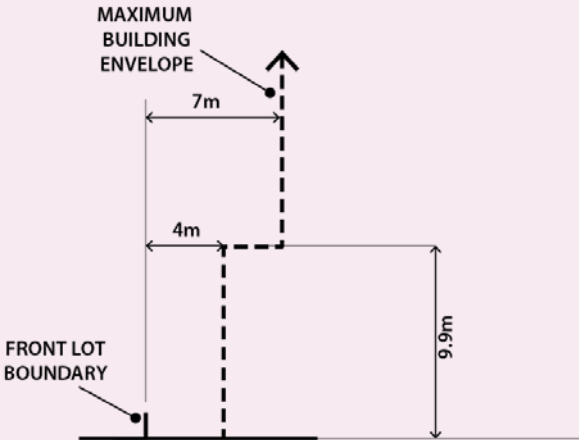
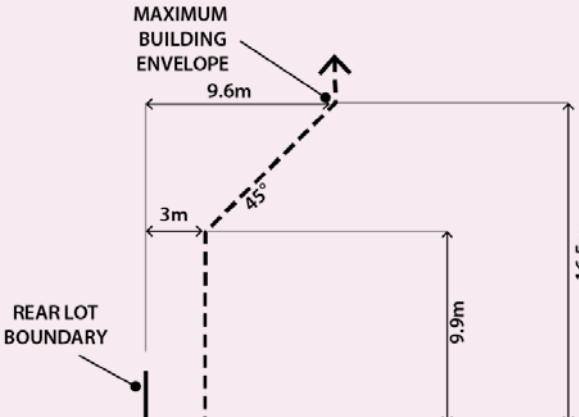
- Complete the Shared Path along Kanooka Grove, and the separated bike lane along Browns Road, as part of the connection up to Monash University,
- Undertake streetscape improvements to provide additional street tree planting along the shared path

P3.4 Pedestrian and Cycle Bridge

- Work with key stakeholders to understand the design, cost, benefits and funding opportunities for pedestrian and cycle footbridge connecting across the Princes Highway and Wellington Road from Browns Road to the Monash University

Precinct 3: Built Form and Development Requirements

The table below outlines specific development requirements for new buildings within Precinct 3. It should be read in conjunction with Figure 31 - Precinct 3 Built Form Plan, Figure 32 - Precinct 3 Cross Sections and Section 5.4 which outlines Activity Centre-wide built form requirements.

Development Requirements	Development Outcomes
<p>Front setbacks</p> <ul style="list-style-type: none"> • 4 metres landscape setback from the street for development up to 9.9m • 3 metres additional upper level setback for development above 9.9m  <p>The diagram illustrates the front setback requirements. A horizontal line represents the 'FRONT LOT BOUNDARY'. A vertical dashed line indicates a 4m setback from this boundary. A solid vertical line shows the building height up to 9.9m. A second vertical dashed line shows a 7m setback from the front boundary for the upper level of the building, which is shown as a dashed line extending upwards. The 'MAXIMUM BUILDING ENVELOPE' is indicated by a dashed line with an arrow pointing to the top corner of the upper level.</p>	<p>Development that:</p> <ul style="list-style-type: none"> • Provides a range of housing types within the Activity Centre • Contributes to creating high quality residential streets • Provides surveillance to footpaths through windows and habitable rooms at the street frontage • Provides a high quality landscaped setting for buildings that relates to the existing character • Avoids unarticulated façades that give a bulky appearance, especially from oblique views • Avoids repetitive stepped/'wedding cake' profile
<p>Rear setbacks</p> <ul style="list-style-type: none"> • 3 metre rear setback for development up to 9.9 metres, plus 1 metre additional setback for every metre of height over 9.9 metres up to 16.5m  <p>The diagram illustrates the rear setback requirements. A horizontal line represents the 'REAR LOT BOUNDARY'. A vertical dashed line indicates a 3m setback from this boundary. A solid vertical line shows the building height up to 9.9m. A dashed line with a 45-degree angle shows the upper level of the building extending to a total height of 16.5m. The horizontal distance from the rear boundary to the top corner of the upper level is 9.6m. The 'MAXIMUM BUILDING ENVELOPE' is indicated by a dashed line with an arrow pointing to the top corner of the upper level.</p>	

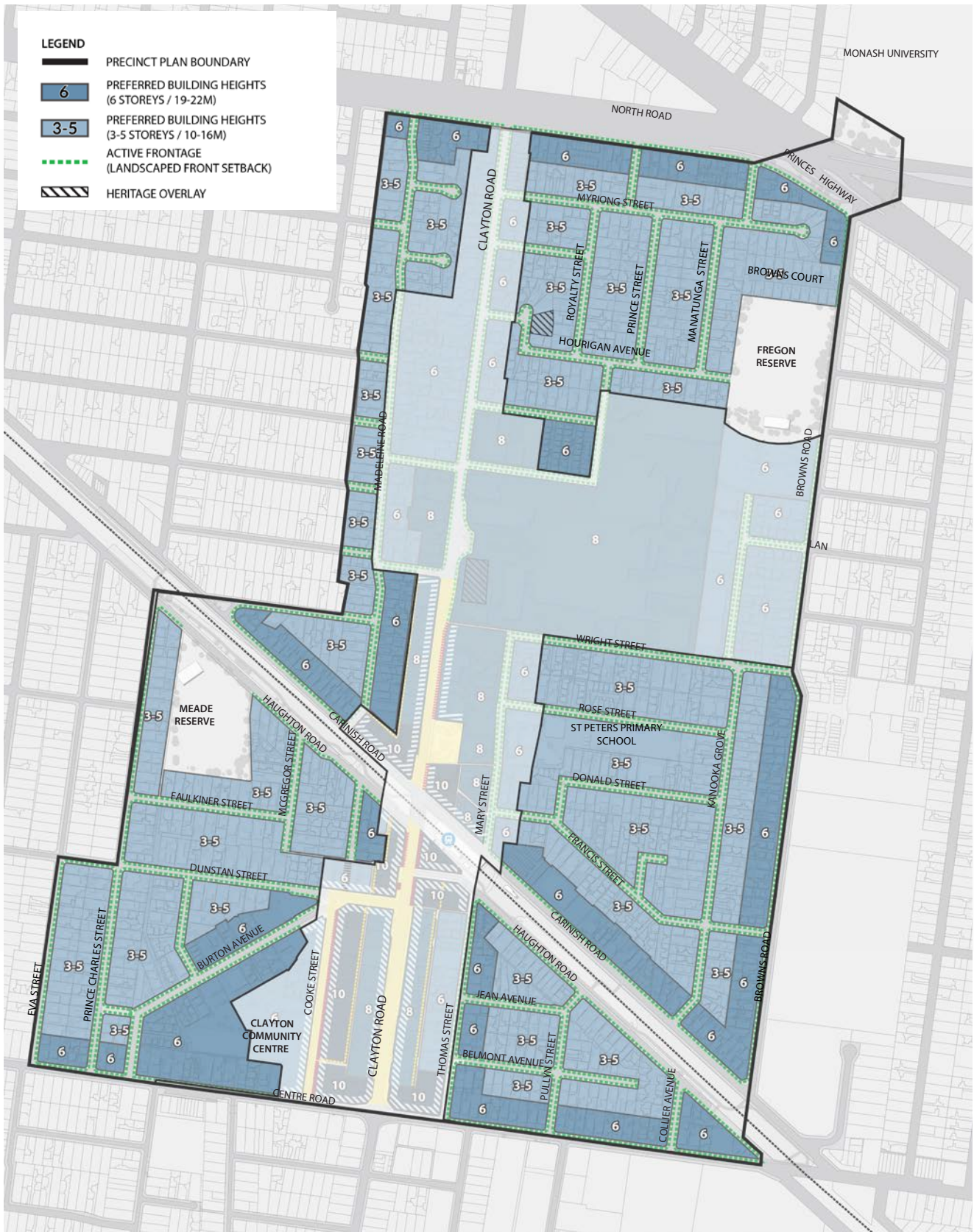
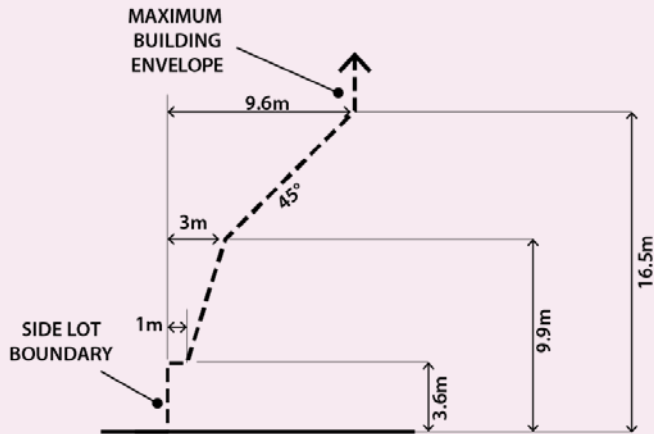


Figure 31. Precinct 3 - Built Form and Interface Diagram

Side setbacks

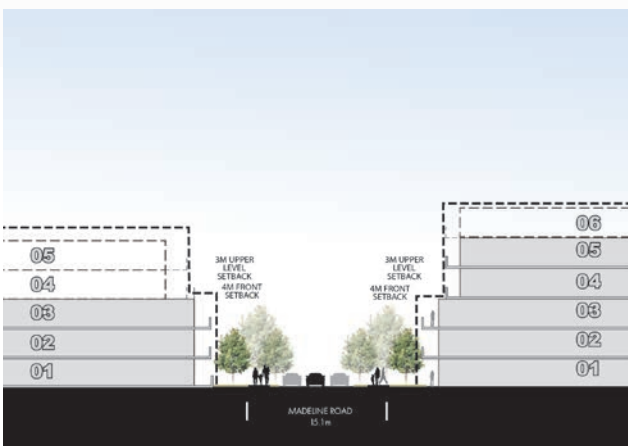
- 1 metre setback, plus 0.3 metres for every metre of height over 3.6 metres up to 9.9 metres, plus 1 metre for every metre of height over 9.9 metres, up to 16.5m



Building Heights

- Building heights should not exceed height specified in Figure 33 and should meet the requirements below:
 - Lots greater than 14 metres in width and less than 20 metres in width – 3 storeys (9.9m)
 - Lots greater than 20 metres in width and less than 30 metres in width – 4 storeys (13.2m)
 - Lots 30 metres in width or greater – 5 - 6 storeys (16.5m – 19.8m)

Cross Section A-A'



Cross Section Key Plan

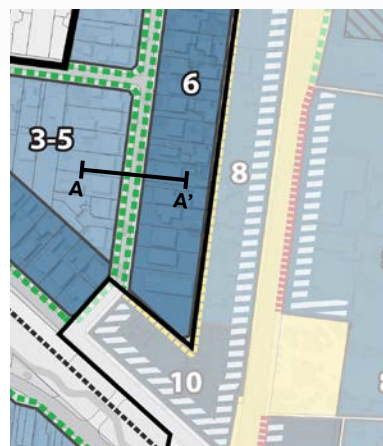


Figure 32. Precinct 3 - Built Form Cross Section

5.4 Centre-Wide Built Form and Development Requirements

The table below outlines Activity Centre-wide built form Objectives and Design Requirements. These should be read in conjunction with Precinct specific built form requirements

Objectives	Design Requirements
<p>Building Form and Design</p> <ul style="list-style-type: none"> <i>To encourage high-quality buildings that contribute positively to the streetscape through the use of innovative architectural responses</i> <i>To retain the existing fine-grain character of commercial/retail buildings throughout the Activity Centre and reflect this character in new development where relevant</i> <i>To encourage development that retains the existing human scale and does not dominate the adjoining public realm</i> <i>To encourage development that contributes to creating an attractive skyline when viewed within Clayton Road and surrounding residential areas</i> <i>To ensure all new development incorporates best practice Environmentally Sustainable Development (ESD) initiatives, such as reducing greenhouse gas emissions, maximising energy efficiency, reducing water consumption and minimising waste to landfill</i> 	<ul style="list-style-type: none"> Building design should minimise the visual bulk of large buildings through significant breaks and recesses in building massing New development should be designed to create human scaled places that promote visual and pedestrian amenity Buildings should reflect the existing grain pattern of narrow shop fronts within the shopping strip by incorporating separate ground floor tenancies and vertically modulated forms Provide wider frontages in each precinct to support opportunities for a diverse range of employment uses On larger buildings, articulate or divide roof forms into distinct sections in order to minimise visual bulk Buildings should utilise materials that do not generate glare, and can withstand the effects of weathering and wear to minimise maintenance and assist in achieving the 'high quality' development objectives of the Built Form Framework All new buildings should incorporate best practice Environmentally Sustainable Development (ESD) principles

Objectives

Design Requirements

Building Separation

- *To ensure development considers the amenity impacts on existing and potential future development across the Activity Centre*
 - *To minimise the visual impact of unarticulated walls to common boundaries*
 - *To provide opportunities for adequate sunlight to future development on adjoining properties*
- Development should provide upper level setbacks of at least 4.5m to the common side or rear boundary where windows of habitable rooms and balconies face onto the common boundary. Where development abuts a laneway the 4.5m setback is measured from the centreline of the laneway
 - Development should provide upper level setbacks of at least 3.0m to the common side or rear boundary for non-habitable rooms and service areas
 - New development should consider the impact on future development opportunities of adjoining sites when determining building setbacks, interfaces and orientation of building forms

Frontages

- *To ensure buildings within core retail areas and along key pedestrian streets contribute to active and engaging street frontages and support a high level of pedestrian amenity*
- On all street frontages, buildings should provide pedestrian interest and interaction with a permeable façade incorporating windows and door openings with clear glazing
 - Buildings on corner sites should be designed to actively address both frontages at street level
 - Buildings incorporating podium forms should provide opportunities for activation of upper podium levels to support passive surveillance of the public realm
 - Buildings fronting laneways should be designed for passive surveillance with a permeable façade, including windows and door openings
 - Upper levels of buildings should be designed to provide habitable rooms with windows or balconies that overlook the public realm

Objectives

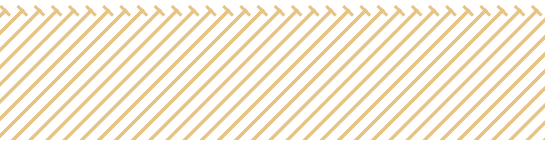
Design Requirements

Access and Services

- *To provide site and building services that are incorporated into the design of developments and screened from public view*
 - *To minimise the visual impact of car parking, access, loading and service activities from the street so that it does not adversely affect streetscape character*
- Pedestrian entries to buildings should be clearly visible and easily identifiable from the street and accessible for all abilities
 - Residential entries should be distinguished from retail and commercial entries.
 - Vehicle crossovers should be minimised and located to prevent traffic disruption and preserve nature strips and street trees if present
 - Parking and vehicle entries should not present as a dominant element when viewed from the public realm. Appropriate and innovative screening and screen planting should be incorporated where necessary.
 - Car parking, turning areas, loading areas or other hard stand areas should not be located in front setbacks
 - Loading and service access to be located off laneways or secondary streets
 - Undercroft and podium level car parking should be set behind built form providing active uses to the street. Use of other screening from the street, such as landscaping and/or articulated screening, may be acceptable where it can be demonstrated that active use to the street is not achievable

Landscaping

- *To improve the amenity of key strategic development sites through the provision of high quality public open space*
 - *To provide landscaping that is integrated with the design of the development and complements the landscaping of the adjoining public realm*
 - *To ensure high quality public realm outcomes*
- Retain existing trees and provide for the planting of new vegetation, including canopy trees, wherever possible.
 - Where landscaped setbacks are required, provide a mix of canopy trees, low shrubs and grasses that allow for uninterrupted views into the adjoining public realm.
 - Communal garden spaces should be provided at podium and rooftop levels where appropriate to create amenity for residents, workers and visitors. The gardens should take into consideration, aspect, materials and solar orientation.
 - Key developments sites are encouraged to provide pedestrian amenity through the provision of high quality public open space that is well connected and integrated to public pedestrian networks.



06 IMPLEMENTATION

This section outlines the next steps for the project, identifies a process for monitoring and reviewing the Precinct Plan and provides a summary of mechanisms for future infrastructure funding,

6.1 Next Steps

Draft Precinct Plan Consultation

The Draft Precinct Plan will be available for public comment for a six week period. During this period there will be a range of ways to provide comment on the contents of the Precinct Plan.

Finalisation of the Structure Plan

Following the consultation period comments and feedback on the Draft Precinct Plan will be collated and considered in the finalisation of the Precinct Plan

Implementation Plan

An implementation plan will be developed following the finalisation of the Precinct Plan. The implementation plan will outline the steps required to implement each of the actions in this document including implementation responsibilities, priorities and timing. This will ensure that the aspirations of the Vision will be realised through projects on the ground.

Council Adoption

The Final Structure Plan and Implementation Plan will be issued to Council for formal adoption.

Planning Scheme Amendment

Once the final Structure Plan is adopted by Council, a planning scheme amendment will be prepared to implement aspects of the Precinct Plan into the Monash Planning Scheme. This will undergo an additional phase of consultation.

6.2 Monitoring and Review

As part of the implementation process, Monash City Council will provide a progress report on the implementation of the Precinct Plan. The Precinct Plan review process will occur every five years. This will identify and analyse:

- The actions and projects that have been undertaken or underway
- The success of key actions or projects
- Consistency of all projects with the vision for the Clayton Activity Centre
- Any blockages preventing successful implementation

This process will allow Council to measure the success of the program and allocated future resources as necessary and allow for the implementation program to be adjusted to ensure the vision is being achieved.

6.3 Infrastructure Funding

This section provides an overview of the mechanisms available to collect contributions to public infrastructure identified in the Precinct Plan from development proponents and landowners within the Precinct.

Potential Infrastructure Types

It is understood that the following public infrastructure types are likely to be required as part of the Precinct Plan:

- Road transport improvements to roads, intersections and parking
- Pathway improvements such as pedestrian and cycling paths
- Streetscape upgrades such as the proposed Clayton Road upgrade
- New and improved open space, e.g. landscaping, civic areas, connections from the Activity Centre to existing open space areas
- New and expanded community facilities
- Major transport initiatives such as fixed rail infrastructure

Some of these infrastructure types may also require land acquisition.

Infrastructure Funding Mechanisms

The table opposite provides a summary of potential infrastructure funding mechanisms available and the circumstances / conditions where each commonly applies. It is important that any infrastructure funding or value capture mechanisms are introduced prior to or concurrent to the introduction of any new planning controls or zones.

The mechanisms identified as having high relevance to Clayton will be considered during the development of an Implementation Plan for the Precinct Plan.

Mechanism	Infrastructure types commonly funded in Activity Centres	Considerations	Relevance to Clayton
Development Contributions Plan (DCP)	Roads, paths, community facilities, recreation, streetscapes, drainage	Requires a Planning Scheme Amendment and detailed justification. Council to manage, need to ensure feasibility based on cost apportionment to existing and new development	High
Infrastructure Contributions Plan (ICP)	Roads, paths, community facilities, recreation, streetscapes	ICP not yet available in established areas. Not clear whether Clayton would qualify as a Strategic Development Area, pending finalisation of system	Low
Public open space (POS) contributions	Open space acquisition and improvements, including passive, linear and active reserves	A separate POS contribution could be applied to the activity centre to fund specific open space works. Requires Planning Scheme Amendment	Moderate
Special Rates Scheme	Marketing, branding initiatives and smaller streetscape improvements	Requires majority of landowners / businesses to agree to any scheme. Commonly applied in activity centres where there is a community of interest, such as a local retail sector	High
Car parking levies	Shared car parking facilities and associated land	Requires an overarching approach to car parking in the activity centre and implementation through the Planning Scheme	High
Broad-base value capture	Major public transport projects	State government rail projects such as Suburban Rail Loop and Rowville Rail Extension could result in land value uplift along rail corridors and at major nodes. Mechanisms such as Tax Increment Financing and Infrastructure Recovery Charges could be applied. Few such mechanisms have been successfully introduced in Victoria	High

